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Merton Council

Planning Applications Committee Agenda

Membership

Councillors:

Linda Kirby (Chair)
Stephen Crowe (Vice-Chair)
Billy Christie
David Dean
Joan Henry
Rebecca Lanning
Russell Makin
Simon McGrath
Dave Ward
Peter Southgate

Substitute Members:

Eloise Bailey
David Chung
John Dehaney
Edward Foley
Edward Gretton
Najeeb Latif

Date: Thursday 22 October 2020

Time: 7.15 pm

Venue: This will be a virtual meeting and therefore will not take place in a physical location, in accordance with s78 of the Coronavirus Act 2020.

This is a public meeting and can be viewed at www.youtube.com/user/MertonCouncil.

If you wish to speak please see notes after the list of agenda items. For more information about the agenda and the decision making process contact democratic.services@merton.gov.uk or telephone 020 8545 3616. Press enquiries: communications@merton.gov.uk or telephone 020 8545 3181.

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Planning Applications Committee Agenda

22 October 2020

- 1 Apologies for absence
- 2 Declarations of Pecuniary Interest
- 3 Minutes of the previous meeting 1 - 4
- 4 Town Planning Applications
The Chair will announce the order of Items at the beginning of the Meeting.
A Supplementary Agenda with any modifications will be published on the day of the meeting.
Note: there is no written report for this item
- 5 Road Bridge Bishopsford Road - London Road Morden SM4 5 - 52
Application: 20/P2438
Ward: Ravensbury
Recommendation: Grant Planning Permission subject to conditions.
- 6 Melrose School, Church Road, Mitcham, CR4 2BE 53 - 88
Application: 20/P2184
Ward: Cricket Green
Recommendation: Grant Planning Permission subject to conditions
- 7 101 Hamilton Road, South Wimbledon, SW19 1JG 89 - 138
Application: 20/P2547
Ward: Abbey
Recommendation: Grant Planning Permission subject to conditions and s106 agreement
- 8 16 - 20 Morden Road, South Wimbledon, SW19 3BN 139 - 192
Application: 19/P3772
Ward: Abbey
Recommendation: Grant Planning Permission subject to conditions and s106 legal agreement
- 9 2 Westcoombe Avenue, West Wimbledon, London, SW20 0RQ 193 - 224
Application: 20/P1483
Ward: Raynes Park
Recommendation: Grant planning permission subject to any resolution made by Committee pertaining to facing material and conditions.
- 10 Planning Appeal Decisions 225 - 228
Officer Recommendation:

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That Members note the contents of the report.

11	Planning Enforcement - Summary of Current Cases	229 -
	Officer Recommendation:	234

That Members note the contents of the report.

Note on declarations of interest

Members are advised to declare any Disclosable Pecuniary Interest in any matter to be considered at the meeting. If a pecuniary interest is declared they should withdraw from the meeting room during the whole of the consideration of that matter and must not participate in any vote on that matter. For further advice please speak with the Managing Director, South London Legal Partnership.

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Agenda Item 3

All minutes are draft until agreed at the next meeting of the committee/panel. To find out the date of the next meeting please check the calendar of events at your local library or online at www.merton.gov.uk/committee.

PLANNING APPLICATIONS COMMITTEE

24 SEPTEMBER 2020

(7.15 pm - 8.23 pm)

PRESENT

Councillors

Councillor Linda Kirby (in the Chair), Councillor Stephen Crowe, Councillor Billy Christie, Councillor Joan Henry, Councillor David Dean, Councillor Rebecca Lanning, Councillor Russell Makin, Councillor Simon McGrath, Councillor Dave Ward and Councillor Peter Southgate

Tim Bryson (Development Control Team Leader (North)), Jonathan Lewis (Development Control Team Leader (South)) and Neil Milligan (Development Control Manager, ENVR)

1 APOLOGIES FOR ABSENCE (Agenda Item 1)

None.

2 DECLARATIONS OF PECUNIARY INTEREST (Agenda Item 2)

Councillor Crowe and Councillor Dean declared a non-pecuniary interest in item 7 (2 Westcoombe Avenue), in that as they are friends with one of the neighbours of the site, they would not participate in the debate or decision on this item.

3 MINUTES OF THE PREVIOUS MEETING (Agenda Item 3)

RESOLVED that the minutes of the previous meeting on 20 August 2020 are agreed as an accurate record of the meeting subject to minute 2 being amended to the following (underlined):

“Item 2: Councillor Dean declared a personal interest in item 6, (Tooting & Mitcham FC, Bishopsford Road, SM4 6BF), in that he had been working with Tooting team for a number of decades and as he knew them in a personal capacity he would not be voting on this item.”

4 TOWN PLANNING APPLICATIONS (Agenda Item 4)

Supplementary agenda: Amendments and modifications to the Officer’s report were published in a supplementary agenda. This applied to item 6 only.

The Chair advised that the order of items would be as they appeared in the agenda.

5 FORMER MITCHAM FIRE STATION, 30 LOWER GREEN WEST, MITCHAM, SW20 0RQ - HOARDINGS (Agenda Item 5)

Proposal: Erection of hoardings to front of fire station for a period of 12 months.

The Committee noted the report and presentation of the Planning officer.

One objector had registered to speak in objection and at the invitation of the Chair, made a number of points, including that the hoardings were erected around an excessive area of additional land beyond the former fire station and visually impaired the Conservation Area.

In response to points raised by the registered speaker and questions from the Committee, the Planning officer gave advice including:

- Enclosures are considered a robust means by which to prevent vacant sites being used by travellers and squatters
- In light of the fact that this is in a conservation area and flytipping issues in other parts of the borough, consideration should be given to keeping the forecourt secure.

Having considered the application and the points raised, the Committee RESOLVED that application 19/P3904 be GRANTED planning permission subject to conditions.

6 RIDGWAY STABLES, 93 THE RIDGWAY, WIMBLEDON, SW19 4SU
(Agenda Item 6)

Proposal: Demolition of existing stable buildings involving re-instatement of ground level retail unit and erection of new stables with offices above plus caretaker accommodation.

The Committee noted the report and presentation of the Planning officer, and the modifications contained in the supplementary agenda.

There were no registered speakers for this item.

Further to questions from the Committee, the Planning officer confirmed the following:

- No viability was put forward but any future plans to change that would be judged on their own merits.
- There was no need for the application to demonstrate demand with the acceptable principle use
- The conditions included material samples conditions
- The site is due to be adopted as locally listed by the Council and a condition is to be attached to seek to retain the important original features internally where feasible (such as the fire places outlined by the Conservation Officer).

Having considered the application, the Committee unanimously RESOLVED that application 20/P0024 be GRANTED planning permission subject to conditions.

7 2 WESTCOOMBE AVENUE, WEST WIMBLEDON, SW20 0RQ (Agenda Item 7)

Proposal: Erection of two storey side extension, a part-single part-two storey rear extension, front porch extension and rear roof extensions with associated facade changes and landscaping.

Note: Cllr Crowe and Cllr Dean had declared a personal interest in this item and did not take part in its discussions or decision by switching off their cameras and muting their microphones (see item 2 above).

The Committee noted the report and presentation of the Planning officer.

Councillor Adam Bush registered to speak against the application, objecting to the proposal's overdevelopment and overlooking into neighbouring gardens. In response to concerns of visual impact, the planning officer advised that whilst the proposal would be closer to the property's boundary, it would not come up against the back garden. The officer also advised that the first floor windows of the existing property already looked into the neighbour's garden; however, if the Committee had concerns of loss of privacy, this could be remedied by introducing restrictions on glazing.

Further to questions from the Committee, the Planning officer advised that if the Committee had concerns that the trees on the site would be damaged during construction, conditions requiring protection measures could be included. There was further debate regarding the quality of the CGI images provided to the Committee and its reflection of the materials to be used, and the visual impact of trees. Further to this debate, the Committee agreed to include a requirement to view the materials to be used for the proposal, but not to include any conditions regarding the trees.

The Chair moved to a vote and it was unanimously RESOLVED that application 20/P1483 be GRANTED planning permission subject to conditions as set out in the report, plus a requirement for the Committee to view the materials to be used for the proposal.

8 PLANNING APPEAL DECISIONS (Agenda Item 8)

This was noted by the Committee.

9 PLANNING ENFORCEMENT - SUMMARY OF CURRENT CASES (Agenda Item 9)

None.

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PLANNING APPLICATIONS COMMITTEE 22 OCTOBER 2020

Item No:

UPRN

APPLICATION NO.

DATE VALID

20/P2438

06th August 2020

Address/Site

Road Bridge Bishopsford Road - London Road Morden SM4

Ward

Ravensbury

Proposal:

ERECTION OF REPLACEMENT BRIDGE TO RECONNECT BISHOPS FORD ROAD TO LONDON ROAD IN MITCHAM, WHERE THE A217 CROSSES OVER THE RIVER WANDLE). THE PROPOSAL INCLUDES A CHANGE IN THE ROAD ALIGNMENT TO THE NORTH OF THE BRIDGE AND CHANGES THAT WOULD BE REQUIRED TO THE EASTERN BOUNDARY OF RAVENSBURY PARK

Contact Officer: Awot Tesfai

RECOMMENDATION

GRANT Planning Permission subject to conditions

CHECKLIST INFORMATION.

- S106 Legal Agreement: Not required
- Mayor of London Referral: Not required
- Secretary of State Referral: Not required
- Is a screening opinion required: No
- Is an Environmental Statement required: No
- Has an Environmental Impact Assessment been submitted – No
- Design Review Panel consulted – Yes
- Number of neighbours consulted – 224
- Press notice – Yes
- Site notice – Yes
- External consultations: Transport for London, Environment Agency, Metropolitan Police (Secured by Design), Metropolitan Police (Traffic Safety) Historic England (Greater London Archaeological Advisory Service), Historic England Development Management)), Thames Water Utilities, Natural England, UK Power Networks, British

Telecom, SGN UK Gas Distribution, National Trust, Canal and River Trust.

- Number of jobs created – N/A
- Public Transport Accessibility Level (PTAL): 2
- Flood Zones 2 and 3
- MOL – Wandle Valley

1. INTRODUCTION AND SUMMARY

This application for replacement of the Bishopsford Road Bridge has been submitted following partial collapse of the bridge during strengthening works undertaken in June 2019. This planning application is brought before Planning Applications Committee for consideration due to the scope and number of representations.

- 1.1 The current application seeks planning permission for replacement of Bishopsford Bridge. The proposals also involve widening the footways to provide improved accessibility for cyclists and pedestrians with designated lanes. It is considered that the proposed development would cause less than substantial harm to the architectural and historic interest of the bridge or the setting of neighbouring surrounding area, and to the character and appearance of the conservation area.
- 1.2 The Flood Risk assessment submitted with the application is also considered acceptable as the 'hydraulic model' carried out demonstrated that there will be further reductions to flood risk for the site and surrounding. The Environment Agency and the Councils Flood Risk officer have reviewed the report and confirm its acceptance, which has also been considered in the main body of this report.
- 1.3 Having also paid special regard to the desirability of preserving the bridge and some of its features of special architectural and historic interest, and paid special attention to the desirability of preserving or enhancing the character and appearance of the conservation area. It is considered that the proposal complies with local and national planning policies in all other respects. No other planning considerations are identified that would warrant refusal of the planning application.
- 1.4 The applicant has fulfilled the requirements to provide plans and various other documents in support of the proposed planning application, which has complied with the statutory validation process. Members should note that conditions are also being imposed with the grant of any planning approval requiring the applicant to provide further details to be submitted and to be agreed in writing by the LPA.

1.5 **Background**

- 1.6 The historical records relating to Bishopsford Bridge show that this location has been a fording point since the middle ages. The three arch Bishopsford Bridge was built circa 1789 century and there are survey records of the current bridge that dates back to 1882.
- 1.7 The upstream footbridge was built over the former ford circa 1947, attached to the highways bridge and wholly changing the view of the bridge from upstream. The deck of the upstream footbridge was completely replaced in 2010 along with piling work to the north and south abutments. In 2010 a wooden footbridge was built downstream about 6 metres from Bishopsford Bridge in Watermeads Park.

2. **SITE AND SURROUNDINGS**

- 2.1 The site comprises the Bishopsford Road Bridge which runs over the River Wandle located within the London Borough of Merton. It carries the single carriageway and footways of the A217 London Road. The approximate Ordnance Survey Grid Reference for the centre of the site is 527174E, 167834N with the nearest post code at SM4 6AW.
- 2.2 To the west of the site lies part of Ravensbury Park and adjoins a wooden footbridge that crosses the River Wandle, situated close to the west (downstream) elevation of the of the road bridge. This was built on the upstream side, over the old ford. Further west of the site lies the neighbouring residential area of Octavia Close. To the East of the Site lies the National Trust Land and the Watermeads Nature Reserve. To the south-east of the site lies the Tooting and Mitcham Community Sports Club.
- 2.3 The application site is located within the Wandle Valley Conservation Area. Sub areas 5 and 6 are the most relevant to Bishopsford Bridge. The characteristics of the conservation area at this point include the forms of enclosure: the river itself, boundary walls, fences, metal railings and gates that typify the conservation area. These are visible on the lead up to the bridge from the Mitcham / Cricket Green side and into Ravensbury Park. Metal railings are also visible on the bridge parapet and will be salvaged during demolition.
- 8.5.1 The site has the following environmental planning designations in the Sites and Policies Plan (2014) and Core Planning Strategy (2011);
- Green Corridor – Ravensbury Park (CS13, DM02)
 - MOL – Wandle Valley (CS13, DM01)
 - WVRP Brangwyn Crescent 400m buffer (CS5, CS13, CS para 21.13, DM01)
 - Green Chains (CS13, DM01)
- 2.4 The site is maintained by Merton London Borough Council. The characteristics and design of the existing bridge comprise of three arch masonry bridge and the material is brick. The total length of the bridge is 22 m (72 ft.), width 10 m (33 ft.), height 1.7 m (6 ft.), with 2 piers in the water and 2 vehicle lanes including shared pedestrian and cycle lanes on the bridge. The clear square span of the arches are 2.709m (north), 3.324m (centre), and 2.724m (south).

- 2.5 The bridge is situated in the area covering the "Watermeads" housing development, National Trust land and the Grove Mill complex. The site was once in the setting of several watermills, which dates back to Domesday. The National Trust land is an important riverine wet land area and is now a nature reserve.

3. CURRENT PROPOSAL

- 3.1.1 The proposed bridge will be constructed at the same location as the existing bridge and will not change the current profile or the alignment of the River Wandle. The proposed bridge comprises of clear span structure with a soffit level of 19.2mAOD and span of 10m. The proposed bridge will provide a greater aperture for flow conveyance beneath the bridge, replacing the existing bridge structure that comprises three brick arches of varying soffit level (19.09mAOD, 19.22mAOD and 19.47mAOD). The existing arch piers have been removed as part of the demolition works and the river bed reinstated with a minimum 200mm depth of washed gravels.
- 3.1.2 The width of the River Wandle upstream of the Bishopsford Road Bridge measures approximately 25m. The distance between the existing bridge abutments measures 10.497m, although the aperture between each of the three arches measures approximately 8.24m. As per above, the new structure will comprise a clear span between the abutments of 10m and the removal of the piers promotes further re-naturalisation of the river bed and an overall increase in gravel bed for fish and spawning habitat. The proposed wing-wall design for the new structure also allows some further bank enhancement compared to the existing structure.
- 3.1.3 The wall bordering Ravensbury Park is proposed to be substituted for a 1.5m high park railing, this will be similar with the form and colour (black), currently in place on the boundary with the National Trust land, which is situated on the other side of the road. Officers consider this to reflect well with the surrounding National Trust railings, which would allow improved views within the Metropolitan Open Land.
- 3.1.4 Proposals would also involve widening the bridge to incorporate improved pedestrian and cycle access, in doing so, the scheme will create significant improvements to provide inclusive access for all in accordance with Policy CS13 and CS14 of the Core Planning Strategy (2011).
- 3.1.5 The proposals noted above are further discussed in detail within the main consideration section of this report.

4. PLANNING HISTORY

- 4.1 There are numerous entries under the planning history for the immediate area surrounding the bridge and most of these applications relate to tree works with applications dating back to circa 1990.
- 4.2 The most relevant applications has been listed below;

4.3 **10/P1491** - FORMATION OF VEHICULAR CROSSOVER ONTO WATERMEADS NATURE RESERVE TO FACILITATE ACCESS FOR FLOOD MANAGEMENT AND MAINTENANCE VEHICLES – Granted on 14/12/2010

5.0 CONSULTATION

5.1.1 Site notice posted, neighbouring properties notified. The planning application was publicised by means of site and press notices, together with individual letters to **224** nearby addresses sent on 10th August 2020. The Council received **16** objection letters from neighbouring residents and local amenity groups. **1** letter of support was received from a local amenity leisure facility and **1** letter of support was received from a neighbouring resident

5.1.2 Furthermore, a total of 45 site notices were put up in the surrounding area, comprising of, General Site Notices, Conservation Area Site Notices and Departure Site Notices.

5.1.3 N.B. The majority of representations have focused on concerns relating to removal of the wall along the boundary of Ravensbury Park and removal of trees to facilitate the provision of new cycle and pedestrian access and the provisions of shared cycle and pedestrian access. The objections received are summarised below;

- The bridge, as designed, is not wide enough to fulfil its active travel purpose or the needs of a 'climate emergency' world extra space needed for a proper southbound cycle lane and a safe pedestrian pavement. recommend that negotiations with the National Trust are re-opened immediately
- Does not meet current recommended standards for cycling provision on main roads and on shared use pavements
- The proposed scheme does not promote active travel
- Objections note the climate change emergency and that we need more active and sustainable ways of travelling
- Potential conflict between pedestrians and cyclists at this location
- No adequate facilities for cyclists
- Do not support the demolition of the wall and the historic brick wall protecting Ravensbury Park and the Wandle Trail. Support the environmental and other benefits of a single span structure.
- Preference would be for the road never to open to traffic again
- Loss of trees and wall will increase; visual, noise and vehicle pollution.
- Loses 40 sq.m of Metropolitan Open Land for Highways.
- Misnaming the bridge
- Lacks evidence that the loss of 12 valued trees will be compensated by new planting
- Fails to take advantage of the opportunity to promote the historic significance of the crossing through use of decorative railings. Fails to confirm use of the Wandle Valley Regional Park visual identity in all signage and interpretation. Incorrect information in the arboriculture report not stating the correct site description, TPO or Conservation Area designations
- important that this screen should be retained, which visually protects the park from views of traffic, its noise and pollution
- Capital Asset Valuation of Amenity Trees (CAVAT) study not submitted

- All woodland and parkland is made up of trees of varying appearance and natural development and contribute to the overall ecology of the area
- impact on the ecology of that corner of the park
- Trees, the wall and undergrowth provide a habitats for a wide range of species and biodiversity
- Removing the wall will expose Watermeads residents in houses and flats to increased traffic noise, and pollution. The green area is considered by residents to be part of the estate, and probably is. Current wall generates a sense of seclusion and therefore safety
- There is already a well maintained, and well used, public footpath that runs adjacent to the current bridge and crosses the river via its own dedicated bridge. This is used by cyclists as well as pedestrians and has low risks in terms of safety
- Whilst I appreciate that you do need to put some safety measures in for cyclists, it would be nice to see some for pedestrians too

Support

- Support the new bridge, we feel it is very well designed and looking forward to seeing in use
- Support the reinstatement of the bridge on Bishopsford Road (MitchamBridge). I recognise that the current plans are considerably better than previous plans

5.1.4 **Internal consultees:**

5.1.5 LBM Flood Risk Officer

5.1.6 No objections to the proposed application subject to conditions relating Construction Method Statement which includes but is not limited to, flood flow conveyance during construction works, river sediment. Management, measures to manage higher flood flows in heavy rainfall for in and out of hours including any emergency contact.

5.1.7 LBM Green infrastructure (Ecology/Biodiversity)

5.1.8 The methodologies and findings set out in the three reports; Preliminary Ecological Appraisal, Nocturnal bat survey and Preliminary Ecological Appraisal. Provided the recommendations, mitigation and enhancements are included as part of the proposed development works / conditioned appropriately, the reports conclude there should be no adverse effects on the habitats or species within and around the site

5.1.9 LBM Greenspaces

5.1.10 Object to the removal of the wall along boundary of Ravensbury Park, object to the removal of trees behind the wall and object to the applicants applying British Standards BS5837:2012 for the assessment on the impact of development on trees. The officer contends that a CAVAT assessment should have been applied instead.

5.1.11 LBM Tree Officer

5.1.12 No objections raised, However, I do consider it important to ensure the removed trees are replaced, something that is recognised by in the arboriculture report.

5.1.13 LBM Urban Design

5.1.14 Broadly agree with the proposed scheme and has referred officers to the design review panel comments.

5.1.15 LBM Conservation and Design

5.1.16 Satisfied with the design of the bridge. It is of simple cantilever design of a single span and looks well enough within the landscape. It is good to open up views of the park by replacing the wall with railings. The railings will still mark the historic boundary of the open land along this old route down into Mitcham.

5.1.17 LBM Highways

5.1.18 H10 is a condition put on the approval, for Construction shall not commence until a working method statement has been submitted to and approved in writing by the planning department

5.1.19 H13 is a condition again put on by planning for a construction logistics plan to be submitted and approved in writing by the planning department

5.1.20 INF9 is information when Highways must be contacted to ensure all relevant licenses are in place prior to starting works

5.1.21 INF12 is information when any works that could affect the public highway shall be coordinated under the requirements of the new roads and street works act and the traffic management act and licensed accordingly and liaised with the borough of Merton's network coordinator.

5.1.22 All above apply to this site

5.1.23 No objections to the proposed application subject to above conditions and informatives.

5.1.24 LBM Transport

5.1.25 No objections raised

5.1.26 LBM Environmental Health

5.1.27 No objections raised

5.1.28 Design Review Panel

5.1.29 The design has moved from a three arched bridge to a single span. The three arches have been articulated in the parapet via the 3 pillars on the western side, while no such articulation existed on the eastern side. The new railing appear to be an improvement on the western side over the existing structure, however the design makes no attempt to further articulate the landing of the bridge and its abutments. It simply merges with the simple greenery and fairly crude abutments. In the context of a conservation area, it appears to be a missed opportunity to articulate – and celebrate – the landing of the structure either side and it's integration with the landscape and start of parapets, in particular after the loss of the arches and pillars on the eastern side.

VERDICT - AMBER

5.1.30 External consultees:

5.1.31 Environment Agency

5.1.32 No objections raised, Flood risk model acceptable, subject to Compliance condition to be added for the provision of an Otter ledge.

5.1.33 Met Police Crime Prevention Officer

5.1.34 No concerns raised - Supports the removal of the wall separating Ravensbury Park and London Road A217 and replacing with railings, this would allow for greater visibility along the pedestrian footpath so to reduce the chance of crime, fear of crime and avoidance of the area.

5.1.35 Transport for London

5.1.36 No objections received in support of the proposed scheme.

5.1.37 Historic England (Greater London Archeological Advisory Service)

5.1.38 No objections to the proposed application subject to conditions and informatives.

5.1.39 Historic England (Development Management)

5.1.40 No comments and no objections raised

5.1.41 Natural England

5.1.42 No comments and no objections raised

5.1.43 Met Policy (Traffic)

5.1.44 No objections raised, in support of the proposed application.

5.1.45 London Buses

5.1.46 No comments received

5.1.47 National Trust

5.1.48 No comments received

5.1.49 SGN UK Gas Distribution

5.1.50 No comments received

5.1.51 UK Power Networks

5.1.52 No comments received

5.1.53 Thames Water

5.1.54 No comments received

5.1.55 BT Open Reach

5.1.56 No comments received

7.0 POLICY CONTEXT

7.1 National Planning Policy Framework (2019)

7.1.1 Specific policy areas considered directly relevant are as follows:

- Promoting healthy and Safe communities;
- Promoting sustainable transport;
- Achieving Well-Designed Places;
- Meeting the challenge of climate change, flooding and coastal change;
- Conserving and enhancing the natural environment
- Conserving and enhancing the historic environment

7.1.1 Planning policy. **London Plan (2015/16).**

7.1.2 The London Plan (2016) is the overall strategic plan for London, and it sets out a fully integrated economic, environmental, transport and social framework for the development of the capital for the next 20-25 years.

7.1.3 The London Plan was published on 14th March 2016. The policies relevant to this application are:

- Policy 2.3: Growth areas and co-ordination corridors
- Policy 2.6 Green Infrastructure: the multi-functional network of green and open spaces
- Policy 5.2: Flood risk management
- Policy 6.1: Transport
- Policy 7.5: Public Realm
- Policy 7.8: Heritage Assets and Archaeology
- Policy 7.9: Heritage Led Regeneration
- Policy 7.17: Metropolitan Open Land
- Policy 7.19: Biodiversity and access to nature

7.2 **The draft New London Plan (2019/20)**

- Policy HC1: Heritage Conservation and growth
- Policy G6: Biodiversity
- Policy SI12: Flood Risk Management

7.3 **London Borough of Merton LDF Core Planning Strategy (2011)**

7.3.1 The relevant policies in the Merton LDF Core Planning Strategy (2011) are:

- Policy CS.5: Wandle Valley
- Policy CS.13: Open space, nature conservation, leisure and culture
- Policy CS.14: Design
- Policy CS.16: Flood risk management
- Policy CS.18 – 20: Active travel

7.4 **London Borough of Merton Site and Policies Plan (2014)**

7.4.1 The relevant policies in the Merton Site and Policies Plan (2014) are:

- Policy DM O1 Open space
- Policy DM O2 Nature conservation, trees, hedges and landscape features
- Policy DM F1 Support for flood management
- Policy DM EP4 Pollutants
- Policy DM D1 Urban design and the public realm
- Policy DM D2 Design considerations in all developments
- Policy DM D4 Managing heritage assets
- Policy DM T1 Support for sustainable transport and active travel
- Policy DM T4Transport infrastructure
- Policy DM T2Transport impacts of development
- Policy DM T5Access to the Road Network

7.5 **OTHER DOCUMENTS AND GUIDANCE**

- Mayors Sustainable Design & Construction SPG (April 2014)
- London Borough of Merton 'Archaeology SPD' (Part 1 & 2) (2004)
- Merton's Strategic Flood Risk Assessment 2019
- The Wandle Valley Conservation Area Character appraisal
- Wandle / Mitcham Archaeological priority area

8.0 **MAIN PLANNING CONSIDERATIONS**

8.1.1 **Key Issues**

8.1.2 The key issues of consideration for this planning application are;

- Principal of development
- Flood Risk
- Design/appearance and heritage
- Heritage
- Trees
- Ecology and Biodiversity
- Transport and Highway

8.1.3 **PRINCIPLE OF DEVELOPMENT**

8.1.4 Bishopsford Bridge provides an important vehicle, pedestrian and cycle crossing point on the A217 from Sutton in the South to Mitcham in the North. Its replacement is therefore essential to maintain such movements. Its location, within the context of the surrounding conservation area with both open spaces and historic building in close proximity and with the additional constraints of flood risk and biodiversity provide the main constraints in terms of the development of its design and the need to ensure the replacement of the former bridge not only perform the vital functional role, addressing flood risk issues, but preserves or enhances the character of the conservation area, promotes high quality inclusive design and safeguards natural habitats and trees.

8.1.5 ***Development on MOL***

8.1.6 Members should note that planning policy for Metropolitan Open Land seeks to protect not just the land itself but the sense of openness. Policy DM.O1 Open Space in Merton's Sites and Policies Plan 2014 states at part (d) Development in proximity to and likely to be conspicuous from MOL or designated open space will only be acceptable if the visual amenities of the MOL or designated open space will not be harmed by reason of siting, materials or design.

8.1.7 This site is also part of the Wandle Valley Regional Park and has similar boundaries to the historic conservation area. The new bridge itself will not be visible from far in the surrounding Metropolitan Open Land or designated open space due to the existing woodland and vegetation in the National Trust Land and the existing wooden footbridge on the western side. In winter months parts of the bridge will be just about visible from the Wandle Trail on the western side but in summer months when vegetation is growing and in leaf it is only visible by leaving the Wandle Trail and walking to the western riverbank in the National Trust Land. Nevertheless, in the places where the new bridge will be visible from the MOL, designated open spaces and Wandle Trail help to demonstrate that the design of the new bridge will be a visual improvement compared to what was visible of the previous structure, particularly from the eastern side.

8.1.8 As such, after carefully consideration of the proposals, officers consider that this development would not have an adverse impact on the MOL and would in fact provide a positive contribution to enhance the visual amenities of the MOL and designated open space, therefore complying with the Sites and Policies Plan (2014) Policy DM.O1

8.6 FLOOD RISK

8.6.1 Core Planning Strategy (2011) Policy CS.16 relates to Flood risk management and Sites and Policies Plan (2014) Policy DM F1 relates to Support for flood risk management. The aim of these policies is to ensure

8.6.2 Policy CS.16 seeks to ensure individual development proposals will have no adverse impact and that essential community infrastructure will be at less risk of damage.

8.6.3 Policy DM F1 Support for flood risk management - Aim to mitigate the impact of flooding in Merton, in line with the National Planning Policy Framework (NPPF 2012) and associated national guidance, the Flood and Water Management Act 2010, Flood Risk Regulations 2009, The Water Framework Directive, the council's duty as Lead Local Flood Authority and Merton's Local Flood Risk Management Strategy.

8.6.4 WSP provided a Flood Risk Assessment (doc no: 70066777). Hydraulic modelling of the baseline scenario (with updated bridge representation) was completed for a range of events from the 1 in 5 to the 1 in 1000 annual probability events. The impacts of climate change were assessed for the 1 in 100 annual probability event for the Higher Central allowance with a 35% increase in peak flows, and the Upper End allowance with a 70% increase in peak flows.

8.6.5 The Bishopsford Road Bridge is surrounded by Flood Zones 2 and 3. Significant flood risk to out of bank areas upstream and downstream of the bridge is indicated. This is predominantly contained within the Watermeads Nature Reserve, Imperial Sports Ground and Ravensbury Park, although also affects residential buildings close to the river as well as residential areas downstream of Ravensbury Park. Some residential properties upstream of the bridge are indicated to be at risk during events as small as the 1 in 5 annual probability event. Many more properties are indicated to be at risk during the 1 in 1000 annual probability event.

8.6.6 The NPPF defines flood risk as a product of the probability (frequency) of flooding and the severity (potential damages, danger and disruption) of the flooding. The findings from assessment by WSP indicates that the frequency of flooding within the surrounding area is not predicted to increase. The assessment has indicated a potential increase in flood depth however this is not predicted to increase potential damage, danger or disruption. Furthermore, the selected 10m span offers significant betterment elsewhere by reducing upstream flood levels, removing properties from areas previously identified to be at risk, and reducing the flood frequency of other areas compared to the baseline scenario. The proposed design is therefore considered to satisfy the requirements of the Exception Test.

8.6.7 **LBM Flood Risk officer**

8.6.8 After reviewing the application, officers have no reservations on flood risk grounds and are satisfied with the flood risk assessment submitted by WSP. The hydraulic model carried out demonstrated that there will be further reductions to flood risk to some properties for the 100year plus 35% climate change event, for the 10.m span bridge. These are clearly show on the Baseline Flood Extent and Proposed Flood Extents maps. With this in mind officers recommend approval subject to condition relating to detailed Construction Method Statement which includes but is not limited to, flood flow conveyance during construction works, river sediment management, and measures to manage higher flood flows in heavy rainfall for in and out of hours including any emergency contact.

8.6.9 **Environment Agency Response**

8.6.10 The Environment Agency (E.A) have reviewed the information submitted and have no objection to the proposed development. The Preliminary Ecological Appraisal by WSP (Project no. 70066777, Date: April 2020) states that if feasible, it is recommended that an otter ledge is incorporated in the new bridge design. However the updated environmental assessment by CGO Ecology Ltd (Version 4, 18th July 2020) concludes no mitigation or enhancement is required for otters. The E.A have agreed with the initial recommendation of the WSP report and understand following further discussion with the applicant that prevision of a ledge will be possible. The E.A have therefore requested a compliance condition for the provision of an Otter Ledge to be provided on the left hand bank.

8.7 **DESIGN/APPEARANCE AND HERITAGE**

8.7.1 Sites and Policies Plan (2014) Policy DM D2 'Design considerations in all developments' seeks to ensure high quality design and protection of amenity within the borough. Core Planning Strategy (2011) Policy CS14 'Design' recognises the importance of design and states that all development needs to be designed in order to respect reinforce and enhance the local character of the area in which it is located and to contribute to Merton's sense of place and identity. These policies are also supported by Core Planning Strategy policies CS13 'Open Space', 'Nature conservation', 'Leisure and Culture'.

8.7.2 The proposed new bridge design sees the integration of the east side footbridge, which was separate from the main river crossing as an extension to the side. The partially collapsed bridge was unsafe, the appearance of the bridge had been compromised as a result of designs, which was in very poor condition. The design of the new bridge makes significant reference to the design of the old and makes use of various aspects of the original design to create a new pedestrian and cyclist-friendly bridge, in this sense this allows for improved appreciation of the River Wandle and Nature Reserve. Members should note that this proposal sufficiently accords with the NPPF paragraphs 185 and 192 and Draft London Plan Policy HC1, which emphasises the importance on drawing upon the positive contributions heritage assets can make to sustainable communities and places. The NPPF states that proposals should seek identify opportunities for enhancement area.

- 8.7.3 It is regarded that the design of the new bridge, with railings to either side would provide a more visually enhancing appearance, improving the character and appearance of the wider street scene and the conservation area as a whole. The proposal also recognises the importance of enhancing and improving visual connection with the wider surrounding, which would allow improved appearance from the Watermeads and from the Grove Mill listed buildings.
- 8.7.4 Officers have considered the impact arising from the removal of the wall and assessed this on balance taking on board the positive contribution that this would provide. Officers have reviewed objections received for the removal of the wall. It is considered that the proposed replacement of the wall with railings would provide an improved appearance of the bridge that will create a more open and inclusive feel, and consider this to be more acceptable form of design and appearance that would also enhance the appreciation of surrounding heritage assets as discussed in detail below. This approach would be policy compliant as it would provide a more visually accessible and enhanced design in accordance with policy DM D2 and CS14. The other benefits that the scheme would bring by widening the bridge is the incorporation of improved pedestrian and cycle access, in doing so, the scheme will create significant improvements to provide inclusive access for all in accordance with Policy CS13 and CS14 of the Core Planning Strategy (2011).
- 8.7.5 The proposal in its design and appearance would not have a detrimental impact on the character and appearance of the street scene or the Wandle Valley Conservation Area.
- 8.7.6 **Heritage**
- 8.7.7 Bishopsford Road Bridge lies within the Wandle Valley Conservation Area. Sub areas 5 and 6 are the most relevant to Bishopsford Bridge. The characteristics of the conservation area at this point include the forms of enclosure: the river itself, boundary walls, fences, metal railings and gates that typify the conservation area.
- 8.7.8 The significance of a heritage asset is defined in the National Planning Policy Framework (NPPF) as being made up of four main constituents: architectural interest, historical interest, archaeological interest and artistic interest. The assessments of heritage significance and impact are normally made with primary reference to the four main elements of significance identified in the NPPF.
- 8.7.9 Paragraphs 193 and 194 of the NPPF state that great weight should be given to the conservation of a designated heritage asset when considering applications that affect its significance, irrespective of how substantial or otherwise that harm might be.
- 8.7.10 In terms of Local Policy context, the following policy considerations include, but are not limited to: Merton's Sites and Policies Plan 2014: Policy DM D4 Managing Heritage Assets; Merton's Core Planning Strategy 2011-2026: Policies CS5 (Wandle Valley) and CS14 (Design) are of particular relevance and make reference to protecting and enhancing the River Wandle, archaeological sites, conservation areas and the desire to raise awareness of the area's heritage. These policies acknowledge Merton's 'rich architectural heritage and diverse built environment'.

- 8.7.11 In regards to London Plan 2016: Policy 7.8 Heritage Assets and Archaeology is relevant to this proposed application and the forthcoming London Plan 2019 intent to publish version: Policy HC1 heritage conservation and growth would be relevant.
- 8.7.12 The main buildings of significance to this part of the conservation area are; the statutory listed buildings around and including the former Mitcham Station, Wandle House, the Grove Mill and former mill workers cottages to the south of Grove Mill and the 'Watermeads' Nature reserve. There are some remnants of the former Surrey Brewery which are also of historic significance.
- 8.7.13 The historical records relating to Bishopsford Bridge show that this location has been a fording point since the middle ages. The three arch Bishopsford Bridge was built in the 18th century and survey records exist from 1882. The upstream footbridge was built over the former ford circa 1947, attached to the highways bridge and wholly changing the view of the bridge from upstream. The deck of the upstream footbridge was completely replaced in 2010 along with piling work to the north and south abutments. In 2010 a wooden footbridge was built downstream about 6 metres from Bishopsford Bridge in Watermeads Park. There is a metal Parish Boundary Marker in the Western parapet bearing the date 1882, it is important to note that this important historic fabric will be reinstated on the new bridge at a mid-river point. Whilst, the existing bridge does retain some 19th century fabric most of this has been damaged and is of no particular architectural interest. Alterations have further compromised the overall architectural integrity of the structure. The location, rather than the bridge itself, is of most local historic interest and is a locally important position insofar as it marked historic parish boundaries on an significant route north-south.
- 8.7.14 NPPF Section 16 and the Draft London Plan (2017 Policy HC1) recognise the positive contribution of heritage assets of all kinds and make the conservation of archaeological interest a material planning consideration. NPPF paragraph 189 says applicants should provide an archaeological assessment if their development could affect a heritage asset of archaeological interest. If you grant planning consent, paragraph 199 of the NPPF says that applicants should record the significance of any heritage assets that the development harms. Applicants should also improve knowledge of assets and make this public.

Applicants Heritage assessment (in accordance with paragraph 199 of the NPPF)

- 8.7.15 The applicants have provided a heritage assessment that was submitted with the planning application. The assessment evaluates and records the significance of any heritage assets that may be harmed by the proposed replacement Bridge and replacement of wall with railings.
- 8.7.16 The report methodically evaluates the significance of heritage assets, of particular relevance is Sub-Area 6: Lower Mitcham – Watermeads and Station, within which the Site is located. The Appraisal notes that there was settlement and industry in the area before the Roman period. Sub-Area 6 is divided into 2 parts. The first being the area around Mitcham Station, including development of the railway and the Surrey Iron railway, and the second being the area around Grove Mill, and the location of the Site, which was historically the focus for a great deal of water-based industrial

activity. A feature of the conservation area is Watermeads, managed by the National Trust and appreciable from the Site.

8.7.17 In summary, the applicant's appraisal identifies a couple of designated heritage assets close to the Site. The observations made on site have confirmed these heritage assets could largely be scoped out of the assessment and would not be subject to any harm or notable impacts due to the proposed bridge reconstruction/replacement. Wandle House (historically known as Wandle Grove) is considered first, insofar as glimpses of it from the bridge and towards the bridge from the listed building are possible, when the foliage on the trees along the river are absent or much reduced during the Autumn/Winter months. Mill Cottages are further to the east and front onto the riverside. At present any downstream views along the river (i.e. westwards) take in unattractive view of the extension footbridge over the ford. In terms of assessment of impact on the setting of these nearby heritage assets the report identifies the following, please see below a short summary of findings;

8.7.18 *Wandle House, Grade II*

8.7.19 Wandle House is set back from the River Wandle to the north but there is some limited indivisibility with it. The Site will not be apparent in any key views of the Wandle House, which is best appreciated from within its immediate environs. The bridge will not be in competition with or distract from the listed building. The bridge has relatively low sides and allows for open views along the River Wandle, which will be maintained. The material and design will be traditional and appropriate with brick columns and iron railings. In terms of the inter-visibility of the bridge with listed building, this will be subject to seasonal change. Although visibility is limited there will be glimpses in winter months when the trees along the riverside have thinned out. That is apparent as existing, so there will be no change to the existing conditions through the bridge replacement.

8.7.20 The appraisal concludes that no harm to the significance of Wandle House will come about due to the introduction of the proposed new bridge, which has an overall positive effect on the character and appearance of the conservation area (within which Wandle House is located).

8.7.21 *Mill Cottages, Grade II*

8.7.22 The bridge will not be seen in any key views towards Mill Cottages. The appraisal concludes that the proposed new bridge will not result in any harm to the significance of Mill Cottages. The observations suggest that there would in fact be an enhancement within their wider setting, as views of the bridge from Mill Cottages will also be improved, allowing for greater appreciation of the river. This is considered an improvement within the setting of these cottages.

8.7.23 Assessment on replacement of wall with railings along boundary of Ravensbury Park

8.7.24 The appraisal assesses this replacement of wall with railings along boundary of Ravensbury Park. In order to add the cycle lane along the bridge, which was added to the proposal following public consultation, the bridge has been slightly realigned and also widened on the west side by 1m. As a result of the modified road alignment

to allow for improved provision of access for cyclists and pedestrians, the high brick wall running along the Ravensbury Park east side boundary, away from the bridge, is to be removed. The wall will be replaced with full height railings 1m closer to the trees. The existing wall is approximately 6ft in height and prevents views into the park, whilst also enclosing one side of the road leading to the bridge. It comprises brickwork of varying date, some 19th century but much of it has been rebuilt. This arrangement encourages pedestrians to edge near to the road and is not ideal.

8.7.25 The appraisal identifies that the replacement of the Ravensbury Park boundary wall would bring about a number of positive changes. Currently the existing high wall encloses the pavement and pushes pedestrians close to the roadside. Removing the wall and replacing it with full height railings, slightly set back further towards the trees, will create a more open and inviting feel, giving pedestrians more space and allowing for better appreciation of the open green landscape and providing much needed natural surveillance. In terms of design the new replacement railings along the boundary of Ravensbury Park will tie in visually to the design of the new bridge. The condition of the brick wall is regarded to be in very poor state and dangerous for passing pedestrians, particularly in moments of high wind with potential tree fall. The heritage assessment notes that the bricks themselves are of no particular interest and of varying date, but the marker as historic boundary of the open land along this old route within the context of the bridge and the conservation area, has been considered and acknowledged.

8.7.26 **Historic England (Greater London Archaeological Advisory Service)**

8.7.27 The site is located in a tier 2 archaeological priority area which covers an area of historic mills associated with the River Wandle. Mills were located to the east of the bridge, but none are known from the immediate vicinity.

8.7.28 Very little historic fabric of the bridge survives, and it is in a bad state of repair and not felt worthy of archaeological recording. It is noted that the historic boundary marker will be saved and re-sited on the new bridge, which is very positive from a heritage perspective and will provide continuation of the site as a the historic crossing point of the Wandle.

8.7.29 In terms of below-ground archaeology, an archaeological watching brief should be conducted on the areas where ground works are required for new piles or other works associated with the replacement of the bridge. This would ensure that no remains of unknown mills or evidence for the historic ford were destroyed without first being recorded.

8.7.30 I have looked at this proposal and at the Greater London Historic Environment Record. I advise that the development could cause harm to archaeological remains. However the significance of the asset and scale of harm to it is such that the effect can be managed using a planning condition.

8.7.31 Historic England have therefore recommended a condition on any consent prior to commencement of development to ensure that a written scheme of investigation (WSI) for site investigation is submitted and agreed by the Local Planning Authority.

8.7.32 *Historic England (Listed Buildings)*

8.7.33 No objections raised to this proposed application, H.E officers have stated that they do not wish to offer any comments. H.E suggest that Merton Council officers seek the views of their specialist conservation advisers.

8.7.34 *LBM Conservation Officer*

8.7.35 The conservation and design officer reviewed and assessed the proposed application for the replacement of the bridge and replacement of the boundary wall with railings. The officer states that the bridge is of simple cantilever design of a single span and looks well enough within the landscape. In terms of the wall, the officer states that it is good to open up views of the park by replacing the wall with railings. The railings will still mark the historic boundary of the open land along this old route down into Mitcham.

8.7.36 *Greater London Historic Environment Record Database*

8.7.37 Greater London Historic Environment Record Database was undertaken on July 2020 for recorded assets and find-spots of Bishopsford Bridge located at grid reference TQ 27171 67843 and including searches within 500m of its setting at grid reference TQ 27173 67841 for the purpose understanding the historical developments of this site and its local setting. The search confirmed that there were no finds of historic importance.

8.7.38 *Planning Officers review*

8.7.39 Planning Officers have looked at the applicant's heritage assessment and reviewed response from Greater London Historic Environment Record Database and comments from Historic England. Officers assessed the impact of the proposal on heritage assets, in line with national and local planning policies.

8.7.40 Site and Policies Plan, Policy DM D4 titled 'Managing heritage assets' seeks to conserve and where appropriate enhance Merton's heritage assets and distinctive character.

8.7.41 Policy CS13 titled 'Open Space, Nature Conservation, Leisure and Culture' further reinforces the need to improve and enhance visual connection with the natural landscape and waterways. The policy encourages new linkages in landscape and visual terms into the river corridor when development opportunities arise. The council recognise the waterways as a natural asset and will ensure these opportunities are enhanced and improved.

8.7.42 In response to objections received relating to provision of signage, officers are satisfied that as part of the proposed works there will be adequate signage as illustrated in the proposed highway layout. Members should also note that Merton Council in its capacity as Highways Authority will enhance the signage relating to the local area, including the Wandle Trail. Merton's Design Review Panel raised that clear signage for cycling must be provided and this will be worked up and installed prior to the bridge reopening. It is also worth pointing out to members that the council

is proposing to hold a local competition to name the bridge, which may lead to appropriate signage.

8.7.43 The proposal would not result in the loss of a structure that makes a positive contribution to a conservation area or heritage site and would not lead to substantial harm on the significance of, or the total loss of heritage assets. In fact the proposal would conserve and enhance Merton's heritage assets and distinctive character. The proposal would also enhance and conserve the setting and significance of the asset of surrounding area and line with the policies noted above.

8.7.44 ***Secured by design***

8.7.45 London Plan policy 7.3 aims to ensure that measures to design out crime are integral to development proposals and are considered early in the design process, taking into account the principles contained in Government guidance on 'Safer Places' and other guidance such as Secured by Design' published by the Police. Development should reduce the opportunities for criminal and anti-social behaviour and contribute to a sense of security without being overbearing or intimidating. Places and buildings should incorporate well-designed security features as appropriate to their location.

8.7.46 The NPPF and London Plan 2016 policies 7.1, 7.3, 7.4 seek to ensure that policies and decisions should aim to create safe and accessible environments where crime and disorder, and the fear of crime, do not undermine quality of life or community cohesion and create safe and accessible developments, containing clear and legible pedestrian routes and high quality public space, which encourages the active and continual use of public areas.

8.7.47 Officers have reviewed objections received in regards to the loss of the wall along Ravensbury Park and also note some comments that state there being low risk levels for safety along the footpath that runs adjacent to the current bridge. Having given due consideration to the details and plans submitted and taken into consideration relevant planning policy including expert advice from the Met police Crime Prevention Design Officer (CPDO), I am satisfied that the design appears to have included Secured by Design and Designing out Crime measures as intrinsic considerations. The following observations have been made by the Met Police in regards to the proposal and taking into consideration the comments above;

8.7.48 "The open rail design is of benefit security wise as it will facilitate natural surveillance to the natural reserve, Ravensbury Park and towards the western pedestrian footbridge. The removal of the wall separating Ravensbury Park and London road A217 and replacing with railings again would allow for greater visibility along the pedestrian footpath to reduce the chance of crime, fear of crime and avoidance of the area. Issues highlighted by the local Safer Neighbourhood Teams at other nearby bridges over the River Wandle are illegal fishing, jumping from the bridge into the river and throwing large items into the river".

8.7.49 Officers are satisfied with the information that has been submitted in support of this application and on the bases that the proposal will improve and enhance security and safety measures for the general public. Secured by design principle notes that it is the fear of crime that has a severe effect on public quality of life, the proposal would substantially reduce the fear of crime and improves natural surveillance and discourage unwanted and anti-social behaviour.

8.7.50 Members should also note officer's observation whilst on site, officers witnessed the unauthorised use of moped vehicles being used along the footpath, this presented significant health and safety risks to pedestrians and cyclists and is seen as serious criminal act. By creating and improving the visual natural surveillance this would discourage these types of anti-social behaviours. Furthermore, the anti-social behaviour acts involving throwing large items into the river, as noted above by the Met Police officer could raise potential flood risk issues and needs to be discouraged as a serious concern.

8.8 TREES

8.8.1 Policy DM.O2 of the Sites and Policies Plan (2014) requires all types of development to protect trees, hedges and other landscape features of amenity value to secure suitable replacement in instances where their loss is justified. Furthermore proposals for new and replacement trees, hedges and landscape features should consist of appropriate species. Core Planning Strategy (2011) Policy CS 13 Open Space, Nature Conservation, Leisure and Culture seeks to protect and enhance the borough's public and private open space network including Metropolitan Open Land parks and other open spaces the policy also recognises the importance of Improving access to open space and nature conservation by public transport, cycle mobility vehicles and on foot.

8.8.2 *Methodology*

8.8.3 CGO Ecology Ltd conducted an arboriculture assessment for the proposed replacement of the Bishopsford Road Bridge. The results from the survey undertaken confirm the following; A BS5837:2012 Tree Survey was conducted on 11th May, 18th May and 6th July 2020, recording the prescribed metrics for all trees that may be affected by the proposals. Trees within the impact zone were recorded, measured, described, and their condition, health, life-expectancy, and other observations were assessed. Tree Preservation Orders (TPOs) and Conservation Areas were researched. Root Protection Areas (RPAs) were calculated following BS5837 methodology, and a Tree Schedule was drafted, presenting metrics and observations. An Arboriculture Impact Assessment (AIA) was conducted, and a Tree Constraints Plan (TCP), Tree Protection Plan (TPP) and Draft Arboriculture Method Statement (AMS) were produced.

8.8.4 *Results*

8.8.5 The site comprises broadleaved woodland, a river, road, and formerly a brick-built bridge. 47 individual trees and two groups of trees were surveyed: 16 sycamore, 10 English elm, seven elder, four hawthorn, four Norway maple, three horse-chestnut, two ash, two false-acacia, two ginkgo, two London plane, one beech, one lime. 18 trees are category B (moderate quality); 18 trees and one group are category C (low quality); 11 trees and one group are category U (unsuitable for retention). The survey revealed that all trees were classified as being of mainly landscape value. Several trees stand out due to their large size (T44-46); many others are young, small or poorly-structured specimens. In particular, the area northwest of the bridge is a dense clutter of low-quality trees and saplings. Of the 36 trees and one group worthy of retention, 12 trees need to be removed to facilitate the development, and nearly all the others will require some management. Several retained trees have

RPA's significantly overlapping the proposed works. A total of 23 trees plus one small group (cluster of young trees) are to be removed

8.8.6 **Recommendations**

8.8.7 The survey goes on to make the following recommendations for the tree protection, mitigation and management, based on the submitted plans (members should refer to the plan accompanying this PAC report); Trees T22, T25-27, T32, T34, T37-40, T42 and G1 are unsuitable for retention and should be removed. Trees T1-2, T4, T11, T23, 24, T28-30, T33, T35 and T43 will need to be removed to facilitate the development. Trees T5-8, T10, T13, T14, T16, T17, T21, T31, T41 and T44-48 will require crown-reduction on some or all sides, to facilitate plant movements and other development activities.

8.8.8 **Compensatory planting**

8.8.9 In order to compensate the loss of 23 trees and one small cluster of young trees, 26 new trees will be planted within the landscaped area to the west of London Road, northwest of the bridge. These will be location-appropriate, standard trees, which will be imposed as condition requiring a schedule list to be provided and agreed firstly. The report arboriculture report states that the species makeup should follow the trees removed to a degree, but should be native species only, and especially avoiding invasives such as false-acacia. Fruit and berry-rich trees such as apple (*Malus pumila*), pear (*Pyrus communis*), wild cherry (*Prunus avium*) and rowan (*Sorbus aucuparia*) should be selected. No new tree or shrub planting should take place within the RPA's of retained trees. **These recommendations comply with Policy DM.O2 which points out a strong preference for native species to be planted.**

8.8.10 No bat roost potential has been observed in trees requiring removal or crown reduction. T15 (unaffected) has low bat roost potential. No nests have been observed in trees to be removed, and only one nest/drey has been identified in a tree to be crown-reduced (T41). The nest/drey will not be affected by the proposed crown-reduction. Hedgehog is likely to forage and seek shelter on site, and fox could potentially inhabit the disused badger set to the northwest of the bridge.

8.8.11 In terms of the Root protection Areas of retained trees, these are treated as high importance in accordance with BS5837:2012, and their protection is a key consideration in the planning and development process. To protect any roots encountered, measures will include careful removal of existing surfaces, air-spade excavation, root pruning where necessary, and cellular confinement systems (CCS) for any level changes and resurfacing. Any root clipping will also follow BS5837 and BS3998:2010 'Tree Work' (BSI, 2010) recommendations.

8.8.12 Greenspaces state that the applicant's assessment of trees under BS 5837 – Trees on Development Sites is not appropriate for the site. The BS is useful in determining where on a development site, the footprint of a building is least damaging to trees and thus categorises trees by their perceived individual quality. Greenspaces consider that a more appropriate assessment of the trees is CAVAT (Community Asset Valuation of Amenity Trees). Greenspaces also state that this is the approach used by many Local Authorities

- 8.8.13 In response to the above, members should note that there are two approaches to undertaking assessments for impact of development on trees and vegetation. Merton Council can only apply consideration to adopted planning policy for the Borough and cannot consider how assessments are conducted in other Local Authorities. Members are kindly requested to refer to Paragraph 5.28, Policy DM.O2 of the Site and Policies Plan (2011) which refers to the British Standards approach in order to protect trees, hedges and other landscape features. ***“The council considers it important that development proposals are accompanied by appropriate reports and surveys to deal with the impact of the proposals on the existing vegetation. When applicable, developers will need to demonstrate that they have paid regard to current British Standards such as, BS 5837:2012 ‘Trees in relation to construction’.***
- 8.8.14 Policy DM.O2 does not require a CAVAT assessment to be carried out, this policy requires applicants to follow British Standards guidelines, BS 5837:2012 ‘Trees in relation to construction’, which the applicants have sufficiently undertaken.
- 8.8.15 **LBM Arboriculture Officer** has reviewed this application and the assessment that has been undertaken for the trees and impact of development. No objections were raised, the officer notes the importance of replacing the removed trees, and confirms that this has been recognised in the arboriculture report.
- 8.8.16 **Planning Officers** are therefore satisfied with the submitted documents, comprising of; Arboriculture Impact Assessment (AIA), Tree Constraints Plan (TCP), Tree Protection Plan (TPP) and Draft Arboriculture Method Statement (AMS). Officers can conclude that the recommendations made in the assessment above are acceptable. Members should also note that a planning condition will be imposed with any such consent to ensure that planning officers and Greenspaces are satisfied with the schedule list of planting type that is to be carried out, in terms of species, planting time, speed of growth and level of foliage for adequate screening particularly for the new replacement railings along Ravensbury Park.

8.5 ECOLOGY AND BIODIVERSITY

- 8.8.17 This part of the assessment looks at the Impact of the proposed replacement bridge on the ecology and biodiversity of the immediate area and the Wandle Valley Conservation Area, Sub areas 5 and 6, which includes the Watermeads. The site has the following environmental planning designations in the Sites and Policies Plan (2014) and Core Planning Strategy (2011);
- Green Corridor – Ravensbury Park (CS13, DM02)
 - MOL – Wandle Valley (CS13, DM01)
 - WVRP Brangwyn Crescent 400m buffer (CS5, CS13, CS para 21.13, DM01)
 - Green Chains (CS13, DM01)
- 8.8.18 The site is also directly adjacent to the following environmental designations;
- Watermeads Open Space (CS13, DM01)
 - River Wandle Riverside Walk Open Space (CS13, DM01)
 - Imperial Club Sports Ground Open Space (CS13, DM01)
 - The Upper River Wandle SINC (CS13, DM02)

- WVRP Watermeads (CS5, CS13, CS para 21.13, DM01)

8.8.19 The applicant has submitted a number of ecological reports, including: Preliminary Ecological Appraisal report by WSP (dated April 2020), Nocturnal bat survey report by CGO (dated May 2020) and a Preliminary Ecological Appraisal report by CGO (dated July 2020). Officers have reviewed these documents and assessed these against the relevant planning policies noted above. The methodologies and findings set out in the three reports above are appropriate.

8.8.20 The ecologist recommendations and mitigation cover the following matters: - Pollution prevention. - Protected species and other species and habitats including bats, birds, amphibians, reptiles, otters, invasive species and aquatic ecology. - Minimise any effects on the SINC. - Tree removal and replanting. - Ecological enhancements.

8.8.21 The following Ecological Enhancements would be carried out as part of the proposal;

- Five bat boxes (e.g. Schwegler 2F) to be installed in suitable trees nearby.
- Five bird boxes (e.g. Schwegler 1B) to be installed on suitable trees nearby.
- One Hedgehog home to be installed in a suitable undisturbed area.
- Opt for the proposed single-span bridge design, as this will bring intrinsic improvements to channel flow and bed characteristics, increasing the extent of gravel bed available for spawning fish.
- Removal of the invasive plants (floating pennywort, Himalayan balsam) from the Watermeads reserve would be a significant benefit to this stretch of the River Wandle.
- Decanalisation and tree removal in the Watermeads stretch of the River Wandle would provide an opportunity for reintroduction or natural recolonization of water voles.
- Creation of deadwood features such as log-piles in the woodland areas around the site.

8.8.22 LBM officers have reviewed the submitted documents and are satisfied with the proposal provided the recommendations, mitigation and enhancements are included as part of the proposed development works / conditioned appropriately. The reports conclude there should be no adverse effects on the habitats or species within and around the site, or the nature conservation values of the SINC. Net biodiversity gain should be realised through the replanting of appropriate trees to replace those lost on site and the ecological enhancements listed below. Furthermore, any other application for removal of trees within the application site will be required to submit further bat survey, a condition be attached to ensure applicants are fully aware of this.

8.9 TRANSPORT AND HIGHWAYS

8.9.1 Sites and Policies Plan (2014) Policy DM T2 Transport impacts of development states that Planning permission will be granted for development proposals provided they do not adversely impact on the road or public transport networks, safety or congestion particularly on strategically important routes. The policy seeks to encourage sustainable transport and active travel that have a positive impact on improving travel journeys, patterns and promotes greater use of travel by modes other than the car. This policy approach can lead to an improvement in health and

well-being from increased levels of physical active travel and help reduce pollution as a result.

- 8.9.2 Merton's Core Planning Strategy Policies CS18 to 20 seeks to ensure that transport implications are assessed and managed to ensure that transport infrastructure is provided where most needed and integrated across the borough and its borders. Policy CS13 also recognises the importance of improving access to open space and nature conservation by public transport, cycle, mobility vehicles and on foot;
- 8.9.3 The new bridge shall improve transport connections and better serve all transport types and pedestrians access across the River Wandle on the A217 and the Wandle Trail. The proposal would accord with Policy DM T2 and CS18 as this will provide improved modes of transport for and promote sustainable modes of transport. The proposal would provide cycle routes with a dedicated 1.5m cycle lane on the western side (running from south to north) and a shared 3m space for pedestrians and cyclists on the eastern side (running north to south). The new bridge will restore a crossing point for pedestrians, including those wishing to visit the National Trust Lands or Tooting and Mitcham Hub to the south.
- 8.9.4 The proposal would also provide adequate signage for the shared cycle and pedestrian access. There will be improved access, permeability and general visual improvement to ensure people can feel safe when commuting over the bridge either on foot or cycle particularly in the winter months when light falls early.
- 8.9.5 Objections have been noted stating that the proposed would result in increased traffic noise, and pollution. The green area is considered by residents to be part of the estate, and probably is. Current wall generates a sense of seclusion and therefore safety. In response to this, Officers state that the proposal would replace the historic boundary marker along Ravensbury Park with railings, the proposal would also comprise further tree planting along this boundary behind the railings, with suitable tree planting to create screening and security, whilst also maintaining adequate levels of natural surveillance. Members should note that a condition has also been imposed with any such consent to ensure the applicants provide a schedule list of planting type, size, and species. Officers will be working closely with LBM Greenspaces to ensure the appropriate submitted schedule planting list is acceptable in order to create good foliage and mitigation for noise and air pollution. It should also be noted that LBM Environmental Health Officers had been consulted and raised no objections on the grounds of noise, disturbance and air pollution.
- 8.9.6 Core Planning Strategy Policy CS13 titled 'Open Space, Nature Conservation, Leisure and Culture' also further reinforces this by stating that proposals should seek to Improve public access to and enhance our waterways, including the River Wandle and its banks, for leisure and recreational use while protecting its biodiversity value. This policy expects new development within the area of the Wandle Valley Regional Park, where appropriate, to incorporate physical, visual and landscape connections that will encourage pedestrian and cycle accessibility and enhance the attractiveness of the park.
- 8.9.7 ***LBM Transport and Highways***
- 8.9.8 Have raised no objections for the proposed layout in terms of carriage width, layout for cycle and pedestrian access and overall sustainable transport. Officers have

reviewed the submitted documents/plans and have accepted the proposal in its design would accord with the relevant policies noted above as the proposal would enhance and improve sustainable modes of transport. Two planning conditions have been recommended by officers and will be imposed with any such consent. These conditions relate to construction working method statement and a construction logistics plan to be submitted.

8.9.9 **Transport for London**

8.9.10 In terms of the proposed design/layout, TfL have no objections concerning the proposed replacement bridge.

8.9.11 Officers have also reviewed the Healthy Streets for London document, an initiative that was launched in February 2017 to support the Mayor's Transport Strategy.

8.9.12 **Healthy Streets for London** sets out how the Mayor and TfL will help Londoners to use their cars less and walk, cycle and use public transport more. It outlines some practical steps to achieve this, including:

- improving local environments by providing more space for walking and cycling, and better public spaces where people can interact
- prioritising better and more affordable public transport and safer and more appealing routes for walking and cycling
- planning new developments so people can walk or cycle to local shops, schools and workplaces, and have good public transport links for longer journeys

8.9.13 This approach recognises the importance of promoting both cycling and walking in equal measures. Officers note the objections received by local amenity groups for the provision of a shared surface and instead request another segregated cycle lane on the eastern side of the bridge.

8.9.14 Members should consider the guidance note from **DfT Local Transport Note 01/20** which states that "*A fully shared surface is preferable to creating sub-standard widths for both pedestrians and cyclists where the available width is 3.0m or less.* Although the DfT guidance 01/20 discourages shared use routes, it also makes clear that in certain circumstances it is possible to use them and makes it clear that a shared use route is better provision than not providing anything for cyclists.

8.9.15 Members should also note that the shared surface comprise of 3m wide and would provide equal space for pedestrians and cyclists on the eastern side (running north to south) and therefore follows the guidance note in the DfT Local Transport Note 01/20. The proposal would provide sustainable forms of transport that is acceptable in provision and safety, and has considered the overall objectives and importance of tackling climate change. **Members should note that the separate cycle provision and shared cycle/pedestrian access was added later into the proposal as a result of public consultation and the objectives of which are to encourage and promote active travel and reduce the dependency on cars.**

- 8.9.16 Whilst it would have been a welcomed aspiration to provide segregated cycle lanes on the eastern side as well as the western side, it is worth pointing out to members that these design changes would mean that equal access for pedestrians and cyclists would be severely compromised and therefore failing short to comply with Healthy Streets for London guidance. The other issues arising from segregated cycle lanes created on both sides of the road would result in significant alteration to road configurations, requiring further road alignment that would cause detrimental impact on the surrounding road network and the River Wandle and could potentially create risk flood risk concerns.
- 8.9.17 Officers also note objections received by residents to ensure equal safe access is provided for pedestrians as well as cyclist. Therefore in on order to achieve this, the proposal will provide a shared layout with appropriate width in line with guidance. Furthermore, adequate levels of signage for pedestrians and cyclists will be provided to ensure safety.
- 8.9.18 In response to objections received for the potential further widening of the bridge on the National Trust Land, this would result in significant impact on the natural landscape of the Watermeads to the detriment of the surrounding ecology and biodiversity. This would therefore be contrary Core Planning Strategy Policies (2011); CS13, and Sites and Policies Plan (2014) Policies; DM.O1 and DM.O2.
- 8.9.19 After carefully reviewing the Planning, design and access statement and the proposed Highway Layout submitted with the planning application and taking into consideration the objections received, officers feel satisfied with the proposed plans and agree with the technical expert's advice, received from LBM Transport and Highways and Transport for London. The proposed application is therefore in accordance with relevant adopted policies and is recommended for approval.

9.0 **SUSTAINABILITY AND ENVIRONMENTAL IMPACT ASSESSMENT REQUIREMENTS**

- 9.1.1 The proposal is for replacement of bridge to reconnect Bishopsford Road to London Road.
- 9.1.2 The application does not constitute Schedule 1 or Schedule 2 development. Accordingly, there are no requirements in terms on EIA submission.
- 9.1.3 WSP Consultants had prepared Project Water Framework Directive (WFD) Screening & Scoping Assessment (Project No.: 70066777) on July 2020. Officers have reviewed the assessment that has been undertaken in accordance with the objectives of the Water Framework Directive (2000/06/EC) to identify if the proposed works may affect the status of identified water features or their supporting attributes and indicators, in order to determine if a full WFD Assessment is required.
- 9.1.4 The assessment identified that there were no impacts to biological, hydro-morphological and chemical quality elements during operation of the proposed scheme. Furthermore, the assessment identified no impact to the priority mitigation

measures as set out in the River Wandle Catchment Plan. The report concluded that a full WFD Assessment was not deemed to be required.

10.0 Conclusion

10.1.1 Overall, it is concluded that the proposed development would be in accordance with the aims and objectives of the Sites and Policies Plan, Core Planning Strategy, the NPPF and the London Plan, and would be suitable to the site and the surrounding area. Officers consider that there are no adverse impacts that would significantly and demonstrably outweigh the benefits of the scheme, when assessed against the policies in the NPPF taken as a whole.

10.1.2 The proposal would provide an enhanced and improved replacement bridge and create a positive contribution to the general visual aesthetics of the heritage assets, whilst maintaining and preserving the natural ecology and biodiversity of the area. The proposal would also comply with Environment Agency requirements as the proposal would result in further reductions to flood risk. The proposed scheme would also maintain the vitality of the site and its contribution to the local area. Therefore this proposed application would be in line with national and local planning policy. Accordingly, it is recommended that planning permission be granted, subject to conditions.

RECOMMENDATION

GRANT PLANNING PERMISSION

Subject to the following conditions: -

CONDITIONS

COMPLIANCE: (Time limit for implementation)

1. The development hereby authorised must be begun not later than the expiration of 3 years from the date of this permission, failing which the permission shall be of no effect.

REASON: This condition is imposed by virtue of the provisions of the Planning & Compulsory Purchase Act 2004 and to prevent the accumulation of unimplemented planning permissions.

COMPLIANCE: Development in accordance with approved drawings and documents.

Approved Plans

2. The approved plans comprise the following drawing numbers:

- 70066777-200-PLA-200 Rev P07
- 70066777-200-PLA-201 Rev P04

- 70066777-200-PLA-202 Rev P05
- 70066777-200-PLA-203 Rev P05
- 70066777-200-PLA-204 Rev P02

REASON: For the avoidance of doubt and in the interests of proper planning.

ENVIRONMENT AGENCY (OTTERS LEDGE)

3. The proposed development will only be acceptable if a mammal ledge is included on the left hand bank at the same height as the proposed wing walls. The ledge shall be set at least 600mm below the soffit of the bridge and be at least 300mm in width.

Reason: This is to mitigate the potential impact on otters, by providing a route for otters (a protected species) and other mammals to navigate the bridge and avoid crossing the road which could lead to road death.

FLOOD RISK

4. Prior to the commencement of the development, the applicant shall submit a detailed Construction Method Statement which includes but is not limited to, flood flow conveyance during construction works, river sediment management, measures to manage higher flood flows in heavy rainfall for in and out of hours including any emergency contact.

Reason: to ensure that flood risk is not increased during construction works.

ECOLOGY AND BIODIVERSITY

5. The Proposed Ecological recommendations and mitigation measures should include the features described in the following submitted documents: Preliminary Ecological Appraisal report by WSP (dated April 2020), Nocturnal bat survey report by CGO (dated May 2020) and a Preliminary Ecological Appraisal report by CGO (dated July 2020). These shall be maintained in accordance with the approved plans and documents for the lifetime of the development.

Reason: To ensure that the proposed development would not have an adverse impact on ecology and biodiversity within the area in accordance with policy CS13 of Core Planning Strategy (2011) and Policy DM.O2 of the Sites and Policies Plan (2014).

LANDSCAPING AND PLANTING SCHEME

6. Prior to the bridge becoming operational full details of the proposed approach to the landscaping, including planting plans, a schedule list of plants, including plant sizes/type, proposed numbers, location of the proposed plants and a timetable of implementation for planting. The details shall also include hard landscape materials and boundary treatments. These shall be submitted to and approved in writing by the

Local Planning Authority. The development shall be carried out in accordance with the agreed plans prior to the bridge becoming operational and thereafter retained and maintained.

REASON: To ensure adequate provision of open space within the development, in accordance with Policy 7.5 of the London Plan (2016), Policies DM O1, DM D1 & DM D2 of the SPP Local Plan (2014), Policies CS2, CS5, CS13 & CS14 of the Core Planning Strategy (2011).

HISTORIC ENGLAND (GLASS) WSI INVESTIGATION

7. No demolition or development shall take place until a written scheme of investigation (WSI) has been submitted to and approved by the local planning authority in writing. For land that is included within the WSI, no demolition or development shall take place other than in accordance with the agreed WSI, which shall include the statement of significance and research objectives, and
 - A. The programme and methodology of site investigation and recording and the nomination of a competent person(s) or organisation to undertake the agreed works.
 - B. Where appropriate, details of a programme for delivering related positive public benefits.
 - C. The programme for post-investigation assessment and subsequent analysis, publication & dissemination and deposition of resulting material. This part of the condition shall not be discharged until these elements have been fulfilled in accordance with the programme set out in the WSI.

Reason: In the interests of archaeological protection in accordance with Policies DM.D2 and DM.D4 of the Sites and Policies Plan (2014), Policies CS2 and CS14 of the Core Planning Strategy (2011).

HIGHWAYS

8. H10 Construction Vehicles, Washdown Facilities etc
9. H13 Construction Logistics Plan

Informatives

Environment Agency

1. Please be aware the replacement bridge will also require permission from us under the terms of the Environmental Permitting Regulations. For further information please visit to: <https://www.gov.uk/guidance/flood-risk-activities-environmental-permits#bespoke-permits>

Historic England

2. The written scheme of investigation will need to be prepared and implemented by a suitably qualified professionally accredited archaeological practice in accordance with Historic England's Guidelines for Archaeological Projects in Greater London.

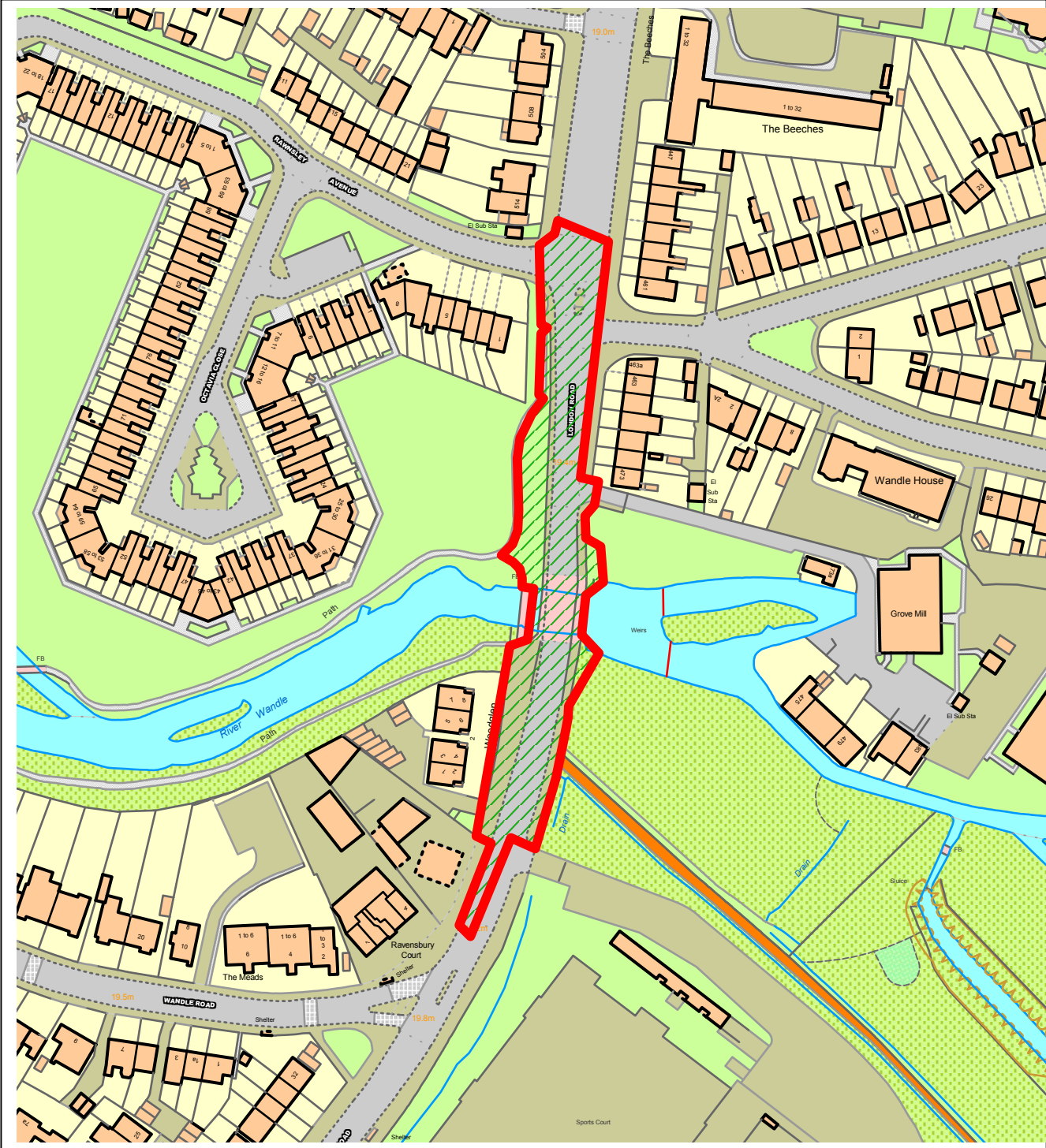
This condition is exempt from deemed discharge under schedule 6 of The Town and Country Planning (Development Management Procedure) (England) Order 2015.

Highways

3. The Highway section of the Council must be contacted prior to any works commencing on site to agree relevant licences, and access arrangements – no vehicles are allowed to cross the public highway without agreement from the highways section.

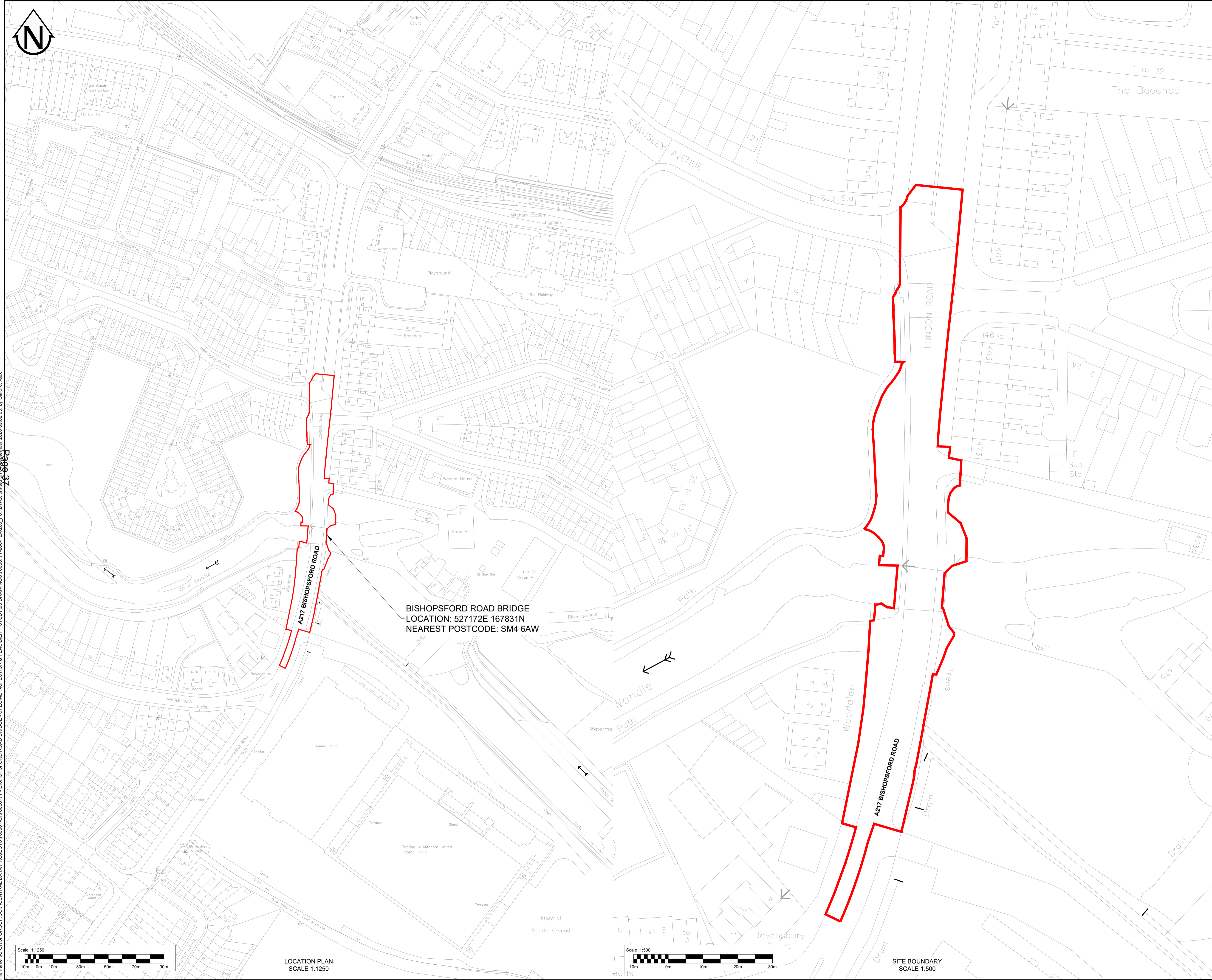
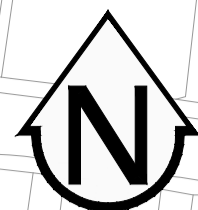
The applicant should contact David Furby of Council's Highway Team on: 0208 545 3829 prior to any work starting to arrange for this works to be done.

NORTHGATE SE GIS Print Template

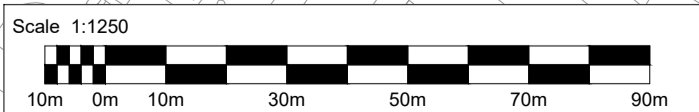


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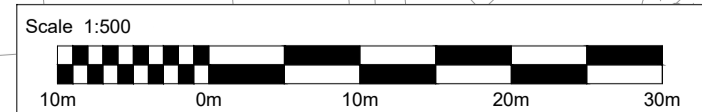
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BISHOPSFORD ROAD BRIDGE
LOCATION: 527172E 167831N
NEAREST POSTCODE: SM4 6AW



LOCATION PLAN
SCALE 1:1250



SITE BOUNDARY
SCALE 1:500

- NOTES:
GENERAL
1. DO NOT SCALE FROM THIS DRAWING.
 2. ALL DIMENSIONS IN MILLIMETERS UNLESS STATED OTHERWISE.
 3. DRAWING TO BE READ IN CONJUNCTION WITH ALL OTHERS IN THE 70066777-200-PLA-XXX SERIES.


REV	DATE	BY	DESCRIPTION	CHK	APP
P07	03.09.2020	AC	RED LINE BOUNDARY UPDATED	BP	ME
P06	20.07.2020	AC	RED LINE BOUNDARY UPDATED	BP	ME
P05	17.07.2020	AC	RED LINE BOUNDARY UPDATED	BP	ME
P04	01.07.2020	AC	RED LINE BOUNDARY UPDATED	BP	ME
P03	24.06.2020	AC	CLIENT COMMENTS INCORPORATED	BP	ME
P02	19.06.2020	AC	RED LINE BOUNDARY UPDATED	BP	ME
P01	12.06.2020	MB	ISSUE	BP	ME

DRAWING STATUS: S2 - FOR INFORMATION



No 2 London Square, Cross Lanes, Guildford, GU1 1UN, UK
T+ 44 (0) 1483 528 400, F+ 44 (0) 1483 528 989
wsp.com

CLIENT:
London Borough of Merton
Civic Centre
Morden,
Surrey,
SM4 5DX



SITE PROJECT: BISHOPSFORD ROAD BRIDGE

TITLE: LOCATION PLAN AND SITE BOUNDARY

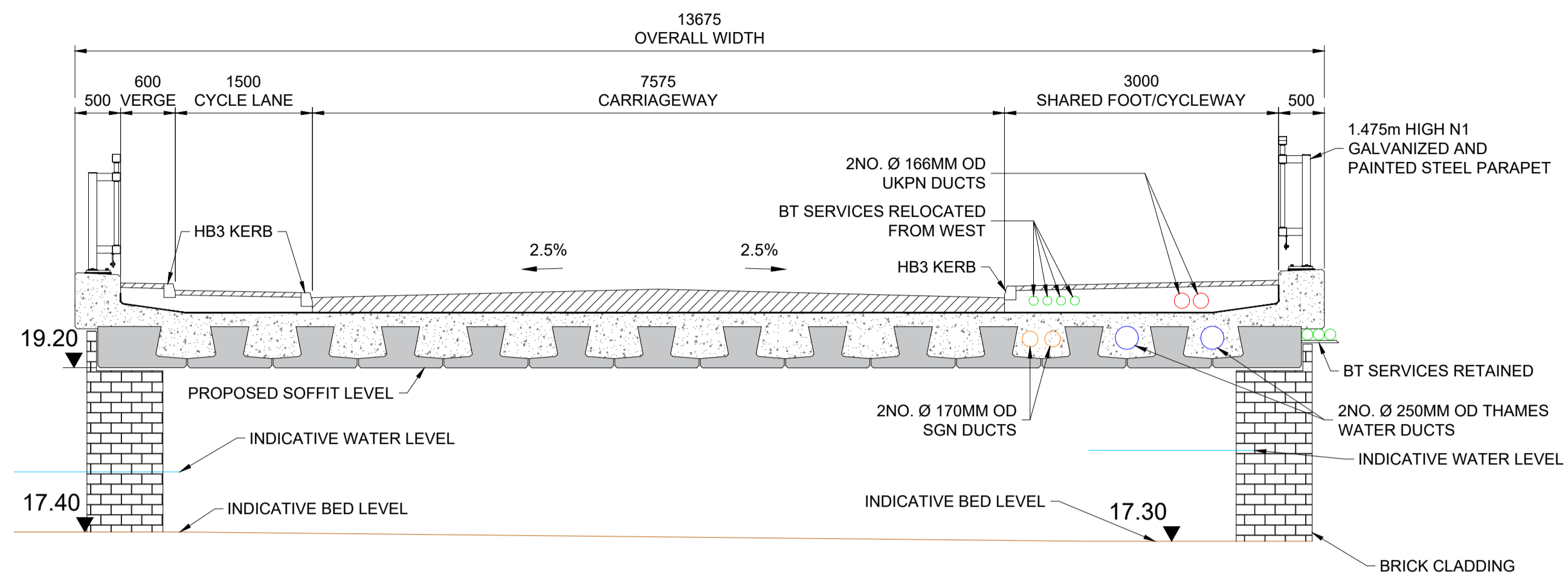
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AS SHOWN	BP	ME

PROJECT NO:	DESIGNED:	DRAWN:	DATE:
70066777	BH	MB	SEP 2020

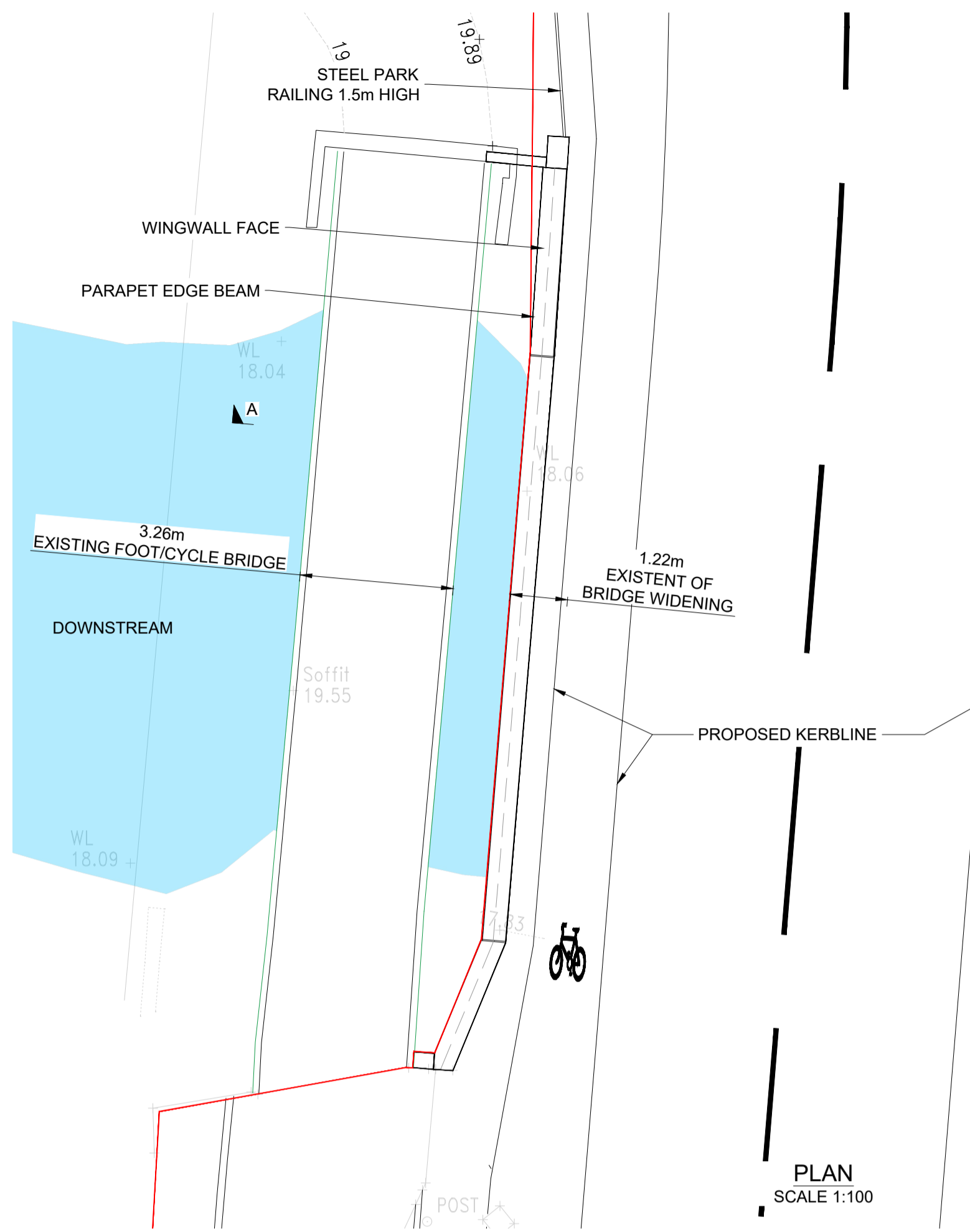
DRAWING NO: 70066777-200-PLA-200 REV: P07

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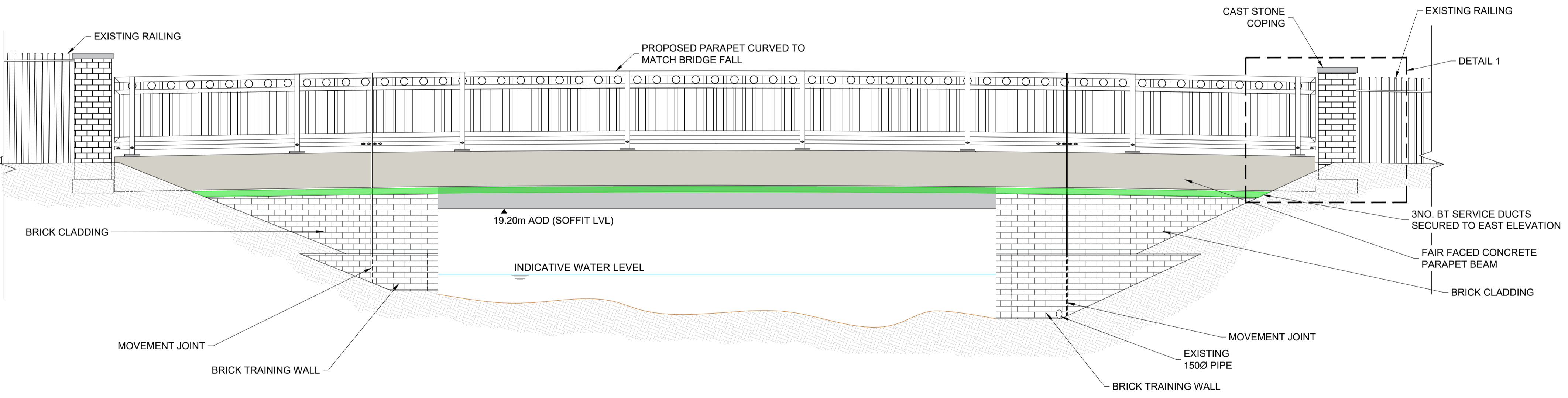
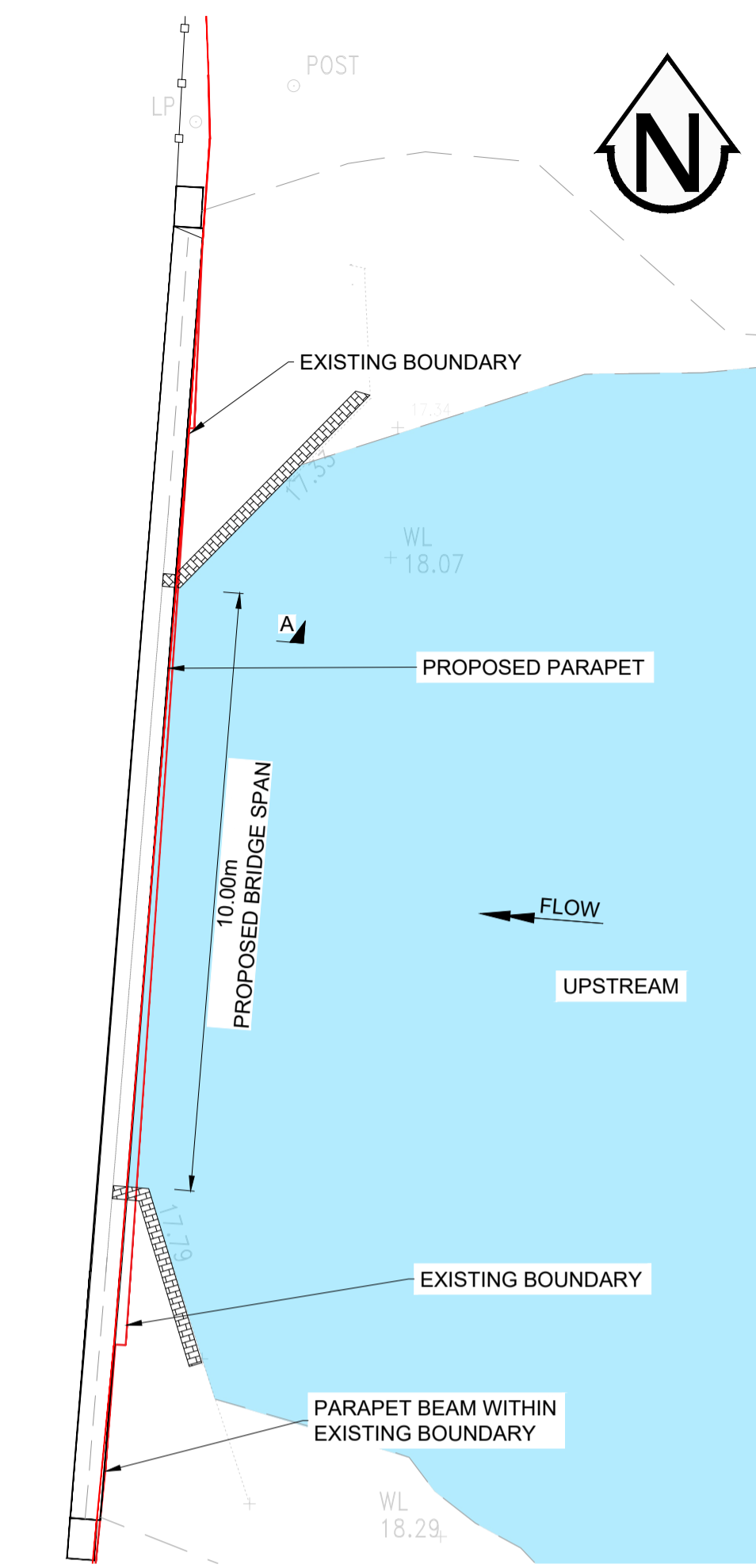
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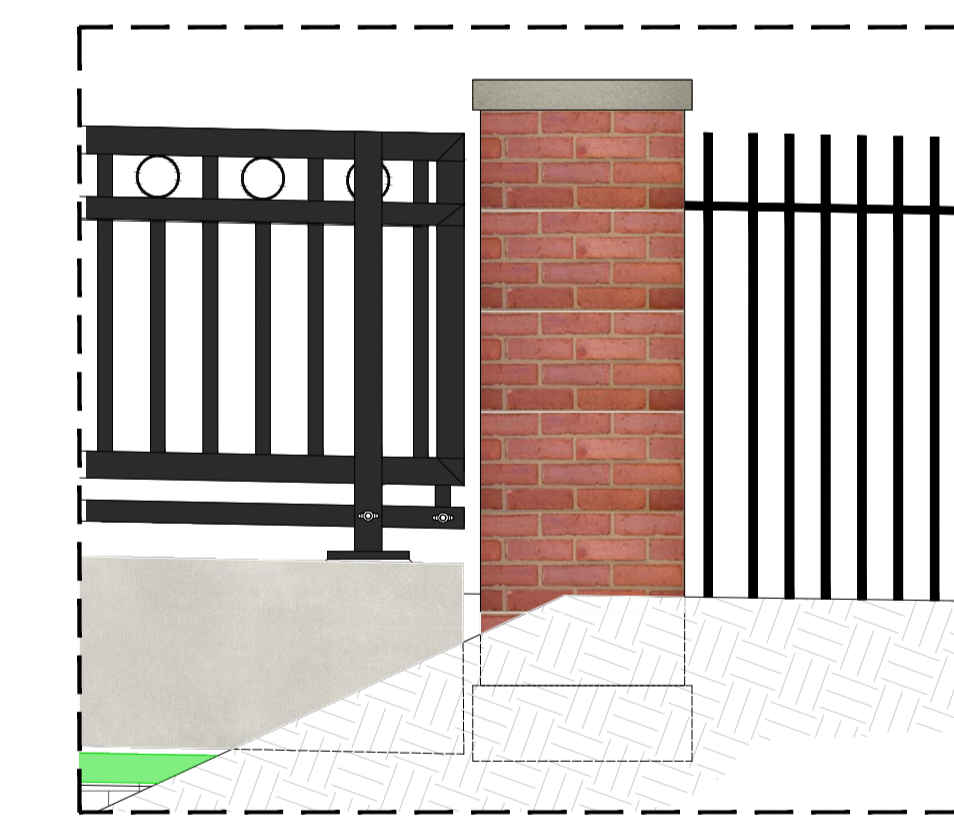
SECTION A-A
SCALE 1:50



PLAN
SCALE 1:100



EAST ELEVATION
SCALE 1:50



DETAIL 1
SCALE 1:25



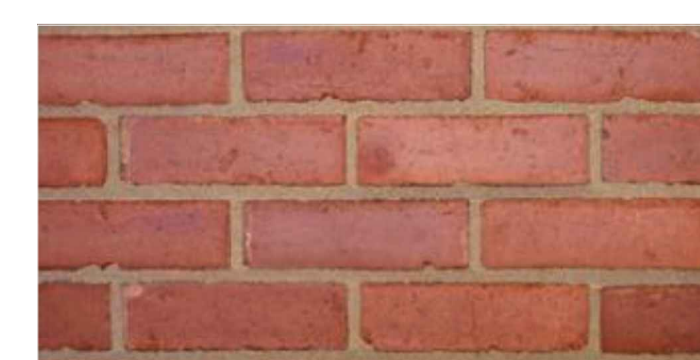
PROPOSED PARAPET COLOUR
SWATCH - RAL 9004



PROPOSED EDGE BEAM
COLOUR SWATCH



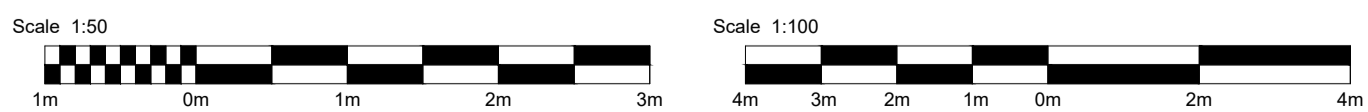
PILASTER COPING
COLOUR SWATCH



BRICK COLOUR SWATCH



PROPOSED PARK RAILING



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 - SERVICE ROUTES NOT SHOWN ON PLAN FOR CLARITY.

- LEGEND:
- EXISTING
 - PROPOSED
 - PROPOSED HIDDEN

REV	DATE	BY	DESCRIPTION	CHK	APP
P05	20.07.2020	AC	CYCLE LANE AMENDED	BP	ME
P04	01.07.2020	AC	RED LINE BOUNDARY UPDATED AND MATERIAL DETAILS ADDED	BP	ME
P03	24.06.2020	AC	CLIENT COMMENTS INCORPORATED	BP	ME
P02	19.06.2020	AC	NW WINGWALL, PARAPET AND KERB ALIGNMENT UPDATED	BP	ME
P01	15.06.2020	MB	ISSUE	BP	ME

DRAWING STATUS: S2 - FOR INFORMATION

No.2 London Square, Cross Lanes, Guildford, GU1 1UN, UK
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CLIENT:

London Borough of Merton
Civic Centre
Morden,
Surrey,
SM4 5DX

SITE/PROJECT: BISHOPSFORD ROAD BRIDGE

TITLE: PROPOSED GENERAL ARRANGEMENT

SCALE @ AS: AS SHOWN	CHECKED: BP	APPROVED: ME
PROJECT NO: 70066777	DESIGNED: BH	DRAWN: MB
DATE: JULY 2020		REV: P05
DRAWING NO: 70066777-200-PLA-202		

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Bishopsford Road Bridge Replacement Before and After (Proposed) Images

July 2020

The following package of “before and after” images are included to ensure a full visual appreciation of the proposal included in this planning submission.



Photo of the former bridge, viewed from upstream / south eastern riverbank (National Trust Land).

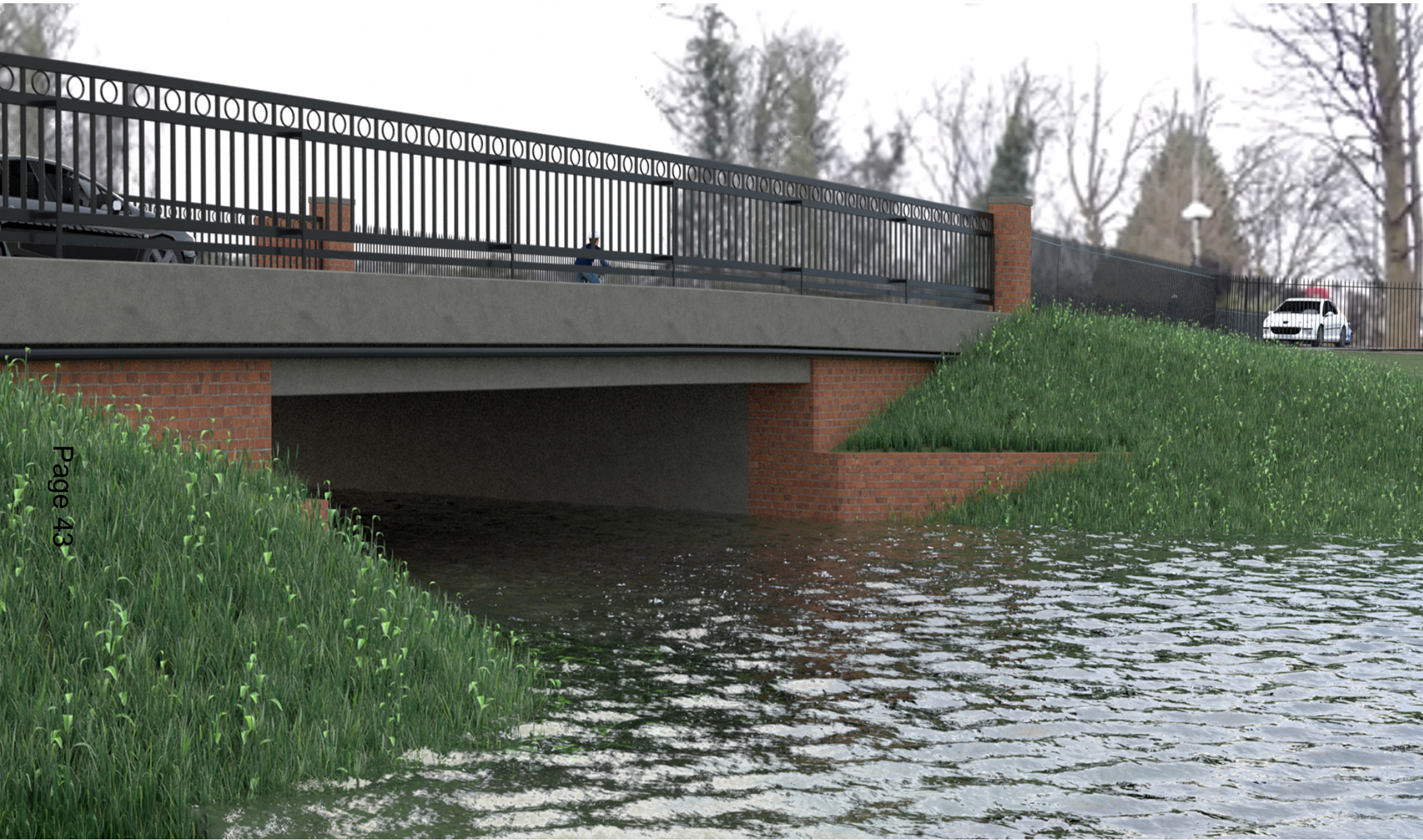


Image of the new bridge, viewed from upstream / south eastern riverbank (National Trust Land).



View southbound (Google maps April 2018).
Watermeads Nature Reserve to left of image. Ravensbury Park to the right.



View southbound on new bridge.

Watermeads Nature Reserve to left of image. Ravensbury Park to the right.

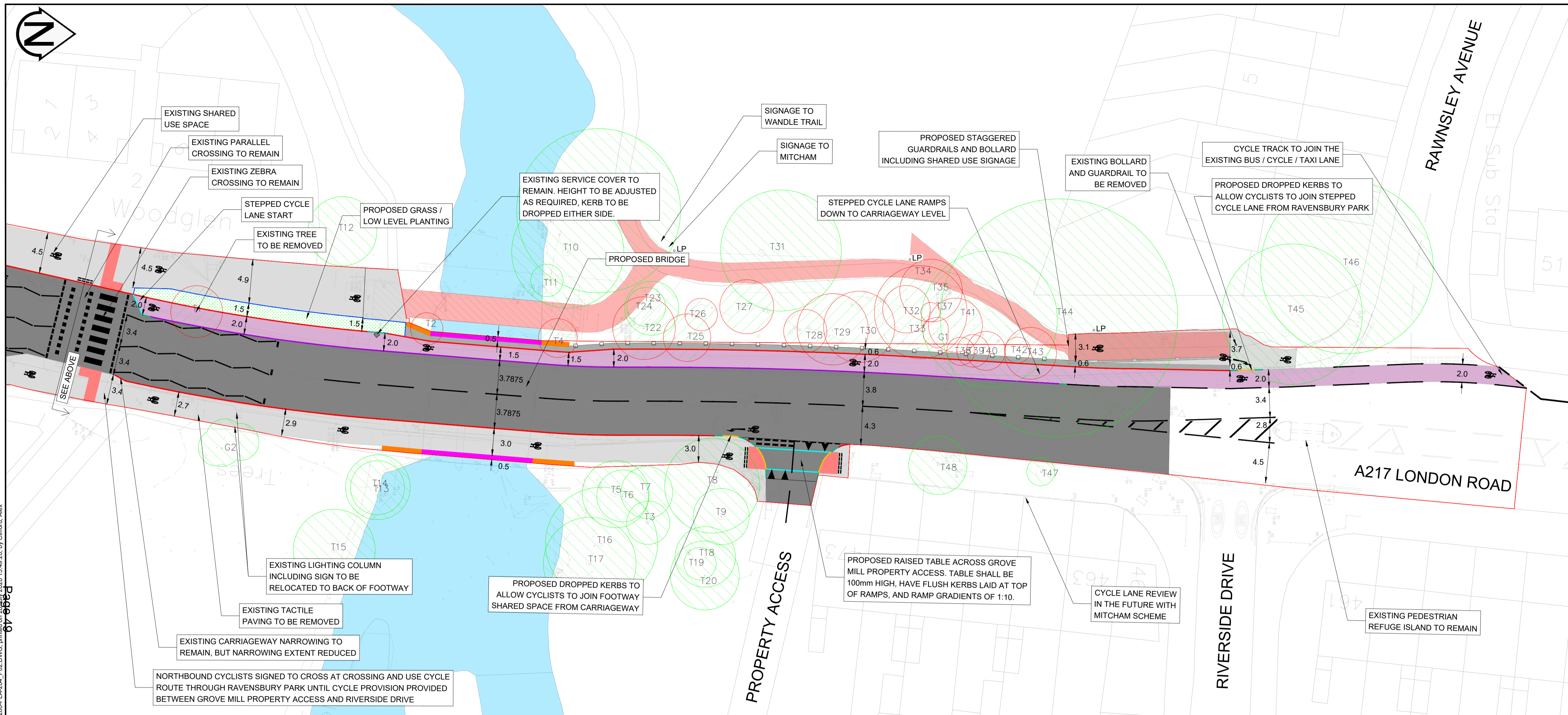


Image of the former bridge, looking north towards Mitcham town centre (Google Maps, April 2018).



Image view of new bridge looking north (towards Mitcham town centre).

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PLAN
SCALE 1:250



(SCALE= 1:250)



EXISTING SHARED USE PATH SHOWING WALL TO BE REMOVED AND EXISTING LAMP POST



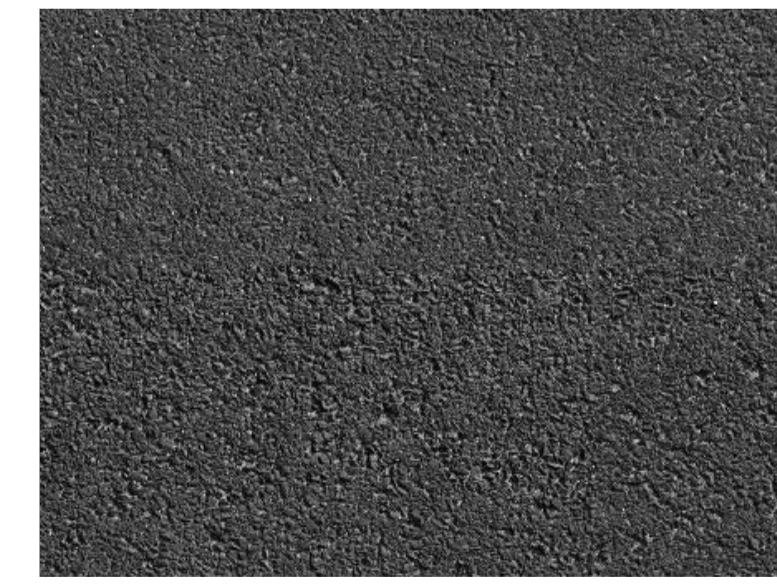
PROPOSED ADDITIONAL LOW LEVEL LIGHTING (IF REQUIRED)



PROPOSED STONE SETTS SURFACE FOR HARD VERGE



PROPOSED KERB COLOUR SWATCH



PROPOSED ASPHALT CARRIAGEWAY SURFACE COLOUR SWATCH

DO NOT SCALE

- NOTES:
- ALL DIMENSIONS ARE IN METERS UNLESS OTHERWISE STATED.
 - THIS DRAWING SHOULD BE PRINTED IN COLOUR.
 - NO TREE TRUNKS ARE LOCATED IN THE WAY OF ANY PROPOSED WORKS.
 - RISK OF DAMAGING TREE ROOTS, ARBORICULTURAL METHOD STATEMENT FOR WORKS TO BE DEVELOPED.

- KEY:
- LP EXISTING LAMP POST
 - RIVER WANDLE
 - EXISTING UNSEGREGATED SHARED USE FOOTWAY / CYCLEWAY
 - PROPOSED CARRIAGEWAY
 - PROPOSED UNSEGREGATED SHARED USE FOOTWAY / CYCLEWAY
 - PROPOSED HARD VERGE
 - PROPOSED ON-CARRIAGEWAY CYCLE LANE
 - PROPOSED BRIDGE PARAPET WALL
 - PROPOSED BRIDGE WING WALL
 - PROPOSED FULL HEIGHT KERB
 - PROPOSED KERB WITH 50mm UPSTAND
 - PROPOSED TRANSITION KERB
 - PROPOSED DROPPED KERB
 - PROPOSED KERB EDGING
 - PROPOSED FLUSH KERB
 - PROPOSED ROAD MARKINGS (INDICATIVE)
 - PROPOSED 1.5m HIGH STEEL PARK RAILING
 - ROOT PROTECTION AREA OF EXISTING TREE WHERE PROPOSED WORKS INFRINGE ON ROOT PROTECTION AREA
 - TREE ROOT PROTECTION AREA OF TREE TO BE REMOVED DUE TO CONDITION OR TO FACILITATE PROPOSED WORKS

REV	DATE	BY	DESCRIPTION	CHK	APP
P02	20.07.2020	AC	CYCLE LANE AMENDED, TREE SURVEY EXTENDED	BP	ME
P01	30.06.2020	AC	FIRST ISSUE	BP	ME

DRAWING STATUS: S3 - FOR REVIEW

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CLIENT:
London Borough of Merton
Civic Centre
Morden,
Surrey,
SM4 5DX

SITE/PROJECT:
BISHOPSFORD ROAD BRIDGE

TITLE:
PROPOSED HIGHWAYS GENERAL ARRANGEMENT

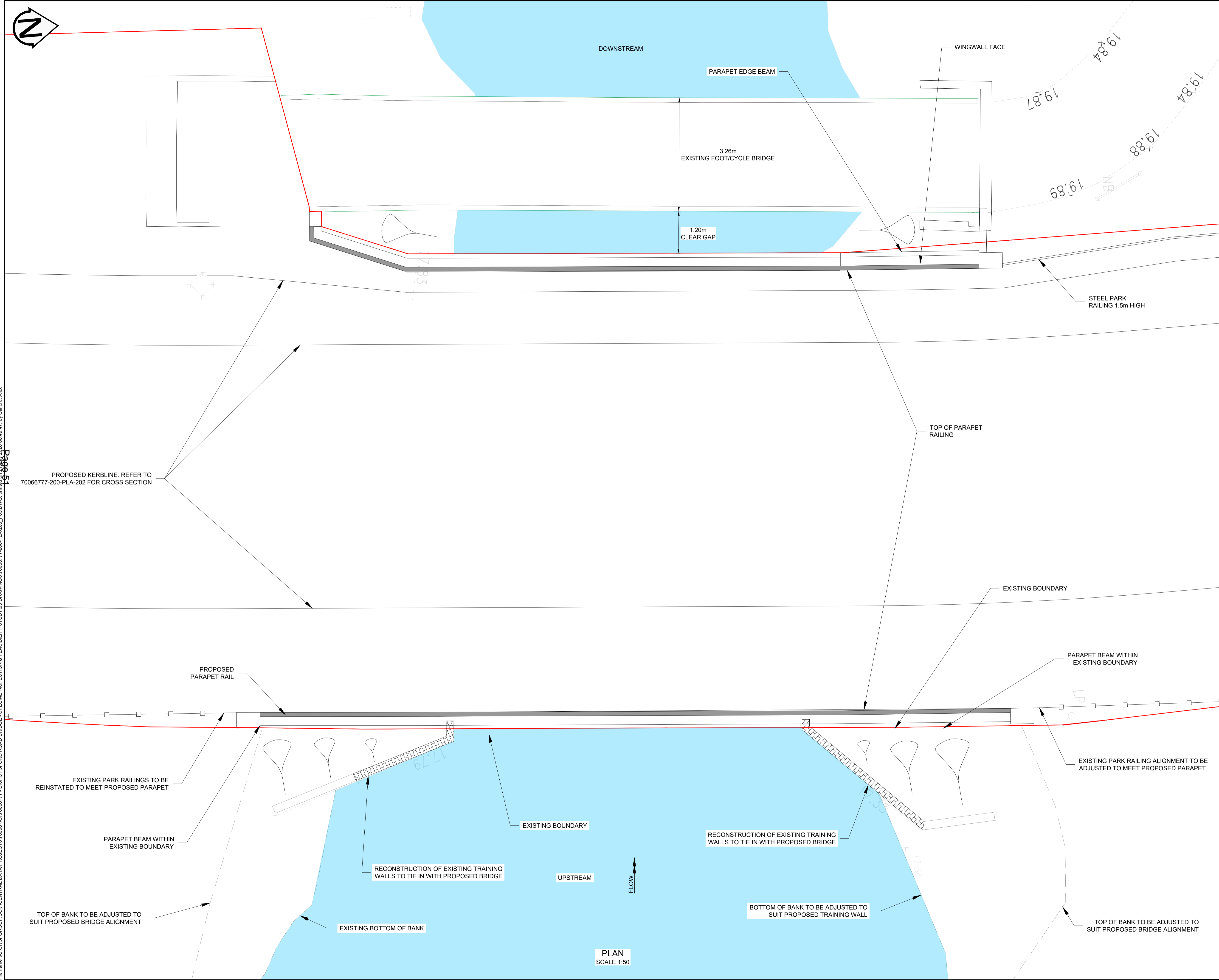
SCALE @ AT:	CHECKED:	APPROVED:
AS SHOWN	BP	ME
PROJECT NO: 70066777	DESIGNED: KP	DATE: JULY 2020
DRAWING NO: 70066777-200-PLA-204		REV: P02

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File name: I:\UK\WSP\GROUP\COMP\CENTRAL\DATA\PROJECTS\70066777 - BISHOPSFORD ROAD BRIDGE - SPECIAL INSPECTION & FEASIBILITY STUDY\05 DRAWINGS\70066777-200-PLA-204_P02.DWG, printed on: 20/07/2020 15:49:20, by: Clifford, Alex

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File name: I:\UK\WSP\GROUP\DATA\PROJECTS\70066777 - BISHOPSFORD ROAD BRIDGE - SPECIAL INSPECTION & FEASIBILITY STUDY\05 DRAWINGS\70066777-200-PLA-203_P05.DWG, printed on 21/07/2020 08:49:47, by Clifford, Alex Page 51



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REV	DATE	BY	DESCRIPTION	CHK	APP
P05	20.07.2020	AC	KERBLINE AMENDED	BP	ME
P04	29.06.2020	AC	MINOR UPDATES	BP	ME
P03	24.06.2020	AC	CLIENT COMMENTS INCORPORATED	BP	ME
P02	19.06.2020	AC	NW WINGWALL AND KERB ALIGNMENT UPDATED	BP	ME
P01	15.06.2020	AC	ISSUE	BP	ME

DRAWING STATUS: **S2 - FOR INFORMATION**

wsp

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 wsp.com

CLIENT:
 London Borough of Merton
 Civic Centre
 Morden,
 Surrey,
 SM4 5DX



SITE/PROJECT: **BISHOPSFORD ROAD BRIDGE**

TITLE: **REINSTATEMENT DETAILS**

SCALE @ AS:	AS SHOWN	CHECKED:	BP	APPROVED:	ME
PROJECT NO:	70066777	DESIGNED:	BH	DRAWN:	AC
				DATE:	JULY 2020

DRAWING NO:	70066777-200-PLA-203	REV:	P05
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PLANNING APPLICATIONS COMMITTEE

APPLICATION NO.

20/P2184

DATE VALID

24/07/2020

Address/Site: Melrose School
Church Road
Mitcham
CR4 2BE

Ward: Cricket Green

Proposal: PROPOSED EXPANSION OF MELROSE SCHOOL, INVOLVING THE ERECTION OF A SCHOOL HALL AND PRIMARY SCHOOL TEACHING BLOCK. THE PROPOSAL WOULD ALSO INVOLVE THE CREATION OF A SECONDARY DROP-OFF AND PARKING AREA. IN ADDITION TO VARIOUS EXTERNAL WORKS, ANCILLARY FACILITIES, LANDSCAPING, HARDSTANDING AND BOUNDARY TREATMENTS

Drawing No.'s:

0150 Rev P7; 0115 Rev P7; 0016 Rev P4; 0135 Rev P4; 0103 Rev P2; 0102 Rev P8; 0125 Rev P7; 0100 Rev P5; Energy Statement Rev01 Dated - 17/08/2020; Preliminary roost assessment and nocturnal bat surveys – Dated 18/07/2020; Proposed Drainage Strategy- September 2020; Preliminary Ecological Appraisal – Dated 18/03/2020

Contact Officer: Jourdan Alexander (020 8545 3122)

RECOMMENDATION

Grant planning permission subject to conditions

CHECKLIST INFORMATION

- Is a screening opinion required: No
- Is an Environmental Statement required: No
- Has an Environmental Statement been submitted: No
- Press notice: Yes
- Site notice: Yes
- Design Review Panel consulted: Yes
- Number of neighbours consulted: 19
- External consultations: 1 (Historic England)
- Conservation area: Yes - Mitcham Cricket Green Conservation Area
- Listed building: No
- Archaeological priority zone: Yes - Tier 2
- Tree protection orders: No
- Controlled Parking Zone: No
- Flood Zone: Flood Zone 1

- Designated Open Space: Yes
- Green Corridor: Yes

1. INTRODUCTION

- 1.1 This application is being brought to the Planning Applications Committee for determination because the proposal involves the Council as both the applicant and landowner and the scheme is not of a minor nature.

2. SITE AND SURROUNDINGS

- 2.1 The site comprises Melrose School in Mitcham, which is a state special school for boys and girls aged from 11 to 16, with Social, Emotional and Mental Health needs (SEMH). The school currently offers education to approximately 45 students.
- 2.2 The school is entered and existed from a driveway formed off Church Road. The London Road Playing Fields are located to the south of the site. To the north are residential houses and 'Hall Place', which offers rooms to homeless people from the Merton area.
- 2.3 The eastern border of the site is formed along an access road from Church Road, which is owned by the Council. Merton Community Transport occupies a large red brick building midway along the access road on the eastern side. At the termination of the access road is 'Worsfold House' a former council housing office.
- 2.3 The potential for development of land comprising Worsfold House and Chapel Orchard is identified for school expansion and housing and is Site Proposal 17 in the Adopted Sites and Policies Plan (2014).
- 2.4 The school comprises a large 'U shaped' single storey building in red brick. Around the school building are external playing areas, landscaped lawns, mature trees with areas of scrubs and other forms of vegetation.
- 2.5 The site has a PTAL (public transport accessibility level) of 3 (0 being the lowest and 6b being the highest). There are bus stops near to the school, along Church Road. These stops provides access to a route which links Raynes Park Station and Mitcham Fair Green. The Belgrave Walk Tram Stop is located 650m east of the site.
- 2.6 The site is located within the Mitcham Cricket Green Conservation Area. The existing buildings on site are not locally or statutorily listed. The site is within designated open space and within a green corridor.

3. CURRENT PROPOSAL

- 3.1 The proposal involves the formation of a primary school element on the south-eastern part of the site to accommodate approximately 24 primary aged SEMH students. The proposed massing to the site involves the construction of a two-storey hall and single-storey teaching block. The new buildings would create 622m² of new floor space. The applicant proposes that the new hall would be connected to the southern end of the existing school building. The new classroom block would join with the eastern elevation of the hall with the massing positioned along the eastern boundary and orientated at a north/east angle. The new built massing on the site would form an 'L' shape, and would be configured around an upgraded multi-use games area (MUGA).

- 3.2 The primary school provision has been designed such that the primary school pupils would not be integrated with the secondary school pupils. To achieve this, certain spaces such as the new classroom block and drop-off / collection area would be allocated for the sole use of the primary school. The new school hall and MUGA would however be shared in its use between both the primary and secondary school, although the timing of usage would be controlled to retain separation. The design and division of usage is necessary to ensure school pupils would have a safe and secure environment, with consideration to the school's roll of students with SEMH needs.
- 3.3 A separate drop-off and collection area would be formed and situated to the north-eastern corner of the site and would link with the existing access road to Worsfold House. This access road is outside the red-line boundary of the site. The placement of massing on the site would require the removal of 9 trees. It would also encroach onto both open space and the green corridor.
- 3.4 In terms of building floor plans, the proposed classroom block would comprise 4 classrooms with internal corridor, along with toilets, storage and administration room. The hall would comprise a singular useable space. The buildings would be finished in red/ brown brick with timber panelling and cladding.

4. PLANNING HISTORY

- 4.1 MER617/65 - EXTENSION TO PROVIDE MEDICAL INSPECTION ROOM. Deemed Consent
- 4.2 MER9/77 - ALTERATIONS AND EXTENSION. Grant Permission
- 4.3 MER1141/79 - GARAGE IN SCHOOL GROUNDS. Grant Permission
- 4.4 08/P1497 - ALTERATIONS TO ROOF COVERING INCLUDING REMOVAL OF SLATES AND REPLACEMENT OF SOFFITS AND FASCIAS . Grant Permission
- 4.5 17/P2300 - INSTALLATION OF APPROX 60M LENGTH OF 3 METRE HIGH METAL MESH FENCING WITH NEW AUTOMATIC TWO LEAF GATE. Grant Permission
- 4.6 Planning Officer's also provided pre-application advice to the applicant concerning the submission scheme in July 2020.

5. CONSULTATION

- 5.1 Public consultation was undertaken by way of site notices, press notices and letters sent to 19 neighbouring properties.
- 5.2 Two representations were received from the Mitcham Cricket Green Community Heritage Group as summarised:
- The application was not supported with a Heritage Statement. It will be important for the Heritage Assessment to address the impact on local views, boundary treatments, materiality and the wooded character of the school grounds.
 - We are also disappointed by the lack of engagement prior to submission of this planning application.
 - We recognise the demand to increase the capacity of Melrose School and support the important contribution it makes to the local community.

- Our fundamental concern with the plans is that they develop a significant proportion of the remaining open space on the site. This development is in the most sensitive part of the site where it is within Mitcham Cricket Green Conservation Area and it will result in the significant loss of trees and a diminished tree canopy cover.
- We believe a well-designed two-storey option (above the existing school block) would be most appropriate to avoid intrusion of open space.
- We welcome the intention for a new link between Church Road and London Road Playing Fields that will run between Melrose School and the Worsfold House site.
- The new boundary fence is functional in design but there are better examples of heritage railings in the conservation area.
- We are also concerned by the narrow dead corridor between the east side of the new primary teaching block and the new fence which lacks an obvious function.
- We believe the proposals will cause unnecessary harm to the Conservation Area and result in the unnecessary loss of significant trees. The plans should include compensatory tree planting that provides trees of greater CAVAT value, along with guarantees for their future maintenance and management from the day they are planted.
- We are also unconvinced that the proposals for log piles, bird nesting boxes, bat boxes and a hedgehog home provides the level of assurance needed that the development will result in biodiversity net gain.
- The Heritage Assessment (later submitted) is incomplete and its conclusions flawed for the lack of consideration given to these important aspects of the Conservation Area.
- Given the significant impacts we have identified and the opportunities for a more sympathetic development we believe planning permission should be refused and an alternative approach brought forward which develops less of the site and does not conflict with Policies CS13, DM D1, DM D2, DM D4 and DM O2 or harm the Conservation Area.

5.3 Officer's response – A heritage statement was provided by the applicant on the 02/10/2020 and uploaded to the planning file as well as sent directly to the Mitcham Cricket Green Community Heritage Group, who have subsequently provided additional comments. The matters raised in the letters of representation are discussed beneath. The comments made which provide support for various parts of the proposal are also noted.

Internal consultees:

- 5.4 LBM Climate Change Officer: No objections subject to conditions that secure:
 - Post-Construction Review Certificate.
 - The approved development must be constructed in accordance with the design specifications for reducing carbon emissions.
- 5.5 LBM Environmental Health Officer: No objection
- 5.6 LBM Transport Planner: Transport have no objection in principle to the proposed development on this location subject to following conditions:
- Grampian Condition or S.106, requiring the upgrade of access to adoptable standards to completed prior to occupation of the new school block.
 - Car parking maintained.

- Condition requiring cycle parking provision.
- Demolition/Construction Logistic Plan (including a Construction Management plan in accordance with TfL guidance) should be submitted to LPA for approval before commencement of work.

5.7 LBM Tree Officer: It is proposed to remove 7 trees in connection with this development. 4 of those have been assessed as being 'B' category trees. No arboricultural objections raised towards the development, subject to conditions.

5.8 Biodiversity Officer: Policies CS13 and DMO2 set out the biodiversity and nature conservation requirements for proposed developments. Biodiversity should be protected and enhanced; trees, hedges and other landscape features of amenity value should be protected, and suitable replacements provided where a loss may be justified. The proposed development has the potential to impact on existing biodiversity through the removal of trees and scrub on site. As such, the applicant has provided the following ecological reports:

- Preliminary Ecological Appraisal (March 2020)
- Preliminary roost assessment and nocturnal bat surveys (July 2020)

The methodology and findings of the Extended Phase 1 Habitat Survey, Preliminary Roost Assessment and Nocturnal Surveys appear suitable for this site. The roost assessment concluded there are no bat roosts on the site, however the dusk and dawn emergence surveys identified a number of bats that use the site for foraging and commuting, although this was deemed to be a relatively low number. Therefore, the proposed development has the potential to impact on the movements of these bats and a number of recommendations are included in the reports to minimise this impact. I would recommend that a condition is added to ensure all the recommendations from the 2 ecological reports are carried out at the appropriate stages of the development.

Conditions are recommended to safeguard and enhance habits for birds, bats and invertebrates, including biosecurity plan

5.9 LBM Flood and Drainage Officer: The development proposed is acceptable provided a condition is in place requiring that the scheme for surface and foul water drainage be completed in accordance with the submitted drainage strategy report.

5.10 Design Review Panel: The scheme was reviewed by the Design Review Panel on 30 July 2020. The comments from the panel are summarised.

The Panel were generally positive towards the proposals but felt that there was need for further thought regarding a number of aspects of the design and layout that could improve the proposal to make it more flexible, user-friendly and bring more joy to the school experience for its special needs pupils. The Panel welcomed the general low-key subtle approach to the design, and the materials were appropriate to this.

The panel thought that opportunities could be taken to enhance the permeability of the layout to improve ease of movement for individuals in the school. The Panel felt it was important to investigate the possibility of positioning the hall closer to the new site entrance to create and investigate potential to build a welcome foyer.

In terms of the general site layout, the Panel noted the new build occupied a high proportion of the available space and suggested that a two-storey element should be investigated.

The tone of the architecture was welcomed; it was felt that further improvement work could be taken to more harmoniously integrate the pitched roof of the school block with the flat roof of the gym. The building would also need to appear compatible with its surroundings and integrate with the adjacent council development site, the panel questioned if this was the case.

It was important that there should be a traffic free route, from Church Road to the school, consisting of a separate footway and suitable crossing of Church Road. The need for an improved foyer extended to the need to provide a generous external space at the school gate. The narrow strip of land along the eastern boundary did not contribute well to this.

Whilst the Panel understood the site constraints and that there are primary and secondary elements to, it felt it was worth considering whether the existing and new accesses could be combined to create a one-way in-out access for the site to overcome some of the physical constraints.

The Panel gave an amber judgement on the scheme

5.11 Design Review Panel Improvements:

Since the Design Review Panel, the applicant has made a series of improvements to the scheme:

- Additional doors have been incorporated into the design to improve accessibility and permeability, although this does need to be carefully controlled by the school to stop vulnerable children accessing the woodland.
- The applicant has provided justification for the siting of the hall. Stating that it is sited to accommodate the operational day-to-day needs of the school, as this is the primary use of the space. The hall will be used by both the secondary and primary students and so its positioning is the most beneficial location for the school.
- In term of building a welcome foyer, the applicant has concluded that due to the low density and nature of pupil arrivals to the school a large atrium space or external gathering areas are not a requirement or desirable for the primary element.
- Justification was also provided for the transition between the school block and hall as well as site layout and massing. In terms of the provision for a two-story classroom the applicant has stated that this option is non-viable due to the existing school being required to remain operational during any construction period. In addition, due to the nature of an SEMH school it was considered that such a strategy was not appropriate due to the significant operational and management compromises that would be required.
- In terms of access changes - Melrose and Merton have reached an agreement with Cricket Green School to provide a new section of footpath along the proposed access route. Parking spaces have not been provided for all staff, 9 parking spaces have been provided for 14 staff members. This is considered by the applicant to be an acceptable ratio for the location of the school.

6. POLICY CONTEXT

London Plan (2016)

Relevant policies include:

- 2.6 Outer London: Vision and strategy
- 2.8 Outer London: Transport
- 3.18 Education facilities
- 5.1 Climate change mitigation
- 5.2 Minimising carbon dioxide emissions
- 5.3 Sustainable design and construction
- 5.12 Flood risk management
- 5.13 Sustainable drainage
- 6.3 Assessing effect of development on transport capacity
- 6.9 Cycling
- 6.10 Walking
- 6.11 Smoothing traffic flow and easing congestion
- 6.13 Parking
- 7.3 Designing out crime
- 7.4 Local character
- 7.5 Public realm
- 7.6 Architecture
- 7.19 Biodiversity and access to nature
- 7.21 Trees and woodlands
- 8.2 Planning Obligations

Merton Local Development Framework Core Strategy – 2011 (Core Strategy)

Relevant policies include:

- CS 11 Infrastructure
- CS 12 Economic development
- CS13 Open space, nature conservation, leisure and culture
- CS 14 Design
- CS 15 Climate change
- CS 17 Waste management
- CS 18 Transport
- CS 19 Public transport
- CS 20 Parking servicing and delivery

Merton Sites and Policies Plan – 2014 (SPP)

Relevant policies include:

- DM C1 Community facilities
- DM C2 Education for children and young people
- DM D1 Urban Design
- DM D2 Design considerations
- DM D3 Alterations and extensions to existing buildings
- DM D4 Heritage assets
- DM EP2 Reducing and mitigating noise
- DM O2 Nature Conservation, Trees, hedges and landscape features
- DM T2 Transport impacts of development
- DM T3 Car parking and servicing standards
- DM T4 Transport infrastructure

6.1 Supplementary planning considerations
Cricket Green Conservation Area Appraisal

7. PLANNING CONSIDERATIONS

7.1 The decisive planning issues towards this application are:

- Principle of development
- Open space
- Biodiversity, trees and landscaping
- Design and heritage
- Transport and highways
- Impact upon neighbouring amenity
- Sustainable design and construction
- Archaeology
- Flooding and drainage

Principle of development

7.2 The most applicable Council policies to determine whether the expansion of the school is acceptable are policies DMC2 and DMC1 of Merton's Sites and Policies Plan.

7.3 Policy DMC2 (Education for children and young people), aims to ensure there are sufficient school places of a suitable modern standard to meet statutory requirements while also ensuring sufficient choice of school places. Development proposals for new schools and/or improved education facilities for children are supported, particularly where new facilities are required to provide additional school places in an area to meet an identified shortfall in supply.

7.4 As per policy DMC2, the applicant has submitted evidence that the improved school facilities and expansion of classroom spaces are needed to meet an identified shortfall in supply. The evidence submitted includes the Cabinet approved DSG Recovery Plan and the Council's Report concerning Merton School Places (Oct 2019). The Report finds that the growth of Special Education Needs (SEN) in education has grown significantly. Finding places for SEN students within Merton has become a significant issue. At present, Merton's accommodation for these students has reached capacity, and therefore there is an increased reliance on Independent Schools to support the Borough's needs. The Council's SEN strategy is to support schools to keep pupils in mainstream education as far as possible, and where practical to use surplus space in primary schools rather than claim further use of non-school council property.

7.5 Policy DMC2 also states that developments that provide additional school places will also be considered against criteria in Policy DMC1 a), which is as follows:

- i. services are co-located where possible;
- ii. facilities are provided in accessible locations with good links to public transport;
- iii. the size of the development proposed is in relation to its context;
- iv. appropriate access and parking facilities are provided, relative to the nature and scale of the development;

- v. the proposed facilities are designed to be adaptable and suitable to accommodate a range of services; and
- vi. the use(s) do not have an undue adverse impact on the amenities of nearby residents and businesses.

7.6 With respect to satisfying each of the above points.

- i. The primary school would be located next door to the existing secondary school, and therefore the school would be co-located.
- ii. The school would be located in an area with a PTAL of 3 which indicates that the site would have good public transport access. The school is also located in a predominantly residential setting in which there are families and thus schoolchildren living close by, and therefore the school would be appropriately located in this regard.
- iii. The proposed building and internal layout would be suitable in size for its context. This position is taken given that the new school would be located within an area that is of relatively low density. The school block would have a similar scale as that of the existing secondary school and therefore would reinforce the established built form within this area.
- iv. To satisfy access and parking criteria, the applicant proposes to provide a new drop off and collection area for the primary school. Further upgrades to the access road would be secured by condition (Please see 'transport and highway's' section beneath)
- v. The proposed building layout would be across one level, and therefore would have a good degree of adaptability. It would also have an inclusive design suitable for less abled individuals. It is not considered that the proposal would 'design out' other potential uses of the building.
- vi. The proposed primary school would occupy part of the site currently occupied by the secondary school, and therefore associated noise from school activities are already established in this area. The proposed school is unlikely to materially increase the existing noise profile of the site. (Please see 'impact upon neighbouring amenity' section beneath).

7.7 The expansion of the school and improvement of facilities would provide a valuable increase of student spaces, which is strongly supported by the Local Plan. Therefore, notwithstanding other decisive planning matters discussed beneath, the principle of the development is welcomed.

Impact on Open Space

7.8 Policy - DMO1 states that Open Spaces should be protected from inappropriate development and enhanced, maintaining appropriate elements of play areas and landscape features to make a positive contribution to the wider network of open spaces.

7.9 The new school building and facilities will be partially located on existing Open Space, which the Council ordinarily seeks to protect. However, the encroachment onto Open Space needs to be weighed against the positive improvements that the scheme would deliver. Firstly, the proposal would be improving the quality of the multi games area (MUGA) on site. Secondly, access to the site would be improved through planned improvement works along the access road from Church Road.

Access into the site would also be enhanced through a Community Use Agreement. This agreement would enable the public accessibility to both the MUGA and the hall (which could be used for sport) out of school hours. The woodland teaching area, which is currently not designated as open space, will be retained and enhanced and could potentially be considered for open space designation through the new local plan process.

- 7.10 Officers acknowledge that the proposals set up a tension between the objectives of the Council's planning policies pertaining to the delivery of improvements to education and those that seek to safeguard open space, in this case not publicly accessible space. However, it is considered that in this circumstance, the need and provision of the school places and the improvements to the quality and accessibility of the Open Space and play areas (through a Community Use Agreement) would contribute positively, and would overall enhance the Open Space, consistent with policy aims. Officers would also highlight that within the Sites and Policies Plan 2014 open space designations have been drawn very closely around this and neighbouring school buildings creating a challenge to the natural expansion of the school.

Impact on biodiversity, trees and landscaping

- 7.11 Policies CS13 and DMO2 set out the biodiversity and nature conservation requirements for proposed developments. Biodiversity should be protected and enhanced; trees, hedges and other landscape features of amenity value should be protected, and suitable replacements provided where a loss may be justified.
- 7.12 The proposed development has the potential to impact on existing biodiversity through the removal of trees and scrub on site. As such, the applicant has provided the following ecological reports:
1. Preliminary Ecological Appraisal (March 2020)
 2. Preliminary roost assessment and nocturnal bat surveys (July 2020)
- 7.13 The above documents have been reviewed by the Council's Biodiversity Officer, who has concluded that the methodology and findings of the Extended Phase 1 Habitat Survey, Preliminary Roost Assessment and Nocturnal Surveys are suitable. The roost assessment concluded there are no bat roosts on the site, however the dusk and dawn emergence surveys identified a small number of bats that use the site for foraging and commuting. The ecological report made a series of recommendations to avoid bat impacts and these recommendations have been secured by conditions.
- 7.14 The development would result in the loss of approximately 38 floral/tree habitats. Appropriate habitat compensation will thereby be required through new native tree and shrub planting on site, containing appropriate berry-rich and nectar-rich species to benefit birds, invertebrates and other biodiversity. The application has not provided a sufficient level of details concerning new landscaping and biodiversity enhancements. However, it is considered that these details can be successfully captured by way of condition. Conditions are therefore recommended requiring that the applicant provide suitable replacements and enhancements on site.
- 7.15 The impacts of the development on birds, insects and other mammals would not be harmful, subject to conditions that minimise impacts such as timing of works outside nesting seeding, site supervision by ecologists, and ecological enhancements installed. A condition requiring a biosecurity plan is also considered necessary to minimise invasive species.

- 7.16 An arboricultural report has been submitted by the applicant, which finds that 7 trees will need to be removed because the built footprint would overlap the tree's root protection areas. These trees include sycamores, maple, beech, and lime trees. Of the 7 trees earmarked for removal 4 of the trees are Category C (low quality). These trees were found to have a number of defects/limitations on their health including cavities, impacts from pollarding, shading from other trees, or have split into two forks. The other 3 trees are Category B (moderate quality). The loss of these trees is regrettable, however it is acknowledged that the applicant in expanding the school is placed in a difficult position between preserving the site's ecology, and positioning the school's facilities to be functional, fit for purpose, and meet the needs of the students.
- 7.17 Overall, it is considered that the applicant has found an acceptable balance. The site would retain a woodland character within certain areas, such as the southern end of the site, in which a large quantity of mature trees would be retained. In addition, the trees that do require removal are predominantly of low quality. To compensate for the trees removed during the works compensatory planting is proposed by the applicant. The precise location of the new trees, species and sizes have yet to be established. However, a condition has been recommended that would secure appropriate tree replanting, the details of which would require written approval from the Council to ensure the replanting proposed is satisfactory.
- 7.18 The construction work and new buildings have the potential to impact trees retained within the site. The arboricultural report has listed a number of tree protection measures that could be adopted. However, the final tree protection plan will be agreed between the local planning authority and applicant before the commencement of works. This has been secured by way of condition.
- 7.19 Subject to the above provisions, the proposal is considered to have an acceptable impact upon biodiversity, trees and landscaping

Design and impact upon the character and appearance of the area

- 7.20 The NPPF section 12, London Plan policies 7.4 and 7.6, Core Strategy policy CS14 and SPP Policy DMD2 require well designed proposals which would optimise the potential of sites. Proposals should be of the highest architectural quality and incorporate a visually attractive design that is appropriate to its context, so that development relates positively to the appearance, scale, bulk, form, proportions, materials and character of their surroundings. SPP Policy DMD4 requires development to conserve and where appropriate enhance Merton's heritage assets and distinctive character.
- 7.21 Parts of the application site fall within the Cricket Green Conservation Area. The conservation area appraisal for this area identifies the buildings that contribute positively to the area, those that have a neutral impact and those with a negative impact. The proposal identifies 'Hall Place' located directly north of where the new school buildings would be placed as negatively impacting the conservation area. Other surrounding buildings including the existing school buildings have a neutral impact. The conservation appraisal describes the area of the subject site as having modern developments, which incorporate a variety of predominantly modern window styles and types commensurate with the diversity of their architecture. Overall, the site in which the new school buildings are proposed has limited heritage value despite its conservation area allocation.

- 7.22 The new school buildings are considered to have a positive impact on the area and would enhance the appearance of the Cricket Green Conservation Area and its setting. In terms of massing, the existing secondary school buildings are single storey with pitched roofs. The bulk and height of the new buildings retain a similar low profile with the school block having a single storey with pitch roof and the hall being two storey with a flat roof. The bulk and height of the buildings would not be out of keeping with the prevailing building heights within the area, and would complement the height and profiles of the existing school buildings. The proposed massing of the buildings would not appear dominant from external vantages.
- 7.23 The site in which the new school buildings would be built is of limited visibility from public spaces, given that the access road from Church Road is not adopted highway. However, views attained towards this part of the site from the access road currently provide a bleak outlook onto the school. The prevailing visible features include a large expanse of concrete, enclosed by a tall chain link fence as well as a hostile appearing metal stake fence. Overall, the site is in a poor condition and is unattractive in appearance.
- 7.24 The new buildings would provide a significant visual improvement to this space and would animate the access road frontage. In terms of building layout, the development would be orientated to provide a clear entrance and new visual identity for the primary school. The school building would partly encircle the MUGA, which would provide a centre point for play activity for the school, whilst the layout would ensure a sense of spaciousness from internal vantages within the classrooms.
- 7.25 The school buildings would be appropriately distanced from Hall Place to the north, which would ensure that this site's residential neighbours retain an open feel, similar to the existing situation and not enclosed by buildings. The existing row of mature trees along the boundary of Hall Place and the school would be retained.
- 7.26 The building layout provides a flexible use in which movement between buildings can be either internal via the hallway or external via a covered passageway. The school buildings have also been positioned in a manner that attempts to capture the benefits of the woodland environment particularly to the south of the site. To achieve this, the classroom block would have large window / door openings as well as designated external spaces to the south of the building providing both outlook into the wooded areas as well as direct access. This design allows teaching to take place in the woodland itself. This is considered a thoughtful design and layout that seeks to integrate the buildings and school activities with the natural environment.
- 7.27 Internally, the building's layout creates a wide range of teaching spaces and facilities. The internal layout makes efficient use of space, as well as optimising the use of facilities. Given that the school provides care and teaching for students with SEMH needs, the internal layout has been designed slightly differently than a traditional school with designated spaces for students to withdraw. The primary school and high school components would have shared access to the new hall. The hall is sited centrally between the primary and secondary components to accommodate the operational day-to-day needs of both schools. The hall would be accessed via an external covered walkway from each respected school element, which would optimise the use of shared facilities.
- 7.28 The layout of buildings within the site also allows a range of useful external spaces to be provided including the upgraded MUGA, woodland teaching zones, and enclosed outdoor spaces for each classroom. The buildings have a good degree of

permeability due to the positioning of doors and openings that provides flexibility of movement.

- 7.29 The proposal also seeks to provide a new access with a drop/off collection area for the primary school. This area would be located to the north of the proposed school buildings and would access onto the access road from Church Road. The drop/off collection provisions would cover a relatively small area within the site. The size of these areas is determined by the required turning circles of vehicles to enter and exit the site safely. Nine carparks are proposed with one to be a designed disabled bay. The drop-off / collection and parking are considered a sensible design and are not of size that would create visual harm.
- 7.30 The new classroom block and school hall would utilise high quality materials that would appear in keeping with the conservation area. The classroom block and hall would be finished in red / brown facing brick in stretcher bond with smaller panels in English / Flemish bond, along with sections of timber panels / cladding. Window framing would be in aluminium. The pitch roof of the school block would be finished with terracotta tiles. The hall would have a flat roof with solar panels installed on top, and orientated to face in a southerly direction. These panels would be set in from the roof edge and appear relatively inconspicuous.
- 7.31 An earlier proposal for the school was reviewed by Design Review Panel who provided a generally positive assessment towards the proposal. Key concerns related to the flexibility of the use and permeability of the buildings. This has been addressed by the application and additional doors and openings have been incorporated into the design.
- 7.32 There were comments made towards the layout of the buildings and whether there could be opportunities for the hall to be repositioned closer to the entrance, as well as the introduction of a welcome foyer to the school. The applicant has reviewed these comments and concluded that such a layout would not meet the operational day-to-day needs of the school. Whilst the welcome foyer is unlikely to be viable given that low school numbers. Planning Officers are comfortable with the applicant's conclusions.
- 7.33 Overall, the proposed school buildings and facilities would preserve and enhance the character and appearance of the Mitcham Green Conservation Area. The site's open feel would be largely retained given that the buildings would be situated around a large external play area (MUGA), with a generous woodland area maintained around the site particularly to the south. The development would be of a low height profile, and therefore the mature trees alongside those replanted by the applicant would have a significant visual presence. This would result in an attractive balance between the natural environment and physical presence of the erected buildings.
- 7.34 Subject to the recommended conditions, the proposal is considered acceptable in appearance, in compliance with London Plan policies 7.4 and 7.6, Core Strategy policy CS14 and SPP Policies DMD2, DMD3 and DMD4.

Transport and highways

- 7.35 Core Strategy Policy CS 20 considers matters of pedestrian movement, safety, servicing and loading facilities for local businesses and manoeuvring for emergency

vehicles as well as refuse storage and collection. Core Strategy Policy CS 18 promotes active means of transport, and CS 19 promotes public transport.

- 7.36 The applicant has submitted a transport statement to support the proposal, which has been reviewed by the Council's Transport Officer.

Public Transport Accessibility

- 7.37 The site lies in an area with a PTAL 3, which is considered to be a moderate rating. A moderate PTAL rating suggests that it is possible to plan regular journeys such as daily work trips or trips to and from school using public transport.

Access Road

- 7.38 The existing vehicular access to Melrose school is via an unnamed access road on to Church Road. For the proposed primary school element, access would be taken from a different access road than that used by the current school. This access road currently serves the Merton Centre for Independent Living, and adjoins the eastern boundary of Melrose School. This access road is outside the school's site boundary, but is owned by the Council.

- 7.39 The access road also provides access to Worsfold House to the south of the site. There is potential for Worsfold House to be redeveloped to create a number of new homes across the site. Therefore, it is imperative that any planned upgrading of the access road for the school is undertaken in a manner that aligns with planned future growth.

- 7.40 The applicant has submitted a plan, which has improvements to the access road that would be used by the primary school. This includes a gravel path along one side and two pelican crossings to reach the school drop off and collection area. Whilst these proposed improvements are appreciated, it is uncertain whether these upgrades could be successfully integrated into future access road improvements that would be needed by the future development at Worsfold House, given that the uplift in traffic generated by the residential development could be relatively significant. The access road is more likely to require a full resign to its entire length including reconstructed road and footway, including drainage and lighting.

- 7.41 Officers are however mindful that any planning obligations made towards the primary school development must be proportionate to the scale and nature of the development. It therefore would not be reasonable for the Council to require that the scheme deliver's access improvements greater than those required to meet the school's requirements. On the other hand, any improvements should be able to integrate with the requirements of other schemes, in this case Worsfold House, to provide a futureproofed development and one that makes efficient use of resources. Planning Officer's support the principle of the new drop off and collection area for the primary school and the development's use of the access road. However, to make the development acceptable in planning terms, improvements would be required to make the access safe, functional and inclusive.

- 7.42 It is considered that the details of the access road improvements could be ironed out in the future through dialogue between the applicant and the local planning authority. This would enable the access road to be comprehensively planned and upgraded in a manner that aligns with the needs of both the school and Worsfold House. It may equally be possible for less substantial access road improvements needed for the school to be delivered over a shorter time-frame, and integrated with more substantial redevelopment in the future. It would also allow matters such as resourcing for this work to be resolved. A pre-occupancy condition is considered

sufficient to cover these matters and has been recommended should permission be granted.

- 7.43 The applicant's transport statement includes swept path analysis testing a range of vehicle sizes including minibuses turning left into the access road from Church Road, and travelling south towards the proposed site, using the proposed turning area and car park, and returning to Church Road in a forward gear. The proposed turning area has sufficient width for car and minibus to manoeuvre however, the applicant needs to demonstrate how a rigid vehicle and fire emergency vehicle can turn within the turning area. This information has sought through a condition.

Car parking

- 7.44 Nine car parking spaces one of which would be a disabled bay parking space is proposed for the new primary school. In terms of policy, the emerging London Plan does not specifically include car parking standards for education uses. However, the 9 spaces proposed to take account of staff parking and other operational requirements is considered acceptable in terms of quantum and layout.

Cycle parking

- 7.45 As per the emerging London Plan standards, short-stay cycle parking spaces will be provided at the ratio of 1 space per 8 full time members of staff, and 1 space per 8 students. This equates to approximately 6 long-stay spaces. 2 short-stay cycle spaces will be provided in the form of Sheffield Stands in the car park area. The cycle parking provision is acceptable.

Trip Generation

- 7.46 It has been estimated that the additional trips arising from the development of the primary school will not result in a harmful impact on local highways or sustainable transport networks. However, it is noted that the transport statement has only tested the traffic movements from the scheme for 24 children, whilst the design and access statement details that the new school buildings are designed to accommodate up to 78 children. Given that the impacts of traffic from higher numbers of students to the school are unknown, a condition is necessary which requires the applicant to submit to the local authority a School Travel Plan within 6 months of occupation of the approved buildings. The development would need to operate in accordance with the approved School Travel Plan thereafter.

Refuse and Recycling

- 7.47 The primary school will continue to use the current storage and collection arrangements in place by the existing secondary school. Given that the site will be managed as a whole in terms of staffing and maintenance, it is considered that this approach is appropriate for the scale of the proposed development.

Impact upon neighbouring amenity

- 7.48 Policy DM D2 seeks to protect the amenity of residents. DM EP2 further seeks to ensure that development is sustainable and managed to minimise its impact on the environment and residential amenity by reducing and mitigating noise.
- 7.49 Consideration towards the potential impacts to neighbouring occupiers from the primary school needs to be taken in light of the current use of the land, which is occupied by the secondary school. Much of the unbuilt areas in which the school would be formed could currently be used for general play, and other school activities

that can be expected to create a reasonable volume of noise, likely to be audible from outside the school boundary.

- 7.50 The proposed arrangement of the new buildings on the site, as well as their separation from residential boundaries, would provide a noise buffer. The buildings and MUGA would also be suitably separated from the site's southern boundary where Worsfold House is located. The multi-use playground is positioned in a similar position within the site to the existing one. With consideration to the proposed building layout, alongside the predicted low number of students attending the school, it is likely that the primary school would have a similar noise profile to that of the existing school, and not be harmful to neighbouring amenity.
- 7.51 The school buildings given their separation from the site boundaries and relative heights, would not create a sense of enclosure or block light to adjacent properties.
- 7.52 The application has been reviewed by the Council's Environment and Health Officer, who has raised no objections towards the scheme. Overall, the proposal would not have a harmful impact on the living conditions of neighbouring occupants and therefore complies with Policy DM D2.

Sustainable design and construction

- 7.53 London Plan policy 5.3 and CS policy CS15 seek to ensure the highest standards of sustainability are achieved for developments which includes minimising carbon dioxide emissions, maximising recycling, sourcing materials with a low carbon footprint, ensuring urban greening and minimising the usage of resources such as water.
- 7.54 A design stage assessment has been provided demonstrating that the development has been designed to achieve BREEAM Standard Very Good in line with Merton's minimum requirements. The submitted Energy Strategy indicates that the proposed development has been designed to achieve improvements in CO2 emissions on Part L 2013 in line with the minimum sustainability requirements of Merton's Core Planning Strategy Policy CS15 (2011) and Policy 5.2 of the London Plan 2016.
- 7.55 The proposal is therefore considered to meet sustainable design and construction policies, and conditions have been recommended to secure this; including Post-Construction Review Certificate, and the development constructed in accordance with the design specifications for reducing carbon emissions.

Archaeology

- 7.56 As the subject site is within a Tier 2 Archaeological Priority Zone, it is recommended that a precautionary condition is imposed requiring an archaeological evaluation to be submitted for approval prior to the commencement of development.

Flooding and drainage

- 7.57 London Plan policies 5.12 and 5.13, CS policies CS13 and CS16 and SPP policies DMD2, DM F1 and DM F2 seek to minimise the impact of flooding on residents and the environment and promote the use of sustainable drainage systems to reduce the overall amount of rainfall being discharged into the drainage system and reduce the borough's susceptibility to surface water flooding.

- 7.58 A Drainage Strategy Report has been submitted with the application. Surface water run-off from the new buildings would be collected into gutters and thence to downpipes that connect into a surface water drainage network. This water would be discharged to ground using a series of soakaways. It is also proposed that external hard landscape areas will have permeable surfacing. Permeable asphalt has been proposed to the new parking and drop off area, whilst a range of permeable surfacing materials has been proposed for the hardstanding pedestrian areas.
- 7.59 In terms of foul water, this will be collected into main runs external to the building footprint. A new foul drainage network will then run around the building and connect to the existing foul drainage network. The new drainage system to mitigate flood risk would be achieved through ground utilising infiltration measures, finished site levels providing positive drainage, and gradients around buildings designed so that water is channelled away from building entrances. Necessary maintenance of drainage features has also been recommended in the report.
- 7.60 The application has been consulted with the Council's Flood Risk Engineers who have concluded that the flood risk from the site would be low and the sustainable urban drainage predominantly achieved through soakaways is adequate. Conditions have been recommended to secure the drainage strategy outlined within the applicant's submitted report.

Other Matters

- 7.61 A preliminary risk assessment has been submitted by the applicant to assess the likely potential of encountering unexploded ordnance (UXO) at the site. The assessment involves the consideration of the basic factors that affect the potential for UXO to be present. The risk assessment finds that Mitcham sustained a high density of bombing. A V-1 pilotless aircraft strike is plotted within the site boundary in July 1944. The report finds that it is difficult to assess potential damage sustained by the site given the lack written records covering the area available making conclusions to the level of risk to the site difficult.
- 7.62 The report recommends that further research be undertaken in the form of a Detailed UXO Risk Assessment. Further research is deemed necessary in order to confirm the exact wartime conditions of the site area and subsequently the risk that UXO could have gone unnoticed within and bordering the site boundary. A pre-commencement condition to cover unexploded ordnance has therefore been recommended.

8. CONCLUSION

- 8.1 The proposals would meet a need for which the Council has a statutory duty to fulfil; namely the provision of school places. The proposals expand an existing specialist facility in the Borough against a backdrop of similarly increased needs to provide for children with special learning needs. The design and layout has been the subject of considerable analysis by the applicant, in which a central endeavor was to ensure that impacts on the site's ecology, open space and green corridor were minimised. However, it is also acknowledged that such impacts cannot be entirely mitigated or avoided given that the new school must also be orientated, designed and configured to be functional and fit for the needs of the intended students.

- 8.2 The new buildings and associated facilities are considered to be visually attractive. The classroom block and hall would use external timber and brick detailing that would respond well to this location. The site in which the new buildings would be placed makes limited contribution to the Cricket Green Conservation Area. The proposed buildings and facilities would animate an underutilised and poorly maintained part of the borough. The proposal is considered to have a positive impact on the appearance of the area along with the wider conservation area. A sizable area of woodland, comprising mature trees, would be retained to the south of the site. Conditions have been recommended requiring that the applicant submit a replanting plan to compensate for the trees removed due to the works.
- 8.3 Other matters including transport and highways, neighboring amenity, drainage and sustainability are also acceptable and comply with council policy. Overall, the proposed development would produce significant benefits to local residents and the Borough and would be acceptable against the relevant policies of the London Plan and Local Plan. It is recommended that planning permission be granted subject to conditions

9. RECOMMENDATION

Grant planning permission subject to conditions

Conditions:

1) Standard condition [Commencement of development]: The development to which this permission relates shall be commenced not later than the expiration of 3 years from the date of this permission.

Reason: To comply with Section 91 (as amended) of the Town & Country Planning Act 1990.

2) Standard condition [Approved plans]: The development hereby permitted shall be carried out in accordance with the following approved plans: [Refer to the schedule on page 1 of this report].

Reason: For the avoidance of doubt and in the interests of proper planning.

3) No development shall take place until details of particulars and samples of the materials to be used on all external faces of the development hereby permitted (notwithstanding any materials specified in the application form and/or the approved drawings) have been submitted to the Local Planning Authority for approval. For the avoidance of doubt this condition does not include windows and doors which are covered by a pre-installation condition. No works which are the subject of this condition shall be carried out until the details are approved, and the development shall be carried out in full accordance with the approved details.

Reason: To ensure a satisfactory appearance of the development and to comply with the following Development Plan policies for Merton: policy 7.6 of the London Plan 2016, policy CS14 of Merton's Core Planning Strategy 2011 and policies DM D2 and D3 of Merton's Sites and Policies Plan 2014.

4) No development shall take place until details of all boundary walls or fences are submitted in writing for approval to the Local Planning Authority. No works which are the

subject of this condition shall be carried out until the details are approved, and the development shall not be occupied until the works to which this condition relates have been carried out in accordance with the approved details. The walls and fencing shall be permanently retained thereafter.

5) No development shall take place until a scheme providing the following ecological enhancements has been submitted in writing for approval to the Local Planning Authority. This scheme shall provide for:

- 5 bird boxes for a range of species to be installed on suitable existing trees on site.
- 5 bat boxes suitable for a range of bat species
- 2 dead wood features such as log piles, to benefit stag beetle and other invertebrates.
- 1 hedgehog home is to be installed on site to provide hibernation opportunities for hedgehogs.

No works which are the subject of this condition shall be carried out until the details are approved, and the development shall not be occupied until the enhancements to which this condition relates have been carried out in accordance with the approved details. The ecological enhancements shall be permanently retained thereafter.

Reason: To protect and enhance biodiversity, and comply with Merton's Core Planning Strategy 2011 policy CS13.

6) No development [including demolition] pursuant to this consent shall commence until an Arboricultural Method Statement and Tree Protection Plan, drafted in accordance with the recommendations and guidance set out in BS 5837:2012 has been submitted to and approved in writing by the Local Planning Authority and the approved details have been installed. The details and measures as approved shall be retained and maintained, until the completion of all site operations.

Reason: To protect and safeguard the existing retained trees in accordance with the following Development Plan policies for Merton: policy 7.21 of the London Plan 2015, policy CS13 of Merton's Core Planning Strategy 2011 and policy O2 of Merton's Sites and Policies Plan 2014.

7) Site supervision: The details of the Arboricultural Method Statement and Tree Protection Plan shall include the retention of an arboricultural expert to supervise, monitor and report to the LPA not less than monthly the status of all tree works and tree protection measures throughout the course of the construction period. At the conclusion of the construction period the arboricultural expert shall submit to the LPA a satisfactory completion statement to demonstrate compliance with the approved protection measures.

Reason: To protect and safeguard the existing retained trees in accordance with the following Development Plan policies for Merton: policy 7.21 of the London Plan 2015, policy CS13 of Merton's Core Planning Strategy 2011 and policy O2 of Merton's Sites and Policies Plan 2014.

8) No development shall take place until full details of a landscaping and planting scheme has been submitted to and approved in writing by the Local Planning Authority and these works shall be carried out as approved before the commencement of the use or the occupation of any building hereby approved, unless otherwise agreed in writing by the Local Planning Authority. The details shall include on a plan, full details of the size, species, spacing, quantities and location of proposed plants, together with any hard

surfacing, means of enclosure, and indications of all existing trees, hedges and any other features to be retained, and measures for their protection during the course of development.

Reason: To enhance the appearance of the development in the interest of the amenities of the area, to ensure the provision sustainable drainage surfaces and to comply with the following Development Plan policies for Merton: policies 5.1, 7.5 and 7.21 of the London Plan 2016, policies CS13 and CS16 of Merton's Core Planning Strategy 2011 and policies DM D2, DM F2 and DM O2 of Merton's Sites and Policies Plan 2014.

9) Prior to the commencement of the development hereby permitted, a Construction Logistics Plan shall be submitted to and approved in writing by the Local Planning Authority. The approved measures shall be implemented prior to the first occupation of the development hereby permitted and shall be so maintained for the duration of the use, unless the prior written approval of the Local Planning Authority is first obtained to any variation.

Reason: To ensure the safety of pedestrians and vehicles and the amenities of the surrounding area and to comply with the following Development Plan policies for Merton: policies 6.3 and 6.14 of the London Plan 2016, policy CS20 of Merton's Core Planning Strategy 2011 and policy DM T2 of Merton's Sites and Policies Plan 2014.

10) The development shall not commence until details of the provision to accommodate all site workers', visitors' and construction vehicles and loading /unloading arrangements during the construction process have been submitted to and approved in writing by the Local Planning Authority. The approved details must be implemented and complied with for the duration of the construction process.

Reason: To ensure the safety of pedestrians and vehicles and the amenities of the surrounding area and to comply with the following Development Plan policies for Merton: policies 6.3 and 6.14 of the London Plan 2016, policy CS20 of Merton's Core Planning Strategy 2011 and policy DM T2 of Merton's Sites and Policies Plan 2014.

11) No part of the development shall be commenced until a detailed unexploded ordnance 'UXO' Risk Assessment has been submitted to and approved in writing by the local planning authority. The risk assessment shall be prepared by a relevant expert. Alternatively, appropriate UXO Risk Mitigation Measures shall be provided for any intrusive works proposed as deemed to be necessary by a relevant expert.

REASON: The property lies within an area of the borough which may be at potential risk from buried explosive ordnance due to wartime bombing. The condition is necessary to reduce the level of UXO risk to an acceptable level.

12) A) No development shall take place until the applicant has secured the implementation of a programme of archaeological evaluation in accordance with a written scheme which has been submitted by the applicant and approved by the local planning authority in writing and a report on that evaluation has been submitted to the local planning authority.

B) If heritage assets of archaeological interest are identified by the evaluation under Part A, then before development commences the applicant shall secure the implementation of a programme of archaeological investigation in accordance with a Written Scheme of Investigation which has been submitted by the applicant and approved by the local planning authority in writing.

C) No development or demolition shall take place other than in accordance with the Written Scheme of investigation approved under Part (B).

D) The development shall not be occupied until the site investigation and post investigation assessment has been completed in accordance with the programme set out in the Written Scheme of Investigation approved under Part (B), and the provision for analysis, publication and dissemination of the results and archive deposition has been secured.

Reason. Heritage assets of archaeological interest may survive on the site. The planning authority wishes to secure the provision of appropriate archaeological investigation, including the publication of results, in accordance with Section 12 of the NPPF.

13) Prior to the commencement of works on the site the applicant must provide a biosecurity plan to the local planning authority that includes details of identification posters, toolbox talks and vetting of contractors to ensure awareness of invasive plant species within the site.

Reason: To protect and enhance biodiversity, and comply with Merton's Core Planning Strategy 2011 policy CS13.

14) Unless otherwise agreed in writing by the Local Planning Authority, no part of the development hereby approved shall be used or occupied until a Post Construction Review Certificate issued by the Building Research Establishment or other equivalent assessors confirming that the non residential development has achieved a BREEAM rating of not less than the standards equivalent to 'Very Good', and evidence demonstrating that the development has achieved CO2 reductions in accordance with those outlined in the approved Energy Statement (dated 24th September 2020).

Reason: To ensure that the development achieves a high standard of sustainability and makes efficient use of resources and to comply the following Development Plan policies for Merton: policy 5.2 of the London Plan 2016 and policy CS15 of Merton's Core Planning Strategy 2011.

15) The development hereby permitted shall not be occupied until the drainage strategy as detailed within the submitted 'Proposed Drainage Strategy' dated May 2020, has been carried out in full. This shall include the flood risk management measures stated in the strategy. The measures above shall be consistent with the technical drawings provided in this strategy.

Reason: to reduce the risk of surface and foul water flooding to the proposed development and future users, and ensure surface and foul flood risk does not increase offsite in accordance with Merton's policies CS16, DMD2 & DMF2 and the London Plan policy 5.13.

16) The development shall not be occupied until a community use agreement has been submitted to and approved in writing by the Local Planning Authority. The agreement shall apply to the hall and multi games area, parking area, toilets and changing facilities. It shall include details of pricing, policy, hours of use, access by non-educational establishment users, management responsibilities, and a mechanism for review. The development shall not be used otherwise than in strict compliance with the approved agreement.

Reason to secure well managed safe community access to the sports facilities, to ensure sufficient benefit of the development of sport, as well as to enable public access onto open space. Therefore, to comply with Policy 3.19 of the London Plan, Policy CS 13 of the Core Strategy 2011, and Policy DM O1 of the Sites and Policies Plan 2014.

17) A School Travel Plan including management arrangements for the picking up and dropping of children shall be submitted to and approved by the Local Planning Authority, within 6 months of first use of the buildings hereby approved. The School Travel Plan shall include details concerning the number of children to attend the primary school alongside the vehicle movements generated. The development shall operate in full accordance with the approved School Travel Plan.

Reason: To promote sustainable travel, and to ensure that the number of vehicle movements created by the proposal maintains the safe and efficient operation of the highway network, and to therefore comply with the following Development Plan policies for Merton: policy 6.3 of the London Plan 2016, policies CS18, CS19 and CS20 of Merton's Core Planning Strategy 2011 and policy DM T2 of Merton's Sites and Policies Plan 2014.

18) The development shall not be occupied until suitable details of the proposed vehicular and pedestrian access (including any interim arrangements that may be necessary pending redevelopment of adjoining land to the east) to serve the development have been submitted in writing for approval to the Local Planning Authority. This shall include details of the improvements to the unnamed access road on to Church Road used to reach the new drop-off and collection area. In addition, the details shall demonstrate how a rigid vehicle and fire emergency vehicle can turn within the school's new turning area. The development shall not be occupied until any interim arrangements forming part of the wider vehicle and pedestrian access scheme have been implemented.

Reason: In the interests of the safety of pedestrians and vehicles and to comply with the following Development Plan policies for Merton: policies CS18 and CS20 of Merton's Core Planning Strategy 2011 and policies DM T2, T3, T4 and T5 of Merton's Sites and Policies Plan 2014.

19) No external lighting shall be installed without the prior approval in writing of the Local Planning Authority.

Reason: To safeguard the amenities of the area and the occupiers of neighbouring properties, and to protect and enhance biodiversity. To ensure compliance with the following Development Plan policies for Merton: policies DM D2 and DM EP4 of Merton's Sites and Policies Plan 2014, and Merton's Core Planning Strategy 2011 policy CS13.

20) Before the relevant part of the work, details of particulars and/or samples of the external window frames and doors shall be submitted to the Local Planning Authority for approval. No works which are the subject of this condition shall be carried out until the details are approved, and the development shall be carried out in full accordance with the approved details.

21) The development shall be constructed in accordance with the approved design specification for reducing carbon emissions.

Reason to reduce CO2 emissions and to comply with policy 5.2 of the London Plan.

22) The development shall be fully completed in accordance with the recommendations, methodology and findings detailed within 'Preliminary Ecological Appraisal (March 2020)' and 'Preliminary roost Assessment an Nocturnal Bat Surveys (July 2020)' reports. An ecologist shall also be present when site clearance is taking place, to safeguard fauna on site.

Reason: To protect and enhance biodiversity, and comply with Merton's Core Planning Strategy 2011 policy CS13.

23) The vehicle parking area including the drop-off and collection area shall be provided before the occupation of the buildings hereby permitted, and shall be retained for the users of the development and for no other purpose.

Reason: To ensure the provision of a satisfactory level of parking and comply with the following Development Plan policies for Merton: policy 6.13 of the London Plan 2016, policy CS20 of Merton's Core Planning Strategy 2011 and policy DM T3 of Merton's Sites and Policies Plan 2014.

24) The development hereby permitted shall not be occupied until the cycle parking shown on the plans hereby approved has been provided and made available for use. These facilities shall be retained for the occupants of and visitors to the development at all times.

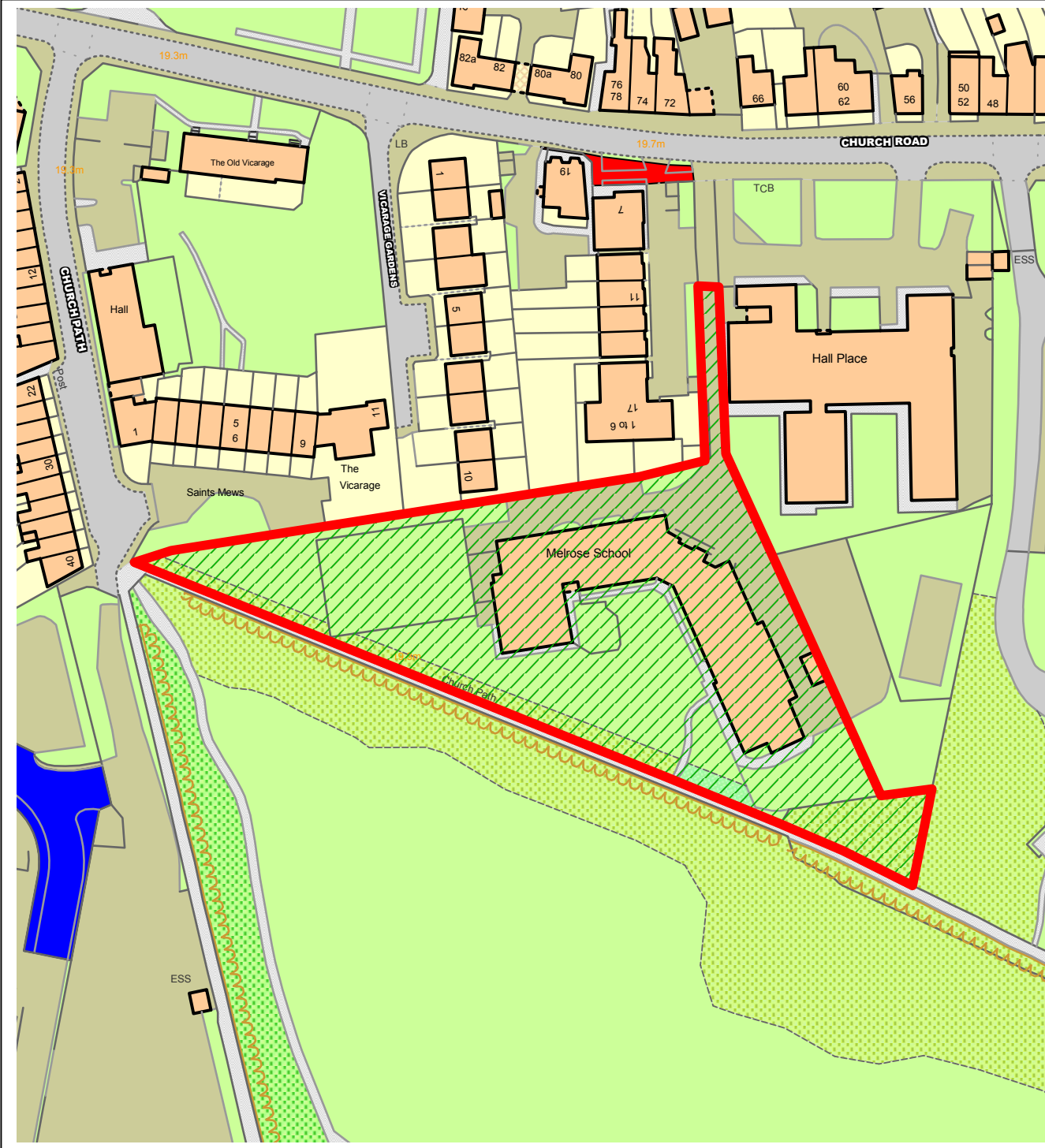
Reason: To ensure satisfactory facilities for cycle parking are provided and to comply with the following Development Plan policies for Merton: policy 6.13 of the London Plan 2016, policy CS18 of Merton's Core Planning Strategy 2011 and policy DM T1 of Merton's Sites and Policies Plan 2014.

25) The development hereby approved shall not be used/occupied until such time as the applicant has installed the green roof and solar panels as described on the approved plans.

Reason. To ensure that the development contributes towards mitigating against climate change, to contribute towards biodiversity and to comply with London Plan policies 5.1, 5.2 and 5.3 and Merton Core Planning Strategy policy CS15 , to reduce flood risk and to comply with London plan policy 5.12, Merton Core Planning Strategy policy CS16, and Merton sites and Policies plan policy DM.F2, and to safeguard the biodiversity quality of the locality, and to ensure compliance with Sites and policy DM D2 and DM.O2 and CS13 and CS14 of the Merton Core Planning Strategy 2011.

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NORTHGATE SE GIS Print Template

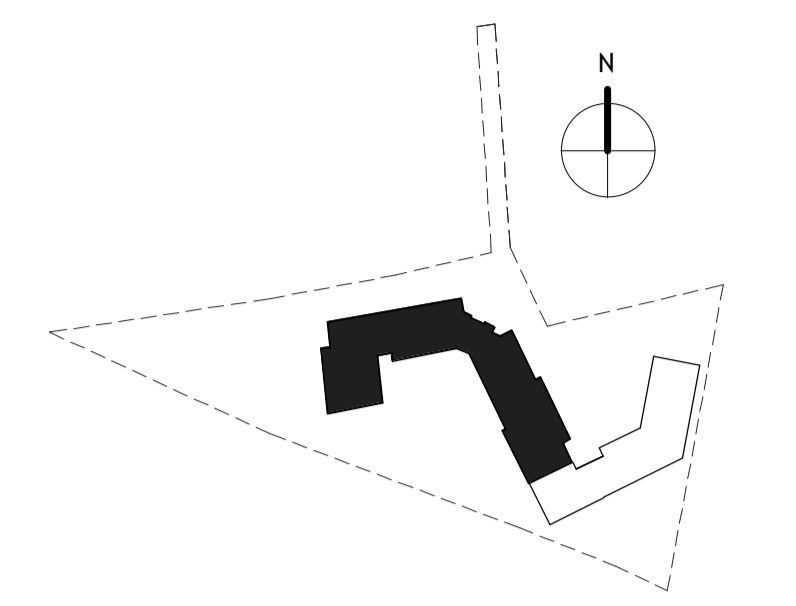


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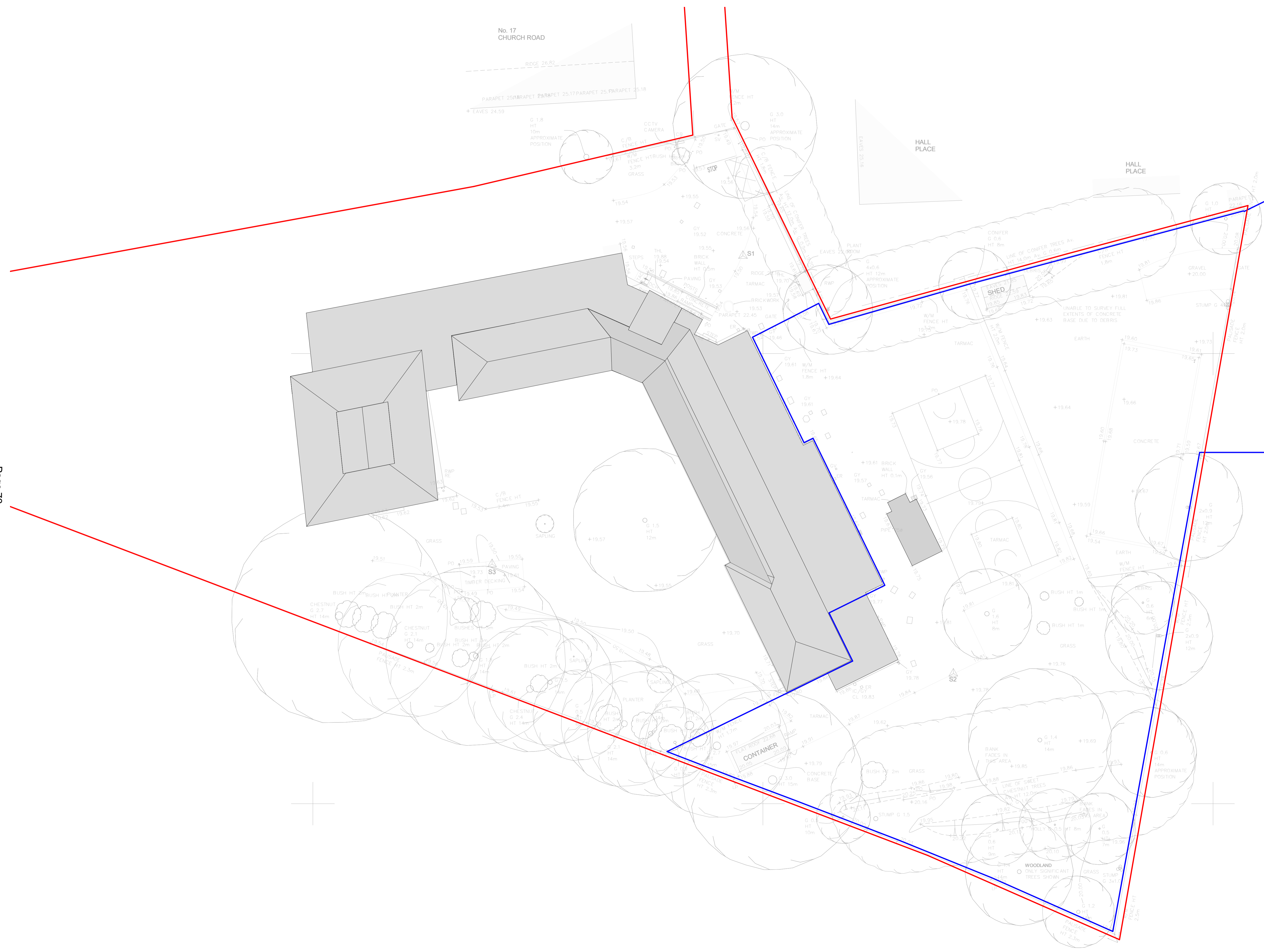
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KEY:
— Site Application Boundary
— Extent of Works

Survey by MK Surveys
Topographical and PAS 128:2014 Utility Survey
Project Number: 28274
Date: February 2020



P5	Updates from DRP incorporated	24.08.20
P4	Planning Amendments Issue	24.07.20
P3	Planning Submission	19.06.20
P2	Planning Issue	04.06.20
P1	Draft Planning Issue	02.06.20

Revision	Amendment	Date
CI	MEPB	MB
Drawn by	Reviewed by	Approved by
61207	MAY 20	As indicated @ A1
MCB Number	Date Created	Scale @ A1

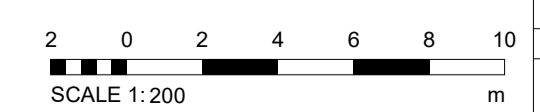
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Client
Melrose School

Project
MELROSE SCHOOL
Church Rd, Mitcham CR4 3BE

Drawing Title
EXISTING SITE PLAN
TOPOGRAPHICAL SURVEY

ARCHITECTURE

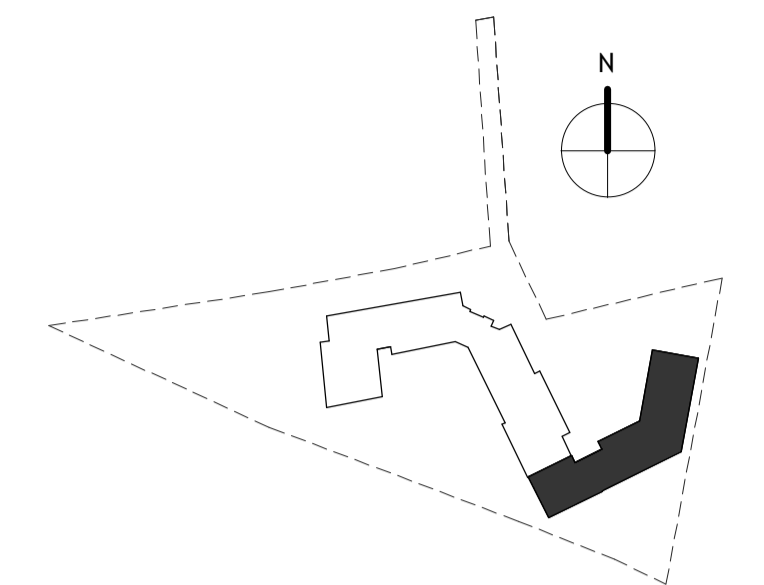


MELO1	MCB	SI	ZZ	DR	A	0101	D5	P5
Project Mem	Originator	Zone	Level	Type	Discipline	Number	Suitability	Revision

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- KEY:**
- Site Application Boundary
 - Extent of Works
 - Wall - Existing to be demolished
 - Site - Fence
 - Existing trees to retain
 - Tree to be removed
 - ▲ Main Entrance
 - Area of Refurbishment
 - Out of Scope

P8	Updates from DRP incorporated	24.08.20
P7	Planning Amendments Issue	24.07.20
P6	Planning Submission	19.06.20
P5	Planning Issue	04.06.20
P4	Draft Planning Issue	02.06.20
P3	Issued for Information	26.05.20
P2	Issued for Information	06.05.20
P1	Internal Review	01.04.20

Revision	Amendment	Date
CI	MEPB	MB
Drawn by	Reviewed by	Approved by
61207	MAR 20	As indicated @ A1
MCB Number	Date Created	Scale @ A1

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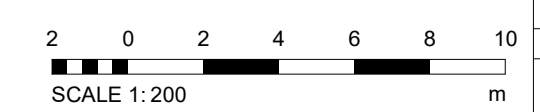
Client
Melrose School

Project
MELROSE SCHOOL
Church Rd, Mitcham CR4 3BE

Drawing Title
PROPOSED SITE PLAN

ARCHITECTURE

Drawing / Document Reference						Status
MEL01	MCB	SI	ZZ	DR	A	0102
Project Id	Originator	Zone	Level	Type	Discipline	Number
						D5 - P8
Project Id	Originator	Zone	Level	Type	Discipline	Number



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Revision

Revision	Amendment	Date
P7	Updates from DRP incorporated	24.08.20
P6	Issued for Information	14.08.20
P5	Planning Submission	19.06.20
P4	Planning Issue	04.06.20
P3	Draft Planning Issue	02.06.20
P2	Issued for Information	26.05.20
P1	Issued for Information	06.05.20

Revision

Revision	Amendment	Date
CI	MEPB	MB
Drawn by	Reviewed by	Approved by
61207	APR 2020	As indicated @ A1
MCB Number	Date Created	Scale @ A1

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Project
MELROSE SCHOOL
Church Rd, Mitcham CR4 3BE

Drawing Title
PROPOSED GROUND FLOOR PLAN

ARCHITECTURE

SCALE 1:100

1 Proposed Ground Floor Plan
1 : 100

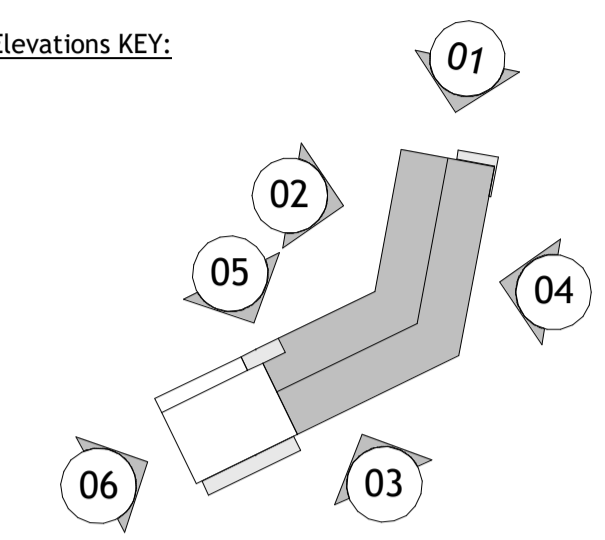
Project Information:
Drawing / Document Reference: MEL01 - MCB - XX - 00 - DR - A - 0115
Status: D5 - P7
Project Name: Originator: Zone: Level: Type: Discipline: Number: Suitability: Revision:

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Elevations KEY:

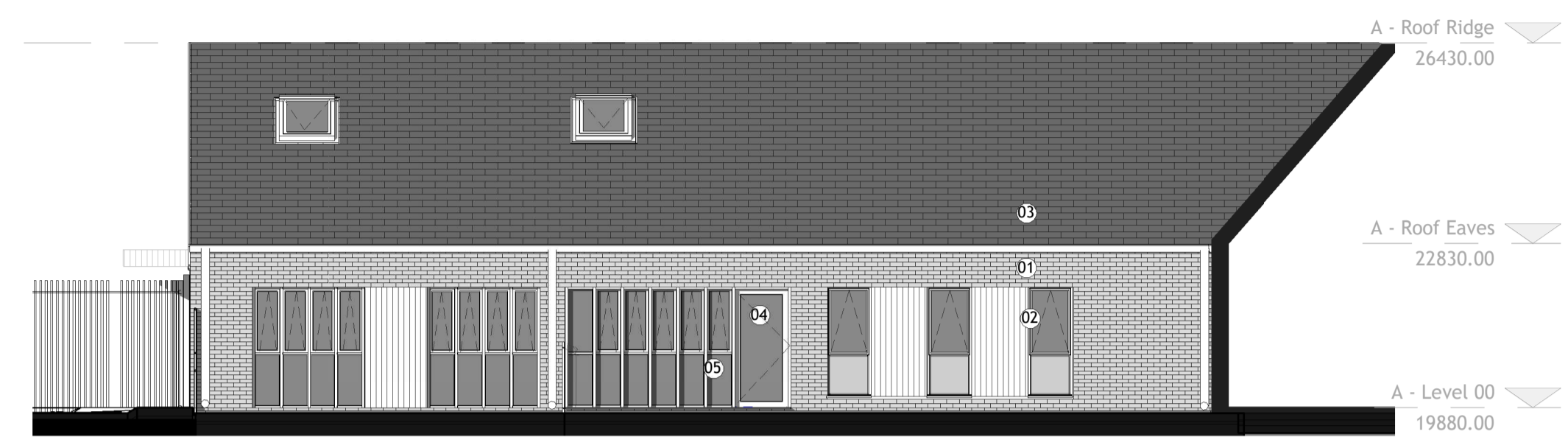


Building Materials KEY:

- 01 External walls: Red / Brown facing brick in stretcher bond with smaller panels in English / Flemish bond.
- 02 Wall Cladding: Timber panel cladding Thermowood or similar.
- 03 Roof: Terracotta Pantile
- 04 Glazed openings: Aluminum curtain walling system
- 05 Solar Shading solution: Timber fins.



1 North East Elevation
1 : 100



2 West Elevation
1 : 100

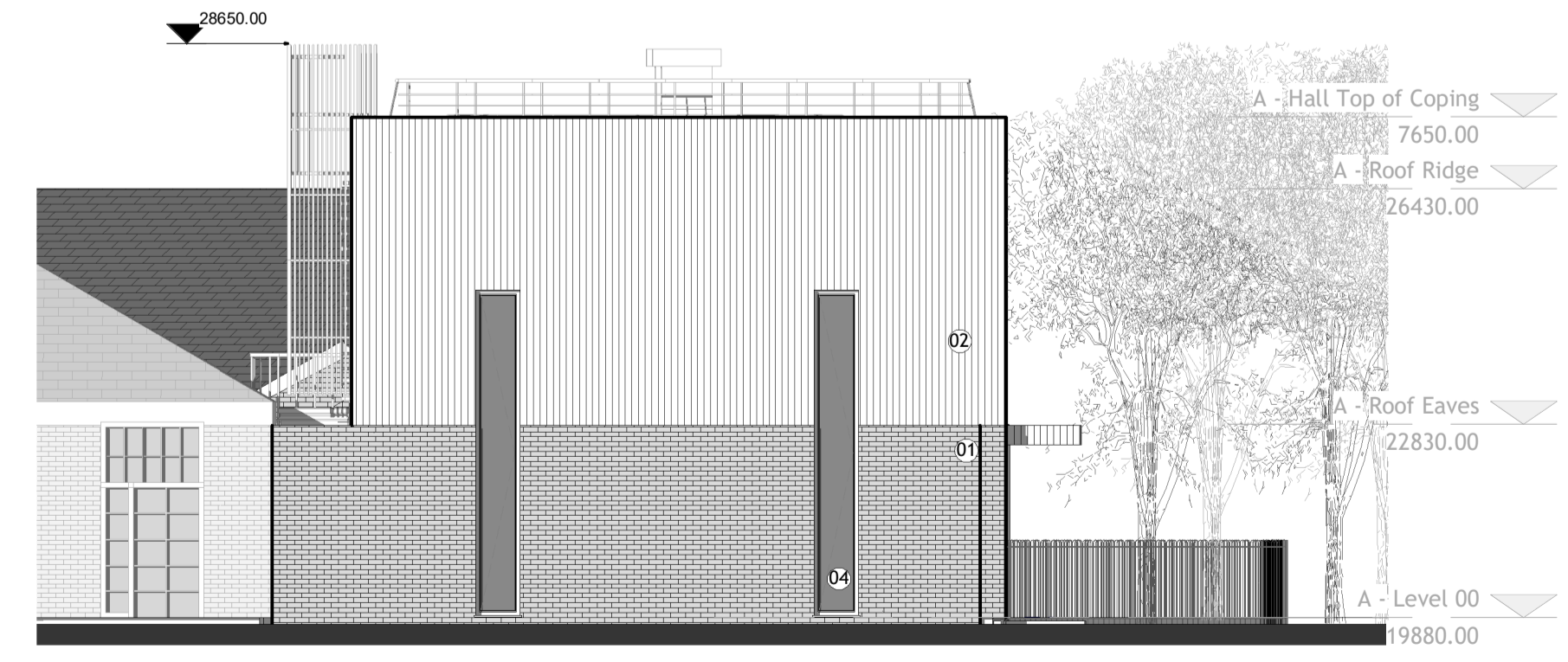


3 South East Elevation
1 : 100

4 East Elevation
1 : 100



5 North Elevation
1 : 100



6 South West Elevation
1 : 100

Revision	Amendment	Date
P7	Updates from DRP incorporated	24.08.20
P6	Planning Submission	19.06.20
P5	Planning Issue	04.06.20
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P3	Issued for Information	26.05.20
P2	Issued for Information	06.05.20
P1	Issued for Information	04.05.20

CI	MEPB	MB
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61207	APR 20	As indicated @ A1
MCB Number	Date Created	Scale @ A1

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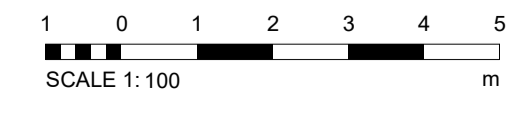
Client
Melrose School

Project
MELROSE SCHOOL
Church Rd, Mitcham CR4 3BE

Drawing Title
PROPOSED ELEVATIONS

ARCHITECTURE

Drawing / Document Reference	Status							
MEL01 - MCB - XX - XX - DR - A - 0125	D5 - P7							
Project Name	Originator	Zone	Level	Type	Discipline	Number	Suitability	Revision

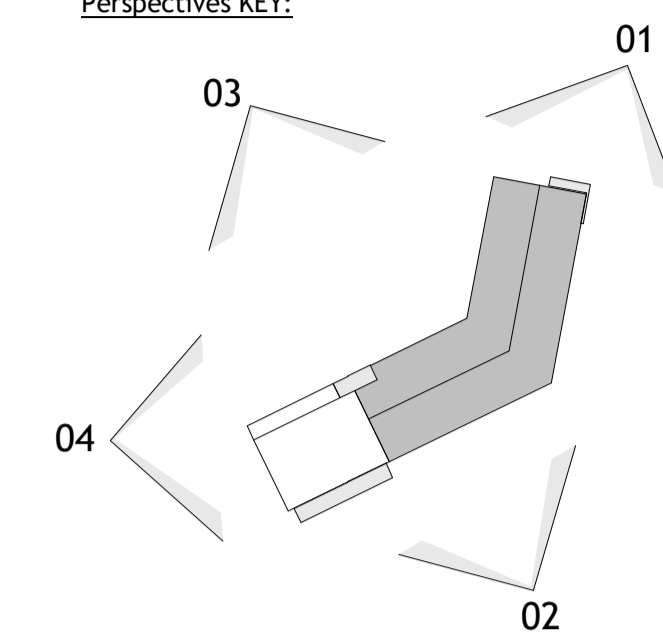


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Notes
Contractors must verify all dimensions on site before commencing any work on shop drawings Do not scale from this drawing
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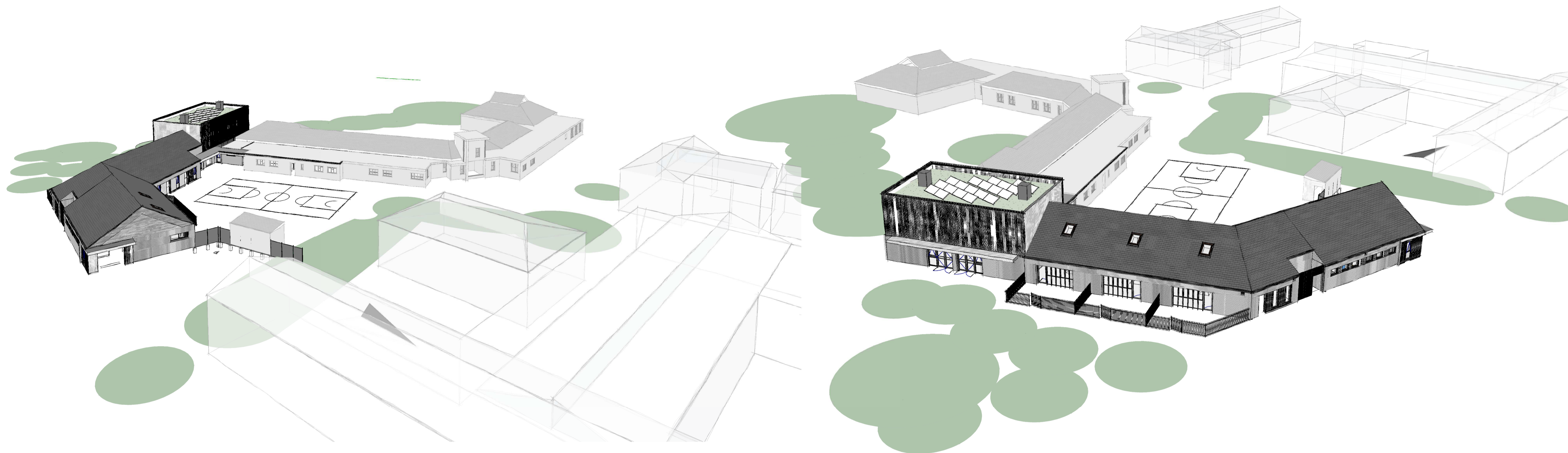
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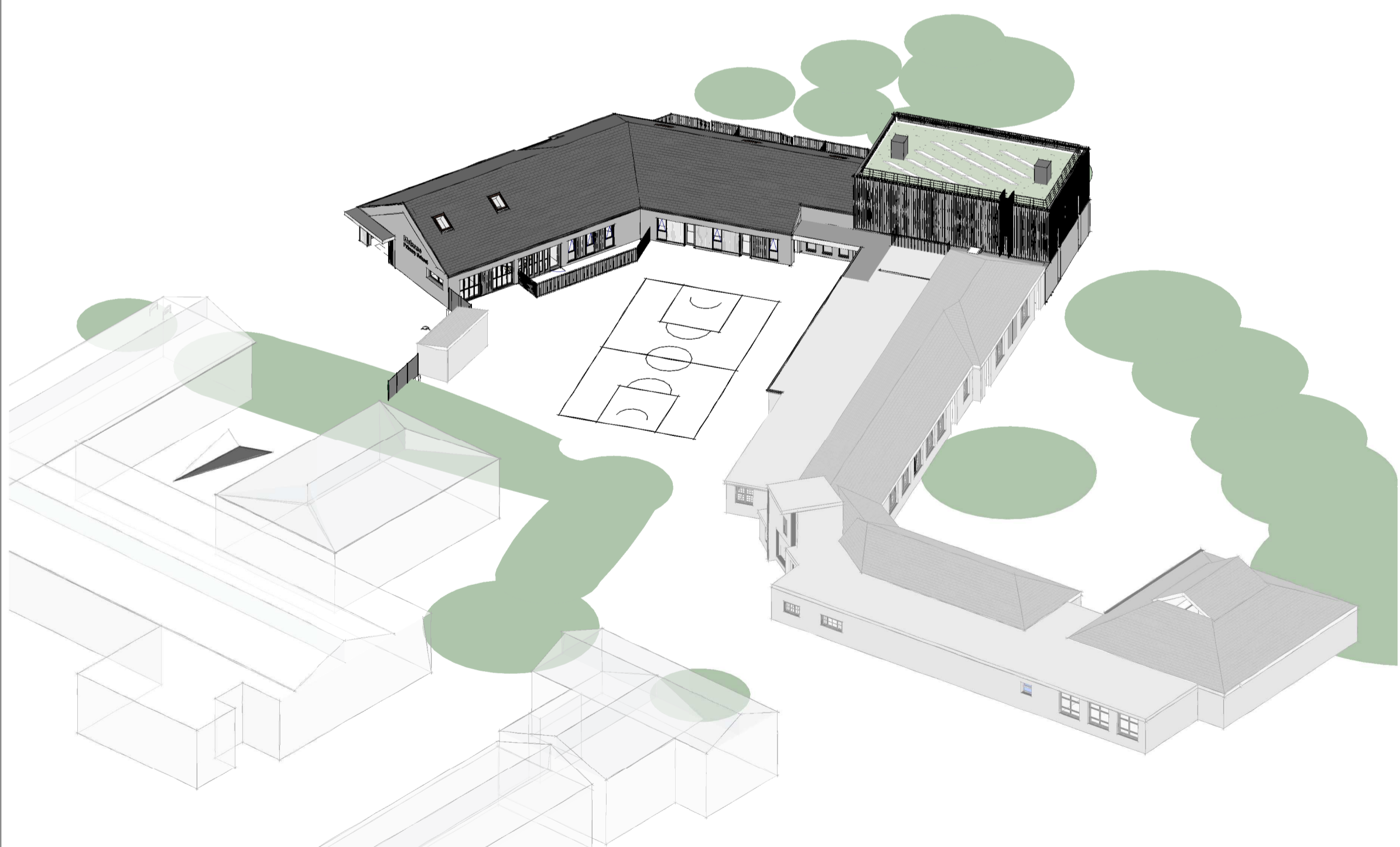
KEY:

- Site Application Boundary
- Extent of Works

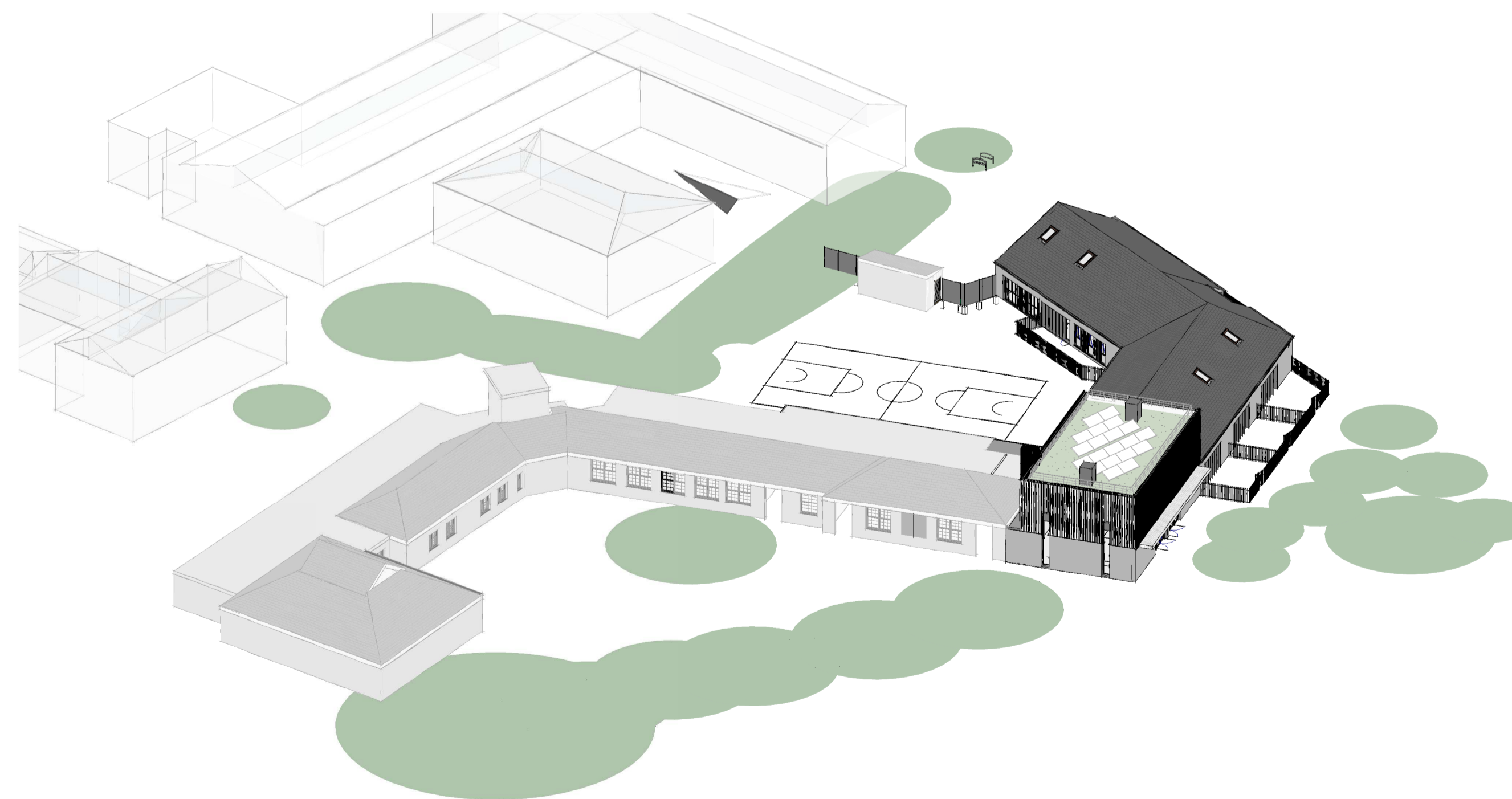


Perspective View 01 - North East view

Perspective View 02 - South East view



Perspective View 03 - North view



Perspective View 04 - West view

P7	Updates from DRP incorporated	24.08.20
P6	Planning Submission	19.06.20
P5	Planning Issue	04.06.20
P4	Draft Planning Issue	02.06.20
P3	Issued for Information	26.05.20
P2	Issued for Information	06.05.20
P1	Internal Review	01.04.20

Revision	Amendment	Date
CI	MEPB	MB
Drawn by	Reviewed by	Approved by
61207	APR 20	As indicated @ A1
MCB Number	Date Created	Scale @ A1

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Client
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Project
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Church Rd, Mitcham CR4 3BE

Drawing Title
AERIAL VIEWS

ARCHITECTURE

Drawing / Document Reference							Status
MEL01	MCB	XX	XX	DR	A	0150	D5 - P7
Project Idn	Originator	Zone	Level	Type	Discipline	Number	Suitability Revision

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PLANNING APPLICATIONS COMMITTEE
22nd October 2020

<u>UPRN</u>	<u>APPLICATION NO.</u>	<u>Item No:</u> <u>DATE VALID</u>
	20/P2547	11/08/2020
Address/Site	101 Hamilton Road, South Wimbledon, SW19 1JG	
Ward	Abbey	
Proposal:	Erection of a two storey terrace building comprising 5 residential units (3 x 5 bedroom houses, 1 x 2 bedroom flat and 1 x 3 bedroom flat) with associated works, including outbuildings, landscaping, car parking and cycle/bin storage.	
Drawing Nos	2003-A0-010 Rev P1, 2003-A4-010 Rev P1, 2003-A4-020 Rev P1, 2003-A4-030 Rev P2, 2003-A4-040 Rev P2, 2003-A4-110 Rev P1, 2003-A4-120 Rev P1, 2003-A4-130 Rev P2, 2003-A-4-210 Rev P2, 2003-A-4-220 Rev P1 and 2003-A-4-230 Rev P1.	
Contact Officer:	Stuart Adams (0208 545 3147)	

RECOMMENDATION

GRANT Planning Permission subject to conditions and S106 agreement

CHECKLIST INFORMATION.

Heads of agreement: - Permit Free (both flats and 1 permit restriction to each of the houses), Highways works and Car Club

Is a screening opinion required: No

Is an Environmental Statement required: No

Has an Environmental Impact Assessment been submitted – No

Press notice – Yes

Site notice – Yes

Design Review Panel consulted – No

Number of neighbours consulted – 46

External consultations – No.

1. **INTRODUCTION**

- 1.1 The application has been brought before the Planning Applications Committee for consideration due to the nature and number of objections received.

2. **SITE AND SURROUNDINGS**

- 2.1 The application site has now been cleared and formally the site comprised a two storey detached property known as Rose Cottage and single storey light industrial units within the rear section of the site. Formally, some ad hoc car parking was provided on site, to the front and side of the former buildings.
- 2.2 To the north of the application site is a two storey Victorian detached property, known as 97 Hamilton Road, with a terrace of similar two storey properties beyond, characterised by two storey projecting bays and recessed porches. Number 97 has been split into two flats. The rear garden area has been subdivided into two, with the upper floor flats having direct access via an external rear staircase along the northern boundary of the application site. The blank flank wall of no.97 forms the northern boundary of the application site.
- 2.3 Directly to the south of the application site is the rear of a two storey building known as 206 – 212 Merton High Street. This building comprises commercial uses at ground floor and flats at the first floor level. A gated rear passageway separates the application site from the rear wall of this neighbouring building. Its main frontage is onto Merton High Street, one of the main thoroughfares within the Borough, characterised by two-/three storey buildings with commercial units at ground floor and residential units on the floors above.
- 2.4 The surrounding area comprises a mixture of residential and commercial properties. The application site is situated on one of the residential streets, at right angles to Merton High Street. These residential streets, are predominantly characterised by traditional two storey terraced housing.
- 2.5 The application site is not located within a Conservation Area.

3. **CURRENT PROPOSAL**

- 3.1 Erection of a two storey terrace building comprising 5 residential units (3 x 5 bedroom houses, 1 x 2 bedroom flat and 1 x 3 bedroom flat) with associated works, including outbuildings, landscaping, car parking and cycle/bin storage.
- 3.2 The proposed application seeks full planning permission for the redevelopment of the site to provide five new residential properties, extending to 2 storeys with accommodation in the roof. The scheme comprises three five-bedroom houses, one two-bedroom flat and one three-bedroom flat. Each unit will have access to private amenity space in the form of a garden or balcony/terrace.
- 3.3 The proposed materials include yellow brick and slate roof tiles to the building, which matches the neighbouring buildings and dark grey window frames, which will ensure it sits comfortably within its surrounding area.
- 3.4 The floor space (GIA) and amenity space standards of individual residential units are as follows compared to London Plan 2016 requirements and Merton planning policy DM D2 (Design considerations in all developments):

<u>Proposal</u>	<u>Type(b)bed (p) person</u>	<u>Proposed GIA</u>	<u>London Plan</u>	<u>Amenity Space (sq m)</u>	<u>London Plan/ Merton requirement</u>
Flat 1	2b4p	70.1	70	22	7
Flat 2	3b6p	109.1	102	11.5	9
House 1	5b10p	184.6	152	50	50
House 2	5b10p	184.6	152	50	50
House 3	5b10p	191.7	152	50	50

- 3.5 All residents will have access to private amenity space comprising of 50sqm private rear garden and front garden with landscaping, cycle storage and bin storage. The front gardens will be enclosed by a brick wall with railings above and pedestrian gates. For the flats, the ground floor flat will have access to a garden and the upper floor flat will have access to balcony and terrace. Both flats will have over 20 sqm private amenity space.
- 3.6 Cycle parking will be provided for each property. For the 3 houses, this will comprise a secure cycle store (2 cycle spaces) located in the front garden. The flats will also benefit from a secure cycle store in the front garden, with space for 4 cycles in total (2 spaces per flat).
- 3.7 The proposed scheme will remove the existing vehicle crossover on the site boundary with Hamilton Road and reinstate two on-street parking

bays for use by permitted local residents, including residents of the scheme who will be able to apply for parking permits (limitations outlined within committee report).

4. **PLANNING HISTORY**

- 4.1 19/P0883 - Erection of a two storey detached building with accommodation at roof and basement level comprising 13 flats (5 x 1, 6 x 2 and 2 x 3 bedroom flats) and associated works – Refused on 06/09/2019 for the following reasons:

The proposed development by reason of its design, height, massing and siting would be an overly bulky and dominant form of development, resulting in an overdevelopment of the site, failing to respect the Hamilton Road street scene and general pattern of development within the area, contrary to Policies DM D2 (Design Considerations in all developments) of Merton's Adopted Sites and Policies Plan (July 2014), CS14 (Design) of Merton's adopted Core Planning Strategy (July 2011) and Policy 7.4 (Local Character) of the London Plan (2016).

In the absence of a signed legal agreement, the applicant has failed to secure the agreed affordable housing contribution of £40,000 and details relating to early and late stage viability reviews contrary to policies DM H3 (support for affordable housing) of Merton's Adopted Sites and Policies Plan (July 2014), CS8 (Housing Choice) of Merton's Adopted Core Planning Strategy (July 2011), 3.12 (Negotiating Affordable Housing on Individual Private Residential and Mixed Use Schemes) of the London Plan (March 2016), Merton's Development Viability SPD 2018 and the Mayor of London Affordable Housing and Viability SPG 2017.

The proposed development, located in an area with a PTAL score of 5 (very good), would generate additional pressure on parking in the area, and in the absence of a signed legal agreement securing a 'car free' agreement (restriction on parking permits only relate to the 1 and 2 bedroom flats), the proposal would be contrary to contrary to Policies DM T1 (support for sustainable transport and active travel), DM T3 (Car parking and servicing standards) of Merton's Adopted Sites and Policies Plan (July 2014) and CS20 (Parking, Servicing and Delivery) of Merton's Adopted Core Planning Strategy (July 2011).

In the absence of a signed legal agreement, the applicant has failed to secure the free Car Club membership (3 years) for future occupiers of the development, contrary to Policies DM T1 (support

for sustainable transport and active travel), DM T2 (Transport impacts of development), DM T3 (Car parking and servicing standards) and DM T5 (Access to the Road Network) of Merton's Adopted Sites and Policies Plan (July 2014) and CS20 (Parking, Servicing and Delivery) of the Core Strategy 2011.

In the absence of a signed legal agreement, the applicant has failed to secure the agreed carbon off-set payment of £20,128, contrary to policies CS15 (Climate Change) of Merton's adopted Core Planning Strategy (2011) and policy 5.2 (Minimising Carbon Dioxide emissions) of the London Plan (2016).

In the absence of a signed legal agreement, the applicant has failed to secure the removal the existing crossovers and provision of 2 on-street car parking bays (including that the developer to meets the costs of implementation and requirement for separate S278 highway agreement), contrary to policies DM T2 (Transport impacts of development) and DM T3 (Car parking and servicing standards) of Merton's Adopted Sites and Policies Plan (July 2014) and CS20 (Parking, Servicing and Delivery) of the Core Strategy 2011.

- 4.2 17/P3242 - Redevelopment of the site (including demolition of existing buildings) and erection of a two storey terrace with accommodation at basement and roof level (6 x 2 bed flats & 2 x 3 bed flats) and 1 x two storey dwelling house at rear and associated landscaping and parking – Appealed non determination – Appeal dismissed on 6th June 2018 (Appeal ref - APP/T5720/W/17/3189000). The Council included the following reasons for refusal in the appeal application:

The proposal would result in the loss of an scattered employment site. The proposal would therefore be contrary to policies DM E3 (Protection of scattered employment sites) of Merton's Sites and Policies Plan and CS 12 (Economic Development) of the of Merton's Core Planning Strategy (July 2011).

The proposed residential units due to their design, form, layout, access and quantum of development would fail to achieve high quality design that relates positively and appropriately to surrounding buildings, urban layout and landscape features. The proposal would therefore be contrary to policies DM D2 (Design considerations in all developments) of Merton's Sites and Policies Plan and CS 14 (Design) of Merton's Core Planning Strategy (July 2011).

The proposed two storey house by reason of its design, height and siting would be an un-neighbourly form of development that would

result in a sense of enclosure and poor outlook, which would be detrimental to the residential amenities of flat 2, 101 Hamilton Road, contrary to policies DM D2 (Design Considerations in all developments) of Merton's Adopted Sites and Policies Plan (July 2014) and CS14 - Design of Merton's adopted Core Planning Strategy (July 2011).

The proposed terrace and two storey house by reason of its design, height and siting would be an un-neighbourly form of development that results in loss of privacy (from 1st floor terrace of flat 5), overly dominant structure and sense of enclosure to the rear amenity area of 97 b Hamilton Road, contrary to policies DM D2 (Design Considerations in all developments) of Merton's Adopted Sites and Policies Plan (July 2014), and CS14 – (Design) of Merton's adopted Core Planning Strategy (July 2011).

The proposed terraces flank wall by reason of its design, height, massing and siting would be an un-neighbourly form of development that would result in poor outlook and sense enclosure, which would be detrimental to the residential amenities of 208 – 210 Merton High Street, contrary to policies DM D2 Design Considerations in all developments of Merton's Adopted Sites and Policies Plan (July 2014), and CS14 - Design of Merton's adopted Core Planning Strategy (July 2011).

The proposed two storey house by reason of its design, height, massing and siting would be an un-neighbourly form of development that would result in poor outlook and sense enclosure, which would be detrimental to the residential amenities of 111 Hardy Road, contrary to policies DM D2 Design Considerations in all developments of Merton's Adopted Sites and Policies Plan (July 2014), and CS14 - Design of Merton's adopted Core Planning Strategy (July 2011).

The proposed development would generate additional pressure on parking in the area, and in the absence of a legal agreement securing a car free agreement, the proposal would be contrary to policy CS20 (Parking, Servicing and Delivery) of the Adopted Core Planning Strategy (July 2011).

The proposed terrace by reason of its design and layout would fail to achieve a high standard of residential accommodation with poor outlook from habitable rooms at basement and roof levels. The proposal would therefore be detrimental to the amenities of future occupiers contrary to policies DM D2 Design considerations in all

developments of Merton's Sites and Policies Plan and CS 14 (Design) of Merton's Core Planning Strategy (July 2011.)

The applicant has failed to demonstrate that the proposed development has suitable flood prevention/mitigation measures. The proposal would therefore be contrary to policies DM D2 Design considerations in all developments, DM F1 (Support for flood risk management), DM F2 (Sustainable Urban drainage system (SUDS) and; wastewater and water infrastructure) of Merton's Sites and Policies Plan and CS 16 (Flood Risk Management) of Merton's Core Planning Strategy (July 2011.)

- 4.3 16/P4444 - Prior notification for proposed demolition of a two storey detached residential building (rose cottage) – Approved - 13/12/2016
- 4.4 16/P3729 - Prior notification for proposed demolition of a two storey detached residential building (rose cottage) – Refused - 21/10/2016
- 4.5 15/P3573 - Renovation of existing rose cottage to create 4 self contained flats including erection of two storey rear extension, erection of new semi detached house (adjoining 97 Hamilton Road) and erection of new detached two storey house at rear of site – Grant - 05/12/2016
- 4.6 14/P2350 - Demolition of existing building and erection of a new two-storey building at front and part 1, part 2 storey building at rear comprising 9 self-contained flats – Withdrawn.
- 4.7 13/P0997 - Demolition of existing building and erection of a new two-storey building comprising 9 x 2 bed self-contained flats and a part single, part two storey building at rear for b1 office use - Withdrawn
- 4.8 12/P2520 - Application for a certificate of lawfulness in respect of the existing use of property as residential (Class C3) – Issued - 14/12/2012
- 4.9 MER791/70 - Established use certificate for light industrial use – Grant - 02/11/1970
- 4.10 MER471/69 - Vehicular access – Grant - 03/09/1969

5. **CONSULTATION**

- 5.1 The application has been advertised by major site notice procedure and letters of notification to the occupiers of neighbouring properties.
 - 5.1.1 In response to consultation, 11 objections received. The letters raise the following objections:

Highway

- Parking in Hamilton Rd is already under severe pressure for residents of Hamilton Road. Adding flats/housing to this end of the road (closest to the station and under even more pressure than the rest of the road for the limited parking spaces available) without adequate parking allocated for the additional properties is not acceptable.
- The developers should ensure that adequate parking is built into the site (i.e. off road) rather than on road. Residents living in these proposed properties should not be able to obtain parking permits for the area.
- Residents living in these properties should not be allowed to be able to obtain parking permits for the area. i.e. Merton Council must agree to an indefinite no parking order
- Additional parking needs created by 5 large residential units would be harmful to highway safety and result in an increase in parking stress and consequent illegal or unsafe parking. There is a lack of spaces presently so any additional would be detrimental to the current residents as well as any future.
- It is worth noting this end of the road is closed and is used by local businesses to park as well as a turning point all adding to the parking shortage
- The proposed development is large and could conceivably, without restrictions, lead to 6 to 10 parking permits being sought. Prior use of the site is not a relevant consideration given the change in car ownership generally since the site was last occupied and its previous business use.
- For the reasons given above, allowing resident permits for the development would lead to considerable pressure on parking. Given this is a no through road this would likely cause issues with road safety as cars would increasingly need to do 3 point turns to look for parking on other streets, often in the dark, as early evening is when there is most strain on parking.
- Whilst 2 new parking spaces are proposed outside the property, this is wholly inadequate

Design

- The road facing roof top dormers are not in keeping with the look and feel of the Victorian terraced housing on the road. I would urge a redesign of this aspect.
- This application represents over development of the site which would be detrimental to the surrounding area.

- The design of the terrace buildings and flats are not in keeping to the character and appearance of the road
- The large dormer windows will look completely out of place and harmful to the in keeping of the street, they will look out of place and not fit in with the surrounding houses (of which I live in one across the road). It is a far cry from the original Rose Cottage that previously occupied the site.

Other

- The noise of the works

5.2 Environmental Health – No objection subject to conditions

A deskstudy, then an investigation shall be undertaken to consider the potential for contaminated-land, and if necessary, a detailed remediation scheme to bring the site to a suitable state for the intended use by removing unacceptable risks to health and the built environment, and submitted to the approval of the LPA. Reason: To protect the health of future users of the site in accordance with policy 5.21 of the London Plan 2015 and policy DM EP4 of Merton's sites and policies plan 2014.

The approached remediation shall be completed prior to development. And a verification report, demonstrating the then effectiveness of the remediation, subject to the approval of the LPA. Reason: To protect the health of future users of the site in accordance with policy 5.21 of the London Plan 2015 and policy DM EP4 of Merton's sites and policies plan 2014.

5.3 Transport Planning Officer

Observations:

The site is currently vacant and comprises a combination of two plots; 99 Hamilton road and 101 Hamilton Road. The scheme proposes to deliver a residential development of 5 residential units comprising the following mix of units:

- 3 x 5 bed houses
- 1 x 2 bed unit
- 1 x 3 bed unit

Hamilton Road is a residential road operating at a 20-mph speed limit. Through the use of bollards, Hamilton Road does not permit vehicular access to / from the south towards Merton High Street.

The site is within a PTAL of 5 which is considered as representing a 'very good' level of accessibility to public transport services.

The local area forms part of Controlled Parking Zone S2. Restrictions are enforced from Monday to Saturday between 8.30am to 6.30pm.

Car Parking:

Due to restricted nature of the development, car parking cannot be accommodated within the site and none is proposed. Permit free option would be acceptable subject to the applicant enters into a Unilateral Undertaking which would restrict future occupiers of the development from obtaining an on-street residential parking permit to park in the surrounding controlled parking zones to be secured by via S106 legal agreement.

The existing dropped Kerb should be reinstated and introduce yellow lines in accordance with the requirements of the Highway Authority to facilitate servicing and turning of vehicles.

Cycle Parking

The London Plan and London Housing SPG Standard 20 (Policy 6.9) states all developments should provide dedicated storage space (secure and undercover) for cycles at the following level:

- 1 per studio and one bed dwellings;
- 2 per all other dwellings

The proposed ground floor layout shows cycle stores providing 2 cycle spaces each for the 3 houses and 4 spaces for the maisonette units. The number provided satisfies the 'London plan' standards however, it is not clear how the cycle stores can be accessed with the refuse blocking its path.

Recommendation: Refuse

The proposed development would generate additional pressure on parking in the area, and in the absence of a legal agreement securing a car free agreement, the proposal would be contrary to policy CS20 (Parking, Servicing and Delivery) of the Adopted Core Planning Strategy (July 2011).

5.4 Climate Change Officer – No objection subject to condition

6. **POLICY CONTEXT**

6.1 Merton Core Planning Strategy (July 2011)

CS8 – Housing Choice
CS9 – Housing Provision
CS12 – Economic Development
CS14 - Design
CS15 – Climate Change
CS18 – Active Transport
CS19 – Public Transport
CS20 - Parking, Servicing and Delivery

6.2 Adopted Merton Sites and Policies Plan (July 2014)

DM H2 Housing Mix
DM H3 Support for affordable housing
DM D2 Design Considerations in All Developments
DM D4 Managing Heritage Assets
DM EP2 Reducing and Mitigating Noise
DM E3 Protection of scattered employment sites
DM T1 Support for sustainable transport and active travel
DM T2 Transport impacts of development
DM T3 Car parking and servicing standards
DM F1 Support for flood risk management
DM F2 Sustainable urban drainage systems (SuDS) and; Wastewater and Water Infrastructure

6.3 London Plan (July 2016)

3.3 (Increasing Housing Supply),
3.4 (Optimising Housing Potential),
3.5 (Quality and Design of Housing Developments),
3.8 (Housing Choice),
5.1 (Climate Change Mitigation),
5.3 (Sustainable Design and Construction).
7.3 (Designing Out Crime)
7.4 (Local Character)
7.6 (Architecture)

Other

- National Planning Policy Framework 2019
- National Planning Practice Guidance 2014
- London Plan 2016 - Housing SPG 2016
- Draft London Plan 2018
- Draft Local Plan 2020

7. **PLANNING CONSIDERATIONS**

7.1 The principal planning considerations related to this application are the planning history, principle of development, loss of employment, the design/visual impact of the building, impact upon the Hamilton Road street scene, standard of accommodation provided, impact upon neighbouring amenity and parking/highways.

7.2 **Amendments**

7.2.1 Following discussions with officers and comments from the Council Transport Planner in regards to a permit free development, the applicant has put forward that the flats would be permit free and only one car parking permit can be obtained for each of the proposed houses.

7.2.2 A 1.8m high screen has been added to the second floor terrace for the upper floor flat in order to prevent views of the neighbouring garden directly to the rear.

7.3 **Planning History**

7.3.1 The application site has been subject of a long and complex planning history. The site has changed ownerships several times and unfortunately resulted in the demolition of Rose Cottage. The site has remained cleared and hoarded for some time, resulting in a blot in the street scene. The redevelopment of the site is therefore welcomed by officers given the long history and current condition of the site.

7.3.2 Members of the planning committee refused the previous scheme on the site (19/P0883 - erection of a two storey detached building with accommodation at roof and basement level comprising 13 flats (5 x 1, 6 x 2 and 2 x 3 bedroom flats) and associated works) mainly due to the size of the building and amount of development on the site.

Refusal reason:

The proposed development by reason of its design, height, massing and siting would be an overly bulky and dominant form of development, resulting in an overdevelopment of the site, failing to respect the Hamilton Road street scene and general pattern of development within the area, contrary to Policies DM D2 (Design Considerations in all developments) of Merton's Adopted Sites and Policies Plan (July 2014), CS14 (Design) of Merton's adopted Core Planning Strategy (July 2011) and Policy 7.4 (Local Character) of the London Plan (2016).

7.3.3 In response to the previous refusal on the site, the applicant (a different applicant compared to the previous scheme) has made material

improvements to the design of the building, including reductions in the size of building and a reduction in the number of units. The proposal is considered to be a vast improvement on the refused scheme, with a reduction in the number of dwellings, incorporation of family houses and reduced bulk and mass.

7.4 **Principle of Development**

7.4.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 states that when determining a planning application, regard is to be had to the development plan, and the determination shall be made in accordance with the development plan, unless material considerations indicate otherwise.

Loss of Employment

7.4.2 Planning approval, 15/P3573, permitted the loss of employment, on the condition that Rose Cottage would be restored and refurbished as part of the redevelopment of the site. Now that Rose Cottage has been demolished, this is no longer an option to mitigate the loss of employment. Given that the former buildings have been demolished, the site is still considered to be a scattered employment site. The proposal must therefore be considered against planning Policy E3 (Protection of scattered employment sites) of Merton's Sites and Policies Plan. The policy seeks to retain/support a range of employment opportunities towards creating balanced mixed use neighborhoods in Merton.

7.4.3 Planning policy E3 states that proposals that result in the loss of scattered employment sites will be resisted except where:

- i. The site is located in a predominantly residential area and it can be demonstrated that its operation has had a significant adverse effect on local residential amenity;
- ii. The size, configuration, access arrangements and other characteristics of the site makes it unsuitable and financially unviable for whole-site employment use; and,
- iii. It has been demonstrated to the council's satisfaction that there is no realistic prospect of employment or community use on this site in the future. This may be demonstrated by full and proper marketing of the site at reasonable prices for a period of 30 months (2½ years).

7.4.4 The previous planning application on the site, 19/P0883, stated that there was no marketing evidence for employment or community uses. The loss of employment will therefore need to be considered against parts i and ii of planning policy E3 above.

i. The site is located in a predominantly residential area and it can be demonstrated that its operation has had a significant adverse effect on local residential amenity;

7.4.5 As set in the planning committee report relating to 19/P0883, the application site is located at the end of a narrow no-through residential street. The surrounding area includes a mixture of both residential and commercial buildings, however for the sake of clarification the site is considered to be located in a predominantly residential area. There is no evidence that the former uses had a significant adverse effect on local residential amenity, however, the site does have a sensitive relationship with neighbouring residential uses due to the number of surrounding units and their close proximity to the site. For example, residential gardens adjoin the site to the north and east. Further, residential units overlook the site and adjoin it. Although the site is cleared, the former employment buildings on the site were at the rear, abutting neighbouring boundaries. The close proximity of surrounding residential would have made it difficult to expand/intensify the former employment operations.

ii. The size, configuration, access arrangements and other characteristics of the site makes it unsuitable and financially unviable for whole-site employment use;

Size

7.4.6 The former employment buildings on the site comprised light industrial units (Class B1c) with a floor area of approximately 200sqm. The amount of jobs the site could deliver is therefore limited given the use of the units and their modest floor area.

Configuration

7.4.7 The three former employment buildings were wedged into the rear/side of the site, directly to the rear of the former Rose Cottage (residential use) and within close proximity of neighbouring residential uses to the rear and side. The site is therefore constrained for employment purposes by the number and close proximity of residential uses.

Access arrangements

7.4.8 The site included onsite car parking; however, Hamilton Road is a narrow no through road, which is usually heavily parked on either side of the street. The existing access arrangements are therefore not considered ideal for commercial activity.

Unsuitable and financially unviable

- 7.4.9 It was acknowledged under the previous planning approval (15/P3573) the former buildings were in a poor condition and would have been difficult to let the premises in the open market in their condition. The prospect of continued employment in the former buildings in their condition were therefore limited for long-term occupation and would require significant financial outlay to bring up to modern standards.
- 7.4.10 Given the constraints of the site (surrounding residential properties) and limited sized employment floor space, it is considered that it would be unrealistic that the site would come forward for employment purposes only (same provision or increase in floor space).
- 7.4.11 further, in dismissing the appeal relating to LBM Ref 17/P3242 (Appeal Ref - APP/T5720/W/17/3189000), the planning inspector do not sight loss of employment as a reason to dismiss the appeal.

Conclusion on loss of employment

- 7.4.12 Whilst the site had previously been in employment use, the employment part of the site only comprised 200sqm of floor space and would therefore not generate a high number of jobs. The access requirements for the site are far from ideal and the site is constrained by adjoining residential units and gardens, making it generally less attractive for other employment or community uses. The loss of employment must also be balanced against other planning benefits. In this instance, the proposal would create 5 new residential units, which will make a modest contribution to meeting much needed housing targets, in a sustainable location. A wholly residential use would be in keeping with the immediate surroundings. The loss of employment is therefore considered to be acceptable in this instance.

Residential

- 7.4.13 The requirement for additional homes is a key priority of the London Plan which seeks to significantly increase the ten year minimum housing target across London from 322,100 to 423,887 (in the period from 2015 to 2025), and this equates to an associated increase in the annual monitoring target across London to 42,389. The minimum ten year target for Merton is 4,107, with a minimum annual monitoring target of 411 homes per year. Paragraph 58 of the 2018 NPPF emphasised the Governments objective to significantly boost the supply of homes.
- 7.4.14 The planning application seeks to provide 5 new residential units, which will make a modest contribution to meeting housing targets, and provides a mix of unit sizes that will assist in the delivery of a mixed and balanced

community in a sustainable location. The provision of new housing is considered to be in accordance with the objectives of the NPPF, London Plan targets, and LBM policies.

7.5 Design/Visual Amenity

- 7.5.1 The overarching principle of national and local planning policy is to promote high quality design. Planning policy DM D2 (Design considerations in all development) of Merton's Sites and Policies Plan states that amongst other considerations, that proposals will be expected to relate positively and appropriately to the siting, rhythm, scale, density, proportions, height, materials and massing of surrounding buildings and existing street patterns, historic context, urban layout and landscape features of the surrounding area.
- 7.5.2 The proposed design, height and massing of the proposed buildings are considered to respect the visual amenities of the street scene as required by planning policy DM D2. The proposed buildings would have a traditional form and modern detailing which respond positively and appropriately to the siting, rhythm, scale, density, proportions, height, materials and massing of surrounding buildings in the Hamilton Road street scene. The ridge height and eaves height would be similar to the existing traditional housing in the road, and combined with the provision of bay windows, facing brick and slate roof tiles, officers consider this to provide a good design approach for the streetscene.
- 7.5.3 Whilst some objections have been received in regards to the front dormers being out of keeping, the proposed dormers have been designed with a uniform design approach for the development. The slightly oversized front dormers add interest to the design, which are not uncommon for developments of this style where new modern developments respond to an existing traditional settings in London. Other dormers in the Hamilton Road street scene exist and have not been successfully introduced as often these are standalone additions which don't relate well to the original design of the building. In this instance the front dormer are considered to relate well to the design approach taken for this standalone development and would not cause any adverse impact upon the character of the street scene. In this regard, permitting front dormers at the applicant site is not considered to set a precedent for other roof extensions in the street for the reasons stated above. The rear elevation of the building would comprise typical rear additions commonly seen on existing properties, with rear roof sections and ground floor additions with glass doors.
- 7.5.4 Overall, the proposal is considered to respond positively to the surrounding area and streetscene of Hamilton Road.

7.6 **Housing Mix**

- 7.6.1 Planning policy DM D2 (Housing Mix) seeks to create socially mixed communities, catering for all sectors of the community by providing a choice of housing with respect to dwelling size and type in the borough. London Plan Policy 3.8, seeks to promote housing choice and seek a balance mix of unit sizes in new developments, with particular focus on affordable family homes. Family sized accommodation is taken in the London Plan and LBM policy to include any units of two bedrooms or more.
- 7.6.2 The borough level indicative proportions concerning housing mix (as set out below) will be applied having regard to relevant factors including individual site circumstances, site location, identified local needs, economics of provision such as financial viability and other planning contributions.

Table in Planning policy DM H2 (Housing Mix) of Merton's Sites and policies plan 2014

Number of Bedrooms	Percentage of units
One	33%
Two	32%
Three +	35%

Proposal – 3 x 5 bedroom houses, 1 x 2 bedroom flat and 1 x 3 bedroom flat.

Number of Bedrooms	Percentage of units
One	0%
Two	20%
Three +	80%

- 7.6.3 The proposed housing mix of the site, would not strictly meeting the Council percentage ratio set out in Policy DM H2 (Housing Mix), however, the proposal is considered to still offer a good range of housing choice with a good proportion of each unit type. The provision of 100% family type accommodation (2 bedroom or more), including three good sized family houses in this location is particularly welcomed given the difficulty of providing new houses on sites coming forward in built up locations.

7.7 **Density**

- 7.7.1 London Plan Policy 3.4 (Optimising Housing Potential) provides guidance of density ranges. Table 3.2 of the policy sets appropriate density ranges that relate to setting in terms of location, existing

building form and massing, and the index of public transport accessibility (PTAL).

- 7.7.2 Policy 3.4 and Table 3.2 are critical in assessing individual residential proposals but their inherent flexibility means that Table 3.2 in particular should be used as a starting point and guide rather than as an absolute rule so as to also take proper account of other objectives, especially for dwelling mix, environmental and social infrastructure, the need for other land uses (eg employment or commercial floorspace), local character and context, together with other local circumstances, such as improvements to public transport capacity and accessibility. The London Plan is clear that the SRQ density matrix should not be applied mechanistically, without being qualified by consideration of other factors and planning policy requirements.
- 7.7.3 The proposed development will provide 5 residential units and taking into account the site area of 0.069ha, the residential density of the proposed development equates to 414 habitable rooms per ha and 72 units per ha. The London Plan density matrix states that within an urban area with a PTAL score of 5, developments should have a habitable room per ha of between 200 – 700 hr/ha and unit per ha of between 70 – 260 u/ha. The proposed development would therefore fall within both ranges set out in the density matrix.

7.8 **Impact upon neighbouring amenity**

- 7.8.1 When assessing neighbouring impact, consideration must be given to the former building on the site (Rose Cottage), as this was a long-standing relationship.

Sun and Daylight

- 7.8.2 The applicant has commissioned an independent sun and daylight consultant who has confirmed that the amenity values of daylight and sunlight to the neighbouring residential properties would be retained to a level that would satisfy the BRE criteria.

206 – 212 Merton High Street

- 7.8.3 Properties in Merton High Street are commercial at ground floor level, therefore there would be no undue loss of amenity to the ground floor. It is noted that the ground floor of 206 Merton High Street has recently been granted prior approval for change of use from commercial to residential. However, it must be stressed that the prior approval process does not take into consideration the standard of residential accommodation proposed, standards of light and outlook etc. In that instance, the levels of light and

outlook serving the proposed flat were already restricted due to the close proximity of the ground floor to the site boundary and former buildings on the site. The proposed development is not considered to make the standard of residential development any poorer than the details permitted under the prior approval process.

- 7.8.4 At the upper levels, this neighbouring building contains flats, these have rearward facing windows towards the application site at first and second floor levels. These windows generally serve bedrooms for the flats, with the living areas fronting onto Merton High Street. The rear windows are however inset approximately between 3m and 3.6m and 4.8m and 5.2m at first and second floors respectively from the flank wall of the proposed building. It should also be noted that the proposed building at the upper levels would be inset 0.8m from the site boundary, unlike the previous refusal and Rose Cottage which were hard up against the boundary. Whilst the level of separation between these neighbours and the proposed building is not generous, this is a highly urban situation, the windows serve bedrooms and there would be a similar relationship to the former and long established buildings on the site. The sun and daylight report confirms that there would be no undue loss of light and there would still remain a suitable level of outlook from the first and second floor windows on the neighbouring properties. It is therefore considered that there would be no undue loss of amenity to warrant refusal of planning permission.

97 (97a & b) Hamilton Road

- 7.8.5 This neighbouring property is spilt into two flats. The proposed buildings would attach to this neighbouring building, however the proposed building at the upper levels would not project beyond the neighbours existing two storey rear wing. Likewise the proposed ground floors would not project beyond the neighbours existing ground floor rear extension or external rear staircase. A new party wall at ground and part first floor level would project along the boundary and beyond the neighbours two storey rear wing, however the proposed party wall would appear as a low level feature when from this neighbouring property. The proposed development would therefore have no undue impact upon the rear facing window or doors within this neighbouring property.
- 7.8.6 This neighbouring property as stated above is spilt into two flats, the arrangement of the rear garden has also been spilt into two, with one section of the garden being situated directly to the rear of the application site (location of proposed flats). The upper floor windows serving the proposed spilt level flat would be situated directly opposite the rear garden of this neighbouring property. Given the close proximity of the proposed windows to the neighbours garden, a planning condition relating the external terrace are at first and second floor levels are required to be fitted

with a 1.8m high screen to prevent overlooking and loss of privacy.

111 & 113 Hardy Road

7.8.9 These neighbouring properties are located directly to the rear of the proposed development. There would be a separation distance of over 30m which would ensure that there is no undue overlooking of these neighbouring properties. It should be noted that the rear outbuildings serving the three houses has been designed to sit within the profile of the former industrial buildings abutting the rear boundary, therefore there would be no additional impact when compared to the long standing former situation.

7.9 **Standard of Accommodation**

7.9.1 London Plan policies 3.5, 3.6, 3.7 & 3.8, CS policy CS 14, and SPP policies DM D1 and DM D2 seek to ensure that new residential development is of a high standard of design both internally and externally and provides accommodation capable of adaptation for an ageing population and for those with disabilities, whilst offering a mix of unit size reflective of local need.

7.9.2 In terms of the quality of the accommodation, the proposed houses and flats would need to meet or exceed the London Plan Gross Internal Area minimum standards with each room being capable of accommodating furniture and fittings in a suitable manner. The GIA of all the units would meet London Plan standards. Each habitable room would have suitable levels of light and outlook and the indicative layout shows that rooms are capable of accommodating furniture and fittings in a suitable manner.

7.9.3 Planning policy DM D2 (Design considerations in all developments) states that for all new houses, the Council will seek a minimum garden area of 50 sqm as a single usable regular shaped amenity space. For flatted dwellings, a minimum of 5 sqm of private outdoor space should be proposed for 1-2 person flatted dwellings (as specified in the Mayors Housing Supplementary Planning Guidance) and an extra 1 sqm should be provided for each additional occupant. The proposed development would meet the private amenity space standards set out in planning policy DM D2.

8. **Traffic, Parking and Highways conditions**

8.1 The site has a Public Transport Accessibility Level (PTAL) of 5, which means it has very good accessibility on account of its proximity to South Wimbledon Tube Station and numerous bus services on Merton High Street. It is also within 20 minutes walking distance to Wimbledon Station

where National, District line and Tramlink services are available as well as extensive shopping and cultural facilities.

- 8.2 The local area forms part of Controlled Parking Zone S2. Restrictions are enforced from Monday to Saturday between 8.30am to 6.30pm. The development will be car-free with no provision for off-street parking. It is proposed to remove the existing crossover and introduce 2 marked parking bays along the site frontage for use of permit holders within the CPZ. These two additional parking bays would benefit existing residents in the surrounding area.

Car Parking

- 8.3 A number of objections have been received from neighbours relating to the already lack of parking in the local area. Their concern is that the provision of 5 extra units will cause harm to highway conditions.
- 8.4 Officers have noted the objection from the Councils Transport Planner. However, consideration must be given to the planning history of the site, the former situation of the site (and its permitted allocation of car parking permits) and what benefits the scheme would deliver for all residents (two new on-street parking bays).
- 8.5 The planning history of the site is a material planning consideration that needs to be taken into account when assessing the current application. There has been no change in adopted planning policy between the 2019 application and the current application for the Council to take a different approach. The level of car parking permits not disputed under the previous planning application (19/P0883) for 13 flats allowed permits for the 2 x 3 bedroom flat (no limit on numbers). The current application would result in a deduction of 8 units on the site (plus a reduction in bedrooms, from 20 bedrooms in total compared to 23 bedrooms under the previous application) and now only 3 permits in total are proposed.
- 8.6 In terms of context, the application site has now been cleared, however, previously the site included approximately 200sqm of light industrial units and a detached residential building used as a House of Multiple Occupation (HMO) (10 bedrooms). It is noted that the site did have some ad-hoc parking on the site (approx. 5 spaces), which would be removed as part of the redevelopment of the site, however this wouldn't restricted the issuing of car parking permits. In terms of the former established uses, it must be noted that it is usual practice that businesses can obtain 2 permits and there is no restriction of the number of permits a residential unit can obtain. In this instance, the former use had the ability of obtaining 2 business permits plus unlimited residential permits (residential building had 10 bedrooms).

- 8.7 The proposal seeks to introduce 2 new on street car parking bays for use of all qualifying residents in the CPZ. It must be noted that if the development were to be fully permit free (as suggested by the Councils Transport Planner), then the applicant would have no reason to offer introducing new bays within the street as the occupiers of the new development would be unable to use these bays. The inclusion of two new bays (funded by the applicant) would allow greater capacity for all users in the CPZ. Objections have been received in regards to lack of parking in the street so the two new bays would have wider public benefit as these can be used by all (not just the proposed family houses). The delivery of 2 on street car parking bays would be delivered under a S287 agreement with the Councils Highway Section.
- 8.8 In conclusion, officers have considered the planning history of the site, the context of the former uses (its buildings and the number of car parking permits that the site could have obtained originally) and what public benefits the scheme can deliver. The proposal in its amended form would only allow for the allocation of 3 car parking permits, one per house. These permits would be issued for each of the 5 bedroom houses and would understandably need at least one car parking space given their size. Despite the objection from the Councils Transport Planner, officers consider that the restriction of only 3 permits being issued within this context is reasonable and would not place any additional car parking pressure on the CPZ, particularly when compared to the previous arrangement.

Cycle Parking

- 8.9 The London Plan and London Housing SPG Standard 20 (Policy 6.9) states all developments should provide dedicated storage space (secure and undercover) for cycles at the following level (1 per studio and one bed dwellings and 2 per all other dwellings). The development will provide 2 spaces per 2bed+ dwelling within a secure cycle store in the front gardens of the respective units. A single shared cycle store will be provided for the maisonette units. Cycle parking is therefore considered acceptable.

Refuse

- 8.10 Refuse collections will continue to be taken from Hamilton Road as per the existing arrangement. Refuse collection vehicle will leave the site via Hamilton Road Mews which is an existing collection route. Undercover bin storage will be provided in the front gardens of each unit. The bin storage provision is considered to be acceptable.

Car Club

8.11 To further encourage sustainable modes of transport and help establish travel patterns for future occupiers, the development would also be subject to a free, three year car club membership. This can be controlled and secured via a S106 agreement.

9. **Sustainability**

9.1 Planning policy CS15 (climate Change) of Merton's adopted Core Planning Strategy (2011) seeks to tackle climate change, reduce pollution, develop low carbon economy, consume fewer resources and use them more effectively.

9.2 Planning Policy 5.2 of the London Plan (2016) states that development proposals should make the fullest contribution to minimising carbon dioxide emissions in accordance with the following energy hierarchy:

1. Be lean: use less energy
2. Be clean: supply energy efficiently
3. Be Green: use renewable energy

9.3 All new developments comprising the creation of new dwellings should demonstrate how the development will:

- i) Comply with Merton's Core Planning Strategy (2011) Policy CS15 Climate Change (parts a-d) and the policies outlined in Chapter 5 of the London Plan (2016).
- ii) As a minor development proposal, outline how the development will achieve a 19% improvement on Buildings Regulations 2013 Part L and submit SAP output documentation to demonstrate this improvement.
- iii) Achieve internal water usage rates not in excess of 105 litres per person per day.

9.4 The proposal would incorporate solar panels on the flat roof sections and the Councils Climate Officer has confirmed that the applicants updated energy report complies with planning policy.

10 **Contamination**

10.1 SPP Policy DM EP4 aims to reduce pollutants and reduce concentrations to levels that will have minimal adverse effects on people and the natural and physical environment. The Councils Environmental Health Officer has confirmed no objection subject to conditions.

11. **Flooding and site drainage**

11.1 The site is within Flood Zone 1 (low probability of flooding) and is not

within a critical drainage area. However, the applicant has submitted a Drainage Strategy based on the principles of the FRA using piped networks and a soakaway to convey, attenuate and treat flows prior to discharge into the ground. The Drainage Strategy follows the SuDS principles to provide amenity, quality and water treatment within the design.

12. **Local Financial Considerations**

12.1 The proposed development is liable to pay the Merton and Mayoral Community Infrastructure Levy (CIL). Merton's Community Infrastructure Levy was implemented on 1st April 2014. This will enable the Council to raise, and pool, contributions from developers to help pay for things such as transport, decentralised energy, healthcare, schools, leisure and public open spaces - local infrastructure that is necessary to support new development. Merton's CIL has replaced Section 106 agreements as the principal means by which pooled developer contributions towards providing the necessary infrastructure should be collected.

13. **SUSTAINABILITY AND ENVIRONMENTAL IMPACT ASSESSMENT REQUIREMENTS**

13.1 The proposal is for minor residential development and an Environmental Impact Assessment is not required in this instance.

13.2 The application does not constitute Schedule 1 or Schedule 2 development. Accordingly, there are no requirements in terms on EIA submission.

14. **CONCLUSION**

14.1 The redevelopment of the site is welcomed as the site has been cleared and provides an un-natural void in the street scene. The proposed new building would offer a high quality contemporary building that respects the existing pattern of development in the area. The proposal would provide good quality residential units with no undue impact upon neighbouring amenity or highway conditions. The application is therefore recommended for approval by planning officers subject to conditions and legal agreement.

RECOMMENDATION

GRANT PLANNING PERMISSION

Subject to the completion of a Section 106 Agreement covering the following heads of terms:-

1. Designation of the development as permit-free (apart from one permit for each of the 5 bedroom houses).
2. Remove existing crossovers and provision of 2 on-street car parking bays (developer to meet the costs of implementation and requirement for separate S278 agreement (highways)).
3. Car club membership (3 years)
4. The developer agreeing to meet the Councils costs of preparing, drafting and monitoring the Section 106 Obligations.

And the following conditions:

1. A.1 Commencement of Development
2. A7 Approved Plans
3. B.1 Materials to be approved
4. B.4 Details of Surface Treatment
5. Details of boundary treatment
6. Refuse implementation
7. Cycle Parking
8. Landscaping details
9. D11 Construction Times
10. Any external lighting shall be positioned and angled to prevent any light spillage or glare beyond the site boundary.
11. Balcony Screens (including roof top level)
12. No use of flat roofs (apart from designated outdoor terraces)
13. A deskstudy, then an investigation shall be undertaken to consider the potential for contaminated-land, and if necessary, a detailed remediation scheme to bring the site to a suitable state for the intended use by removing unacceptable risks to health and the built environment, and submitted to the approval of the LPA.

Reason: To protect the health of future users of the site in accordance with policy 5.21 of the London Plan 2015 and policy DM EP4 of Merton's sites and policies plan 2014.

14. The approached remediation shall be completed prior to development. And a verification report, demonstrating the then effectiveness of the remediation, subject to the approval of the LPA.

Reason: To protect the health of future users of the site in accordance with policy 5.21 of the London Plan 2015 and policy DM EP4 of Merton's sites and policies plan 2014.

15. No part of the development hereby approved shall be occupied until evidence has been submitted to, and approved in writing by, the Local Planning Authority confirming that the development has achieved CO2 reductions in accordance with those outlined in the approved Sustainability Statement (dated 8th October 2020), and internal water consumption rates of no greater than 105 litres per person per day.

16. No PD Rights (Extensions)

17. No PD Rights (Windows)

18. Obscured glazing (upper level side windows)

- 19 F09 Hardstanding

- 20 Construction and Environmental Management Plan

Planning informative:

1. Carbon emissions

Evidence requirements for domestic post construction stage assessments must provide:

- Detailed documentary evidence confirming the Target Emission Rate (TER), Dwelling Emission Rate (DER) and percentage improvement of DER over TER based on 'As Built' SAP outputs. The outputs must be dated and include the accredited energy assessor's name and registration number, the assessment status, plot number and development address.

OR, where applicable:

- A copy of revised/final calculations as detailed in the assessment methodology based on 'As Built' SAP outputs; AND
- Confirmation of Fabric Energy Efficiency (FEE) performance where SAP section 16 allowances (i.e. CO2 emissions associated with appliances and cooking, and site-wide electricity generation technologies) have been included in the calculation.

AND, where the applicant has used SAP 10 conversion factors:

- The completed Carbon Emissions Reporting Spreadsheet.

AND, where applicable:

- MCS certificates and photos of all installed renewable technologies.

Water efficiency

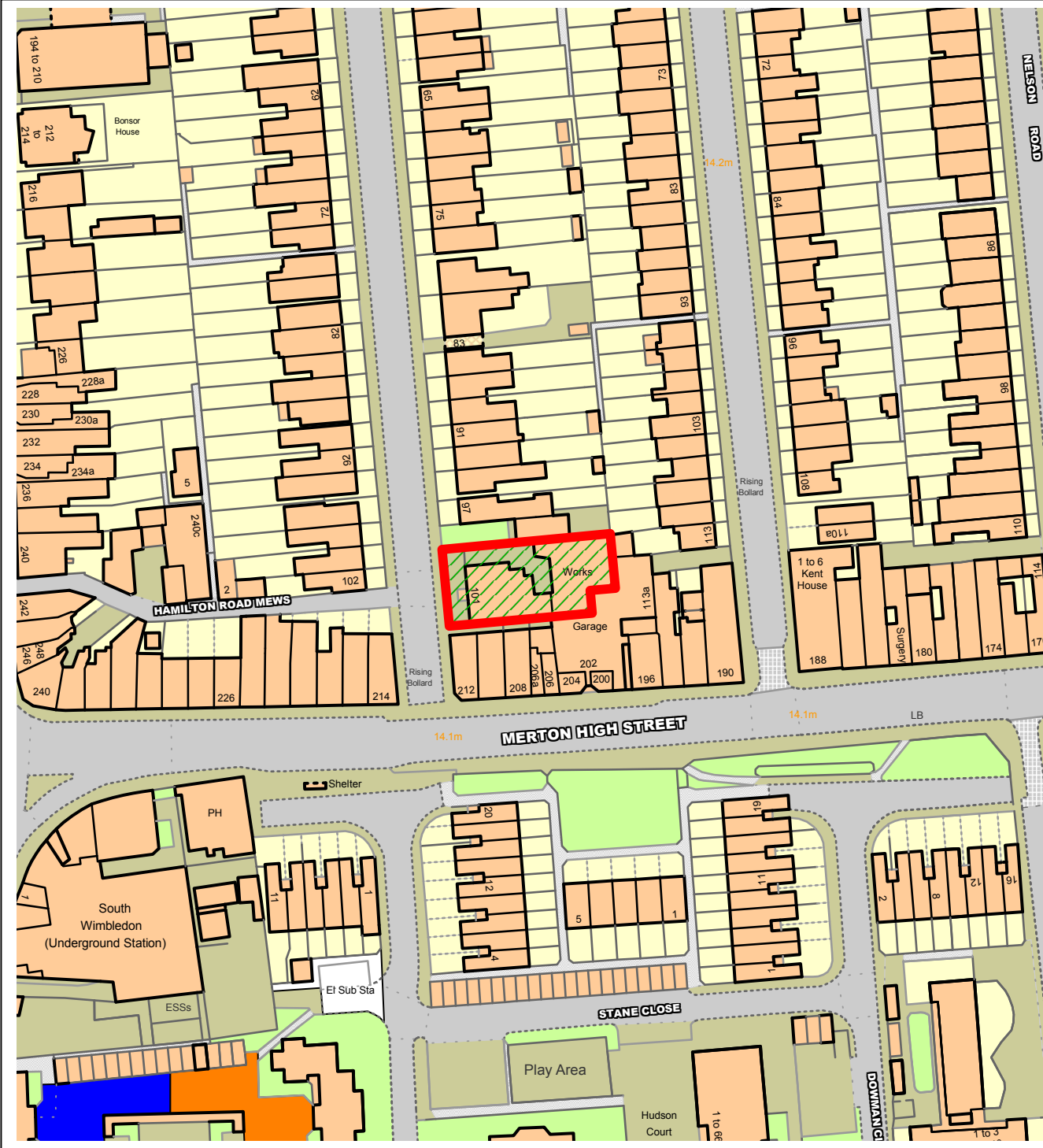
Evidence requirements for domestic post construction stage assessments must provide:

- Water Efficiency Calculator for New Dwellings, representing the dwellings 'As Built', demonstrating that the dwelling(s) has achieved internal water consumption rates of no greater than 105 litres per person per day; AND
- Detailed documentary evidence representing the dwellings 'As Built'; showing:
 - the location, details and type of appliances/ fittings that use water in the dwelling (including any specific water reduction equipment with the capacity / flow rate of equipment); AND
 - the location, size and details of any rainwater and grey-water collection systems provided for use in the dwelling.

2.Third party wall legislation guidance.

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NORTHGATE SE GIS Print Template



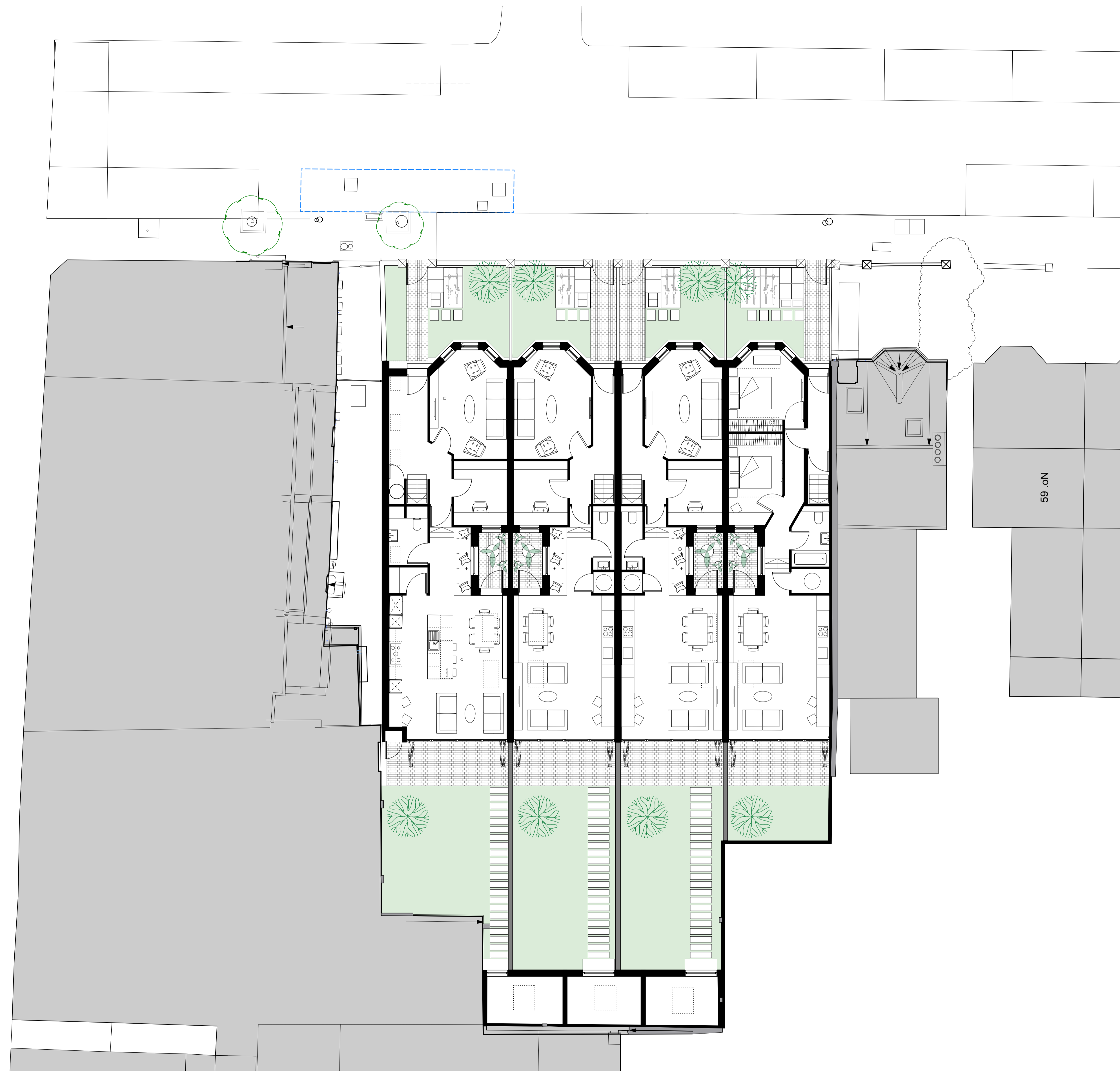
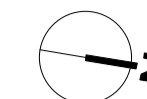
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FOR PLANNING

Rev	Date	Drawn	Description
P1	7/8/20	FP	For Planning

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Project title
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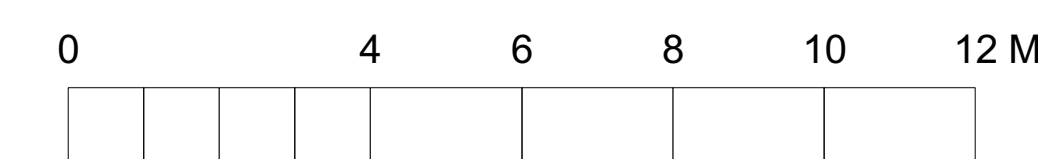
Drawing title
**Proposed Ground
 Floor Plan**

Date	Drawn by	Checked
Apr 2020	FP	GCS

Scale	Rev.
1:100 @ A1 1:200 @ A3	P1

Drawing number:
2003-A4-010

1 A4-010 Proposed Ground Floor Plan
 Scale: 1:100

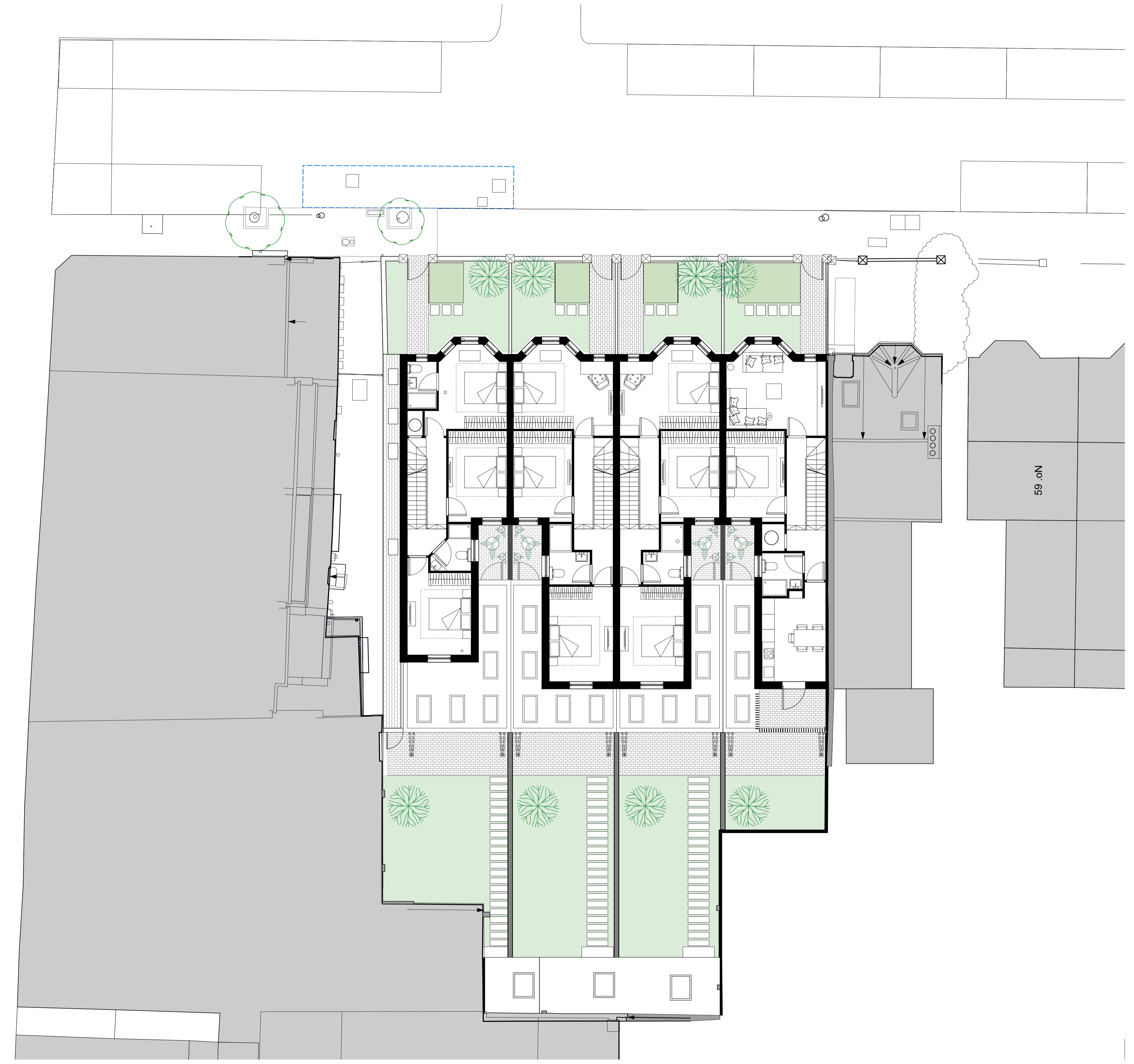
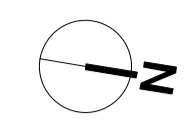


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Rev	Date	Drawn	Description
P1	7/8/20	FP	For Planning

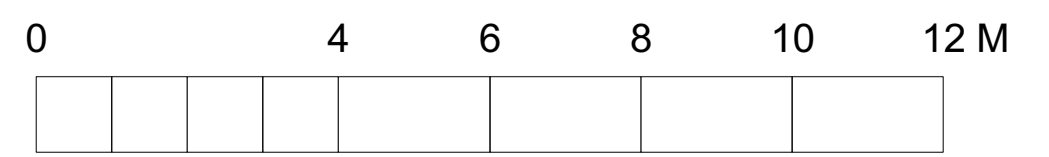
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Project title
**99-101 Hamilton Road,
 South Wimbledon**

Drawing title
**Proposed First Floor
 Plan**

Date	Apr 2020	Drawn by	FP	Checked	GCS
Scale	1:100 @ A1 1:200 @ A3	Rev.	P1		
Drawing number:	2003-A4-020				

1 A4-020 Proposed First Floor Plan
 Scale: 1:100

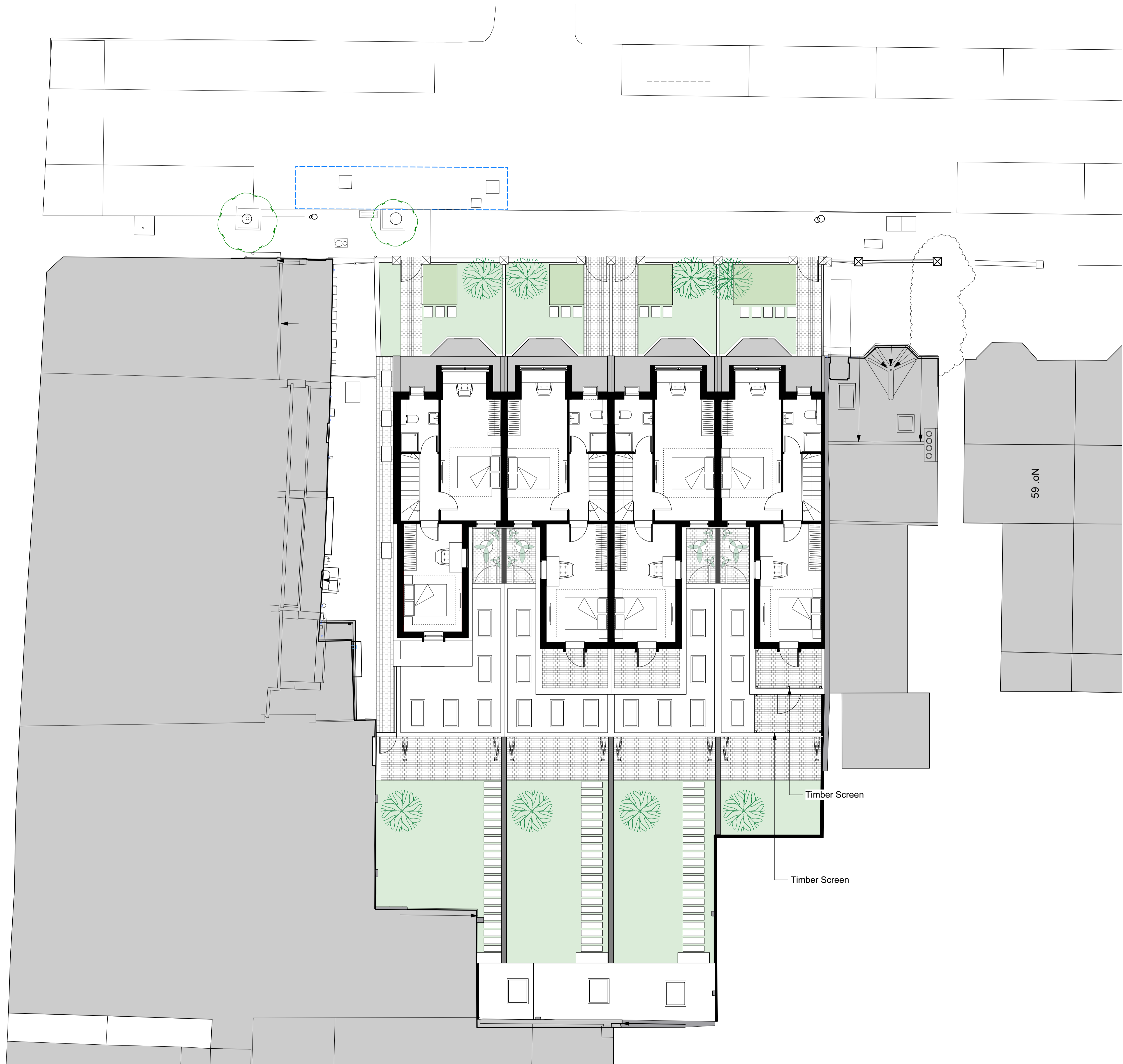


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FOR PLANNING

Rev	Date	Drawn	Description
P1	7/8/20	FP	For Planning
P2	9/20	FP	Amended following planner's comments

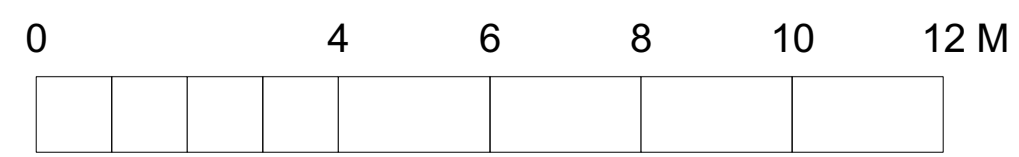
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Project title
**99-101 Hamilton Road,
 South Wimbledon**

Drawing title
**Proposed Second
 Floor Plan**

Date	Drawn by	Checked
Apr 2020	FP	GCS
Scale	1:100 @ A1 1:200 @ A3	Rev. P2
Drawing number:	2003-A4-030	

1 A4-030 Proposed Second Floor Plan
 Scale: 1:100

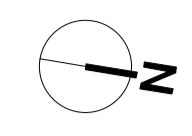


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FOR PLANNING

Rev	Date	Drawn	Description
P1	7/8/20	FP	For Planning
P2	10/20	FP	Amended following planner's comments

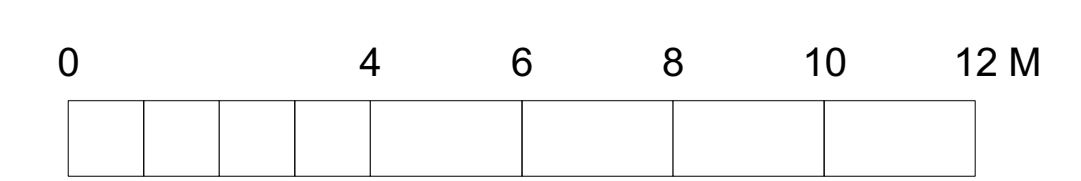
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Project title
**99-101 Hamilton Road,
 South Wimbledon**

Drawing title
Proposed Roof Plan

Date	July 2020	Drawn by	KSH	Checked	GCS
Scale	1:100 @ A1 1:200 @ A3	Rev.	P2		
Drawing number:	2003-A4-040				

1 A4-040 Proposed Roof Plan
 Scale: 1:100

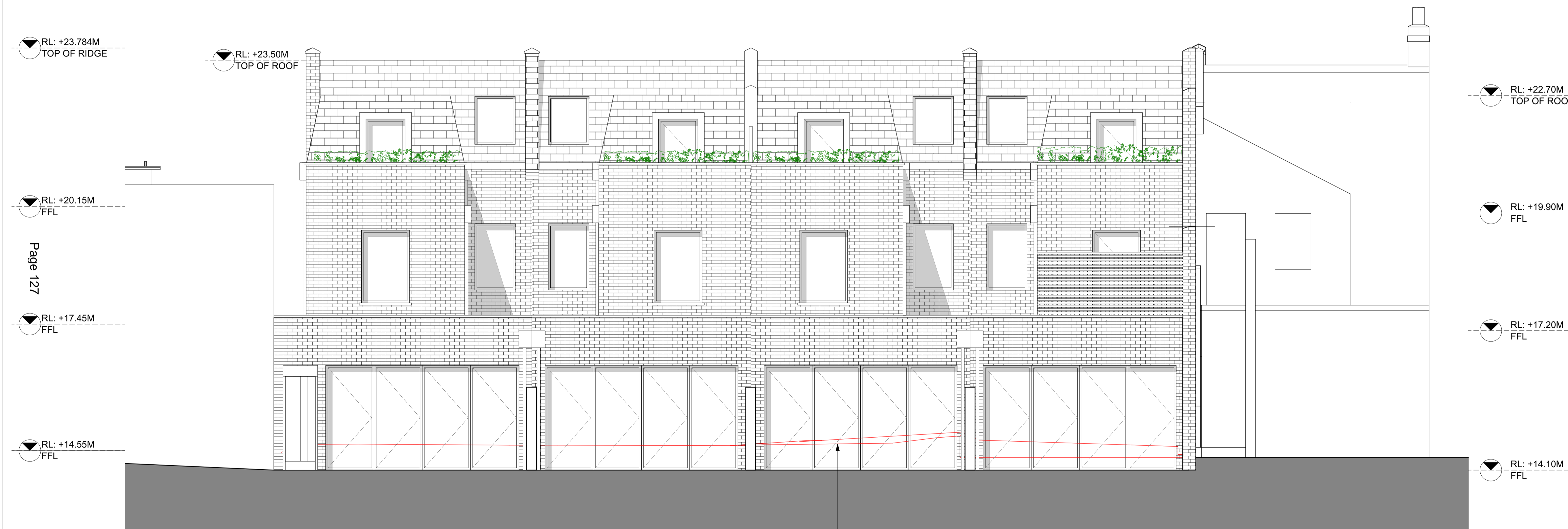


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1 A-4-130 Proposed East Elevation
Scale: 1:50

Line of excavation

FOR PLANNING

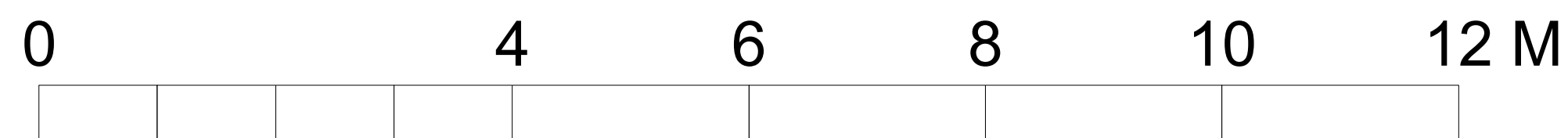
Rev	Date	Drawn	Description
P1	7/8/20	FP	For Planning

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Project title		
99-101 Hamilton Road, South Wimbledon		
Drawing title		
Existing South Elevation		

Date	July 2020	Drawn by	FP	Checked	GCS
Scale	1:50 @ A1 1:100 @ A3	Rev.	P1	Drawing number: 2003-A4-130	



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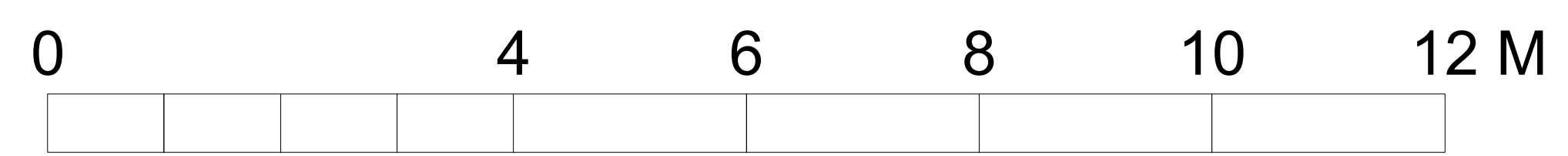
Rev	Date	Drawn	Description
P1	7/8/20	FP	For Planning
P2	9/20	FP	Amended following planner's comments

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Project title
**99-101 Hamilton Road,
 South Wimbledon**

Drawing title
**Existing South
 Elevation**

Date	Drawn by	Checked
July 2020	FP	GCS
Scale	1:50 @ A1 1:100 @ A3	Rev P2
Drawing number:	2003-A4-130	



1 A-4-130 Proposed East Elevation
 Scale: 1:50

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Rev	Date	Drawn	Description
P1	7/8/20	FP	For Planning

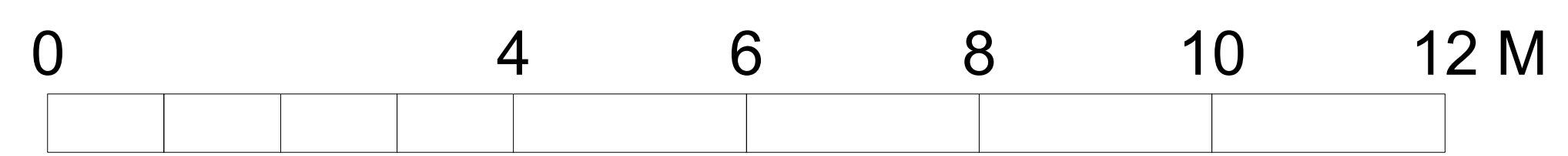
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Project title
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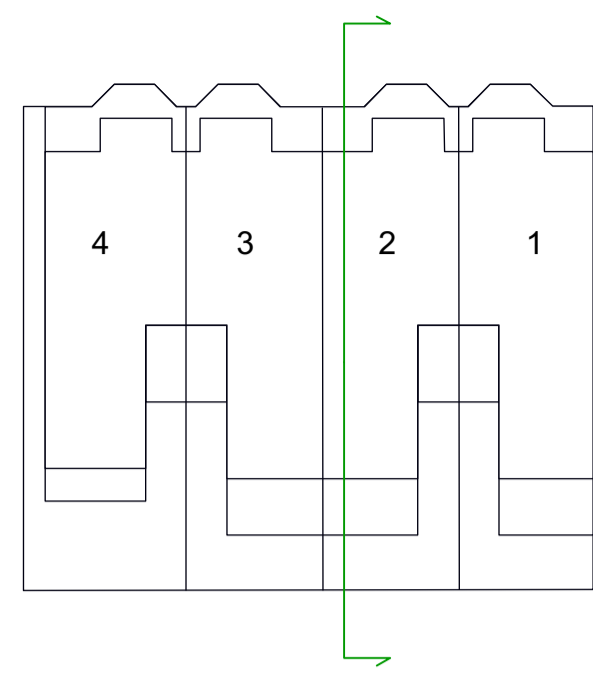
Drawing title
**Proposed West
 Elevation**

Date	July 2020	Drawn by	FP	Checked	GCS
Scale	1:50 @ A1 1:100 @ A3	Rev.	P1		
Drawing number:	2003-A4-120				

1 A-4-120 Proposed West Elevation
 Scale: 1:50



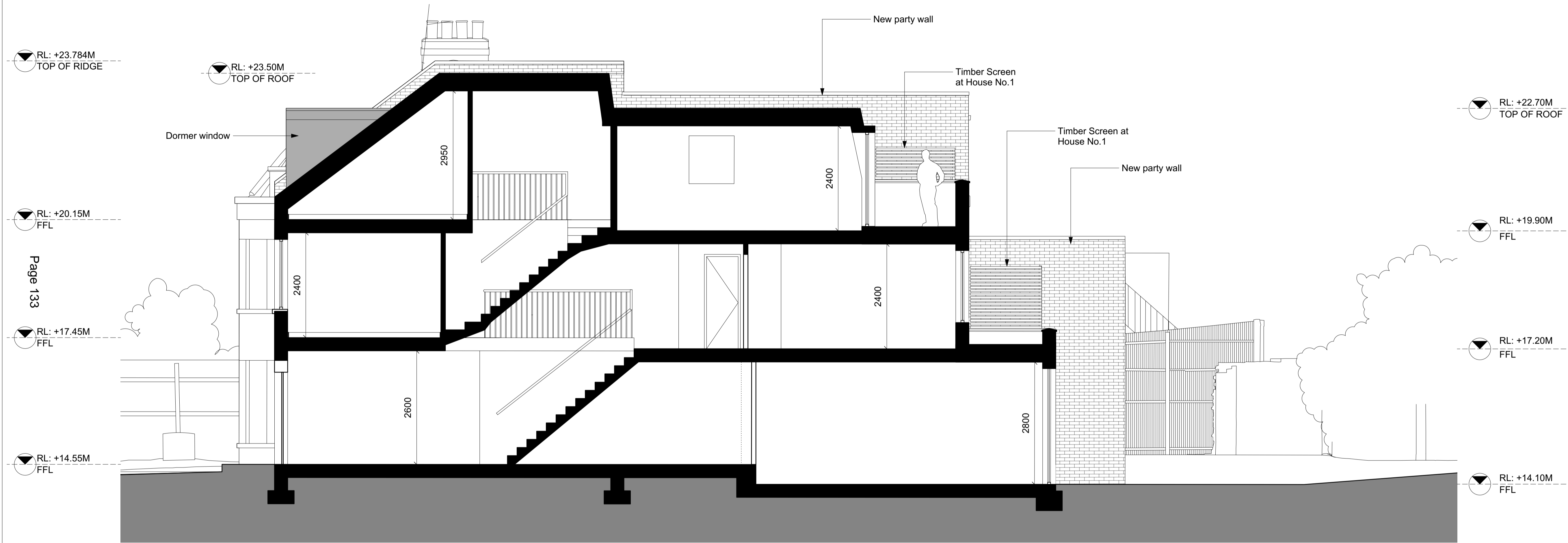
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1 A-4-210 Proposed Section AA
Scale: 1:50

FOR PLANNING

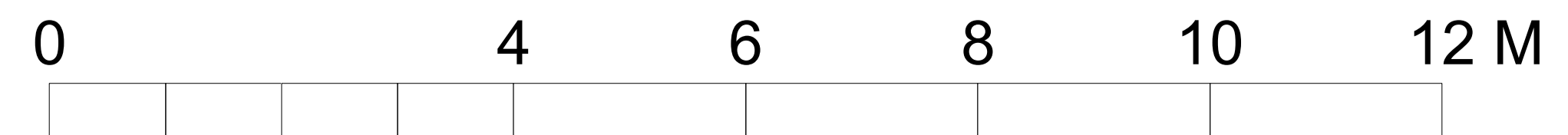
Rev	Date	Drawn	Description
P1	7/8/20	FP	For Planning
P2	9/20	FP	Amended following planner's comments

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Project title	99-101 Hamilton Road, South Wimbledon
Drawing title	Proposed Section AA

Date	July 2020	Drawn by	KSH	Checked	GCS
Scale	1:50 @ A1 1:100 @ A3	Rev.	P2		
Drawing number:	2003-A-4-210				

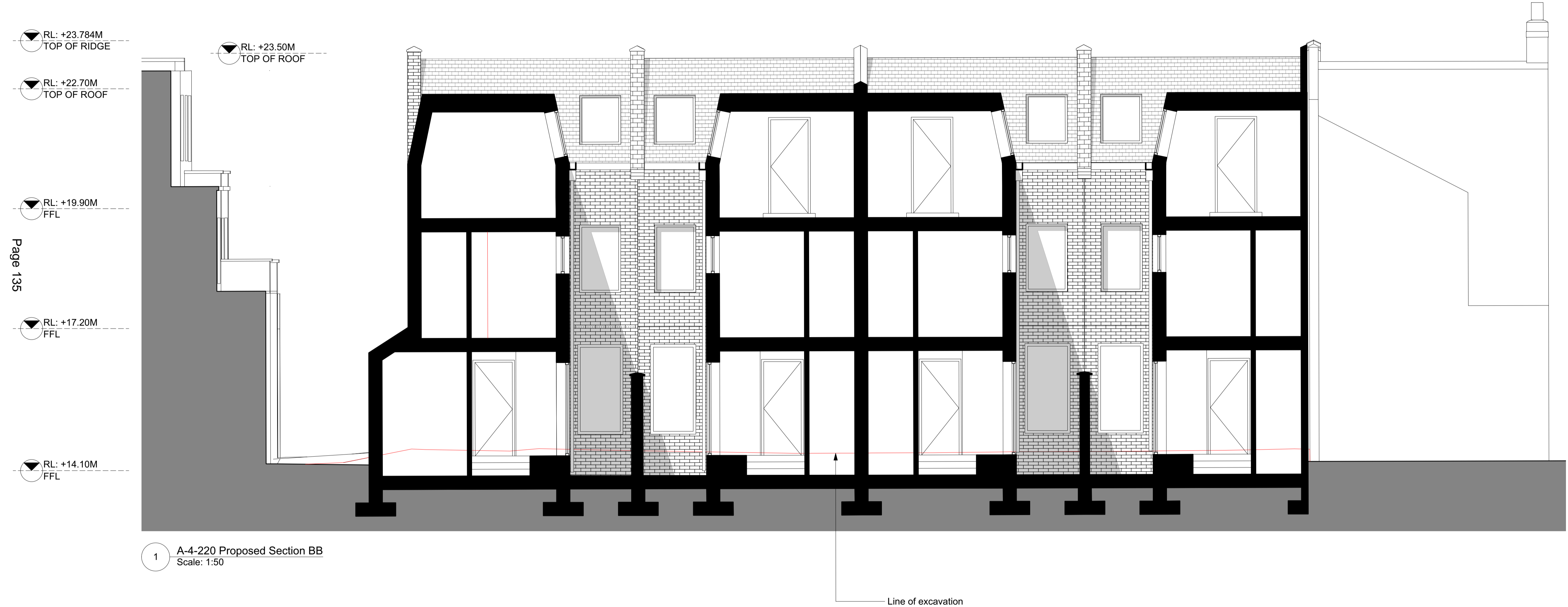
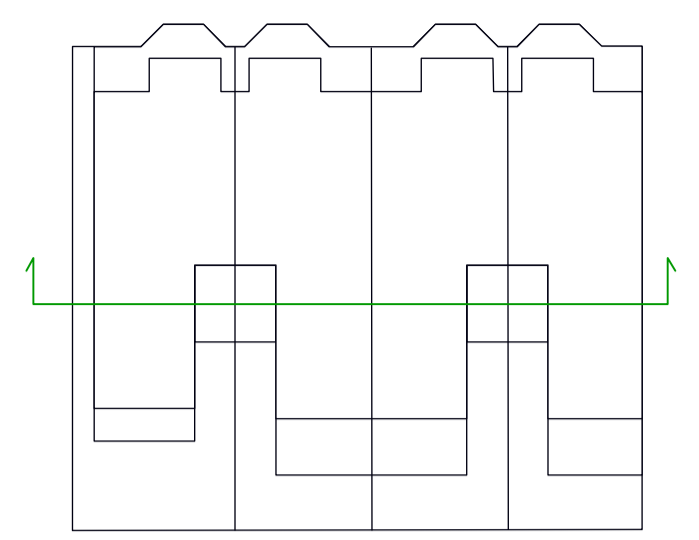


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1 A-4-220 Proposed Section BB
Scale: 1:50

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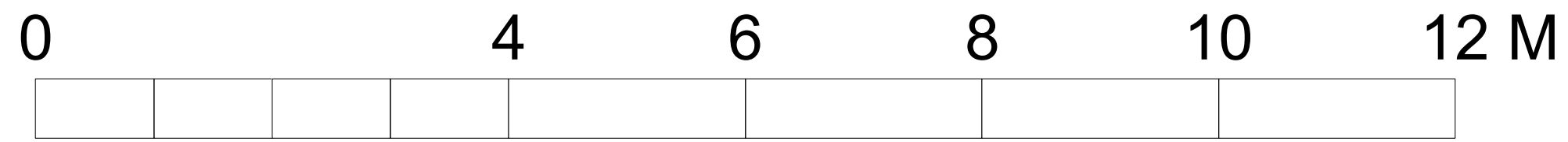
Rev	Date	Drawn	Description
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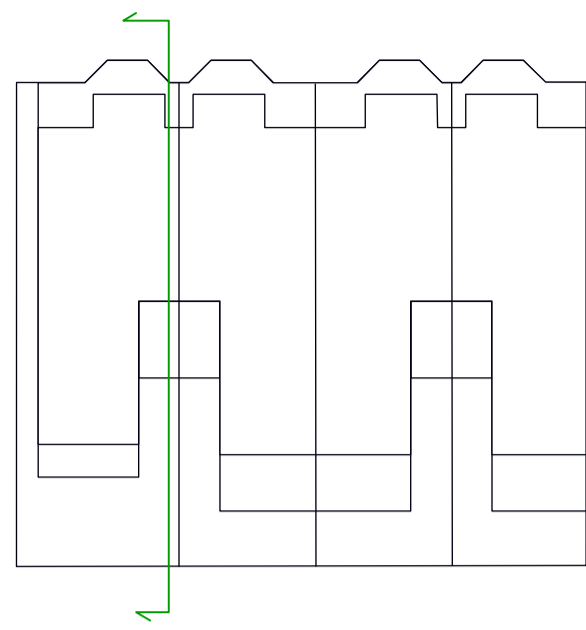
Project title
**99-101 Hamilton Road,
South Wimbledon**

Drawing title
Proposed Section BB

Date	July 2020	Drawn by	KSH	Checked by	GCS
Scale	1:50 @ A1 1:100 @ A3	Rev.	P1	Drawing number: 2003-A-4-220	



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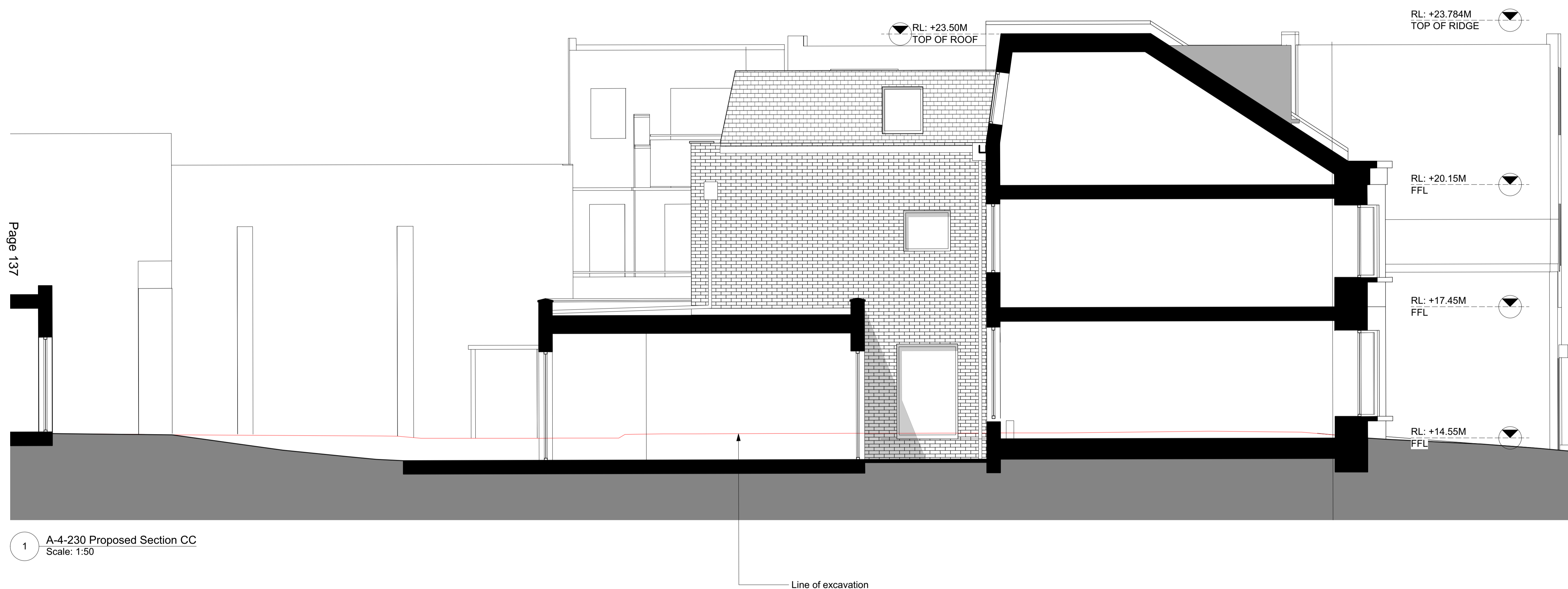


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1 A-4-230 Proposed Section CC
Scale: 1:50

FOR PLANNING

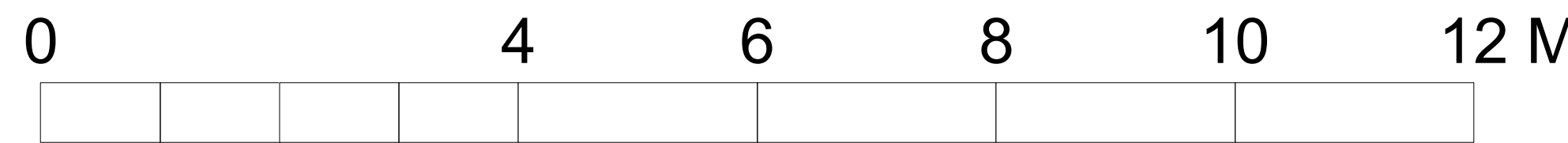
Rev	Date	Drawn	Description
P1	7/8/20	FP	For Planning

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Project title
**99-101 Hamilton Road,
South Wimbledon**

Drawing title
Proposed Section CC

Date	July 2020	Drawn by	KSH	Checked	GCS
Scale	1:50 @ A1 1:100 @ A3	Rev.	P1		
Drawing number:	2003-A-4-230				



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PLANNING APPLICATIONS COMMITTEE
22nd October 2020

Item No:

<u>UPRN</u>	<u>APPLICATION NO.</u>	<u>DATE VALID</u>
	19/P3772	24/10/2019
Address/Site	16 – 20 Morden Road, South Wimbledon, SW19 3BN	
(Ward)	Abbey	
Proposal:	DEMOLITION OF EXISTING BANK (CLASS A2) AND ERECTION OF A NEW RESIDENTIAL BLOCK (CLASS C3), COMPRISING 26 x SELF-CONTAINED FLATS WITH ASSOCIATED PARKING AND LANDSCAPING.	
Drawing Nos:	P/01 F, P/02 G, P/03 G, P/04 G, P/06 G, P/07 G, P/08 G, P/09 C, P/10 F, P/11 C and P/12 D.	
Contact Officer:	Tim Lipscomb (0208 545 3496)	

RECOMMENDATION

Grant Permission subject to conditions and s.106 legal agreement.

CHECKLIST INFORMATION

- Heads of Agreement: Yes, restrict parking permits.
- Is a screening opinion required: No
- Is an Environmental Statement required: No
- Has an Environmental Statement been submitted: No
- Press notice: Yes (major application)
- Site notice: Yes (major application)
- Design Review Panel consulted: No
- Number of neighbours consulted: 138
- External consultations: Yes
- Conservation area: No
- Listed building: No
- Tree protection orders: No
- Controlled Parking Zone: Yes (S1)
- Green corridor – Yes (bordering the site to the north)
- Site of importance for nature conservation (SINC) – Yes (bordering the site to the north)

1. **INTRODUCTION**

- 1.1 This application is being brought to the Planning Applications Committee for determination due to the nature and scale of the development.

2. **SITE AND SURROUNDINGS**

- 2.1 The application site is located on the west side of Morden Road, South Wimbledon. The site is occupied by a flat roof, three storey building (11.2m in height), comprising A2 use on ground floor with ancillary offices above. To the rear of the building is parking lot. The site is generally regular in shape and has an approximate area of 770sq.m.
- 2.2 Adjoining the site to the north is Spur House, a nine storey mixed use building (recessed top floor – 25.7m to parapet, 28.3m to lift overrun) with residential on the upper floors and retail at ground floor level (permitted under application ref. 09/P2219). To the rear of the site, beyond the carpark, is a terrace row of two storey dwellings (with additional loft level). Immediately south of the site is an 8.4m wide shared vehicle access (provides access to the site), beyond which is a four storey block of flats. Opposite the site, across Morden Road within the High Path Estate, are four storey blocks of flats. However, outline planning permission has been granted for the redevelopment of the High Path Estate (17/P1721), which would result in nine storey buildings opposite the application site (buildings of 34m in height).
- 2.3 The surrounding area is characterised by a mix of land uses, albeit it is predominantly residential in nature, becoming more commercial to the north of the site. The area is very well connected, being 70m from South Wimbledon Underground Station and having a public transport accessibility level (PTAL) of 6a.
- 2.4 The site has the following designations and restrictions:
- Controlled parking zone (CPZ) – Yes
 - Conservation area – No
 - Building listed – No
 - Tree Protection Orders – No
 - Flood Zone – Yes (zone 2)
 - PTAL – 6a
- 2.5 The site is also identified with the Draft Merton Local Plan as being on the edges of a new proposed Local Centre. However, that plan is not yet adopted and as such limited weight can be attributed to this potential designation.

3. **PROPOSAL**

- 3.1 The revised proposal (amended plans received 13/07/2020) is for the demolition of the existing three storey mixed use building and the erection of a part 5, part 6 storey building, with basement, to provide 26 residential flats.
- 3.2 The building would have a height of 19.7 (with a maximum height of 20.7m) to the top of the lift shaft overrun.
- 3.3 The building would stand to the frontage of the site, with a slight setback to provide some defensible space at ground floor level. To the rear of the site the existing ground level car park would be reconfigured to provide parking for three cars, a standalone single storey building to house air source heat pump plant and refuse storage. A new communal garden/playspace would be provided to the rear of the standalone building (with an area of 130sqm).

At fifth floor level a roof terrace would be provided (area of 42sqm), along with a sedum green roof (on a non-accessible part of the roof)

- 3.4 A basement would be created beneath the entrance lobby/bike store area, and would be non-habitable, providing a servicing/maintenance/plant area accommodating boilers, tanks and electric meters.
- 3.5 Vehicular access would be via the existing access road to the south of the site, which currently provides vehicular access to the site. No changes to the access are proposed.
- 3.6 The building would feature partly projecting balconies, with integrated winter gardens, to the front and rear elevations.
- 3.7 The ground floor setback to the frontage of the site would accommodate some planting and soft landscaping. A further area of planting and soft landscaping would be provided to the immediate rear of the proposed building. A line of street tree planting is shown to the frontage of the site (within land owned by the applicant).
- 3.8 The building would be finished in light grey and dark grey facing brickwork, with projecting window frames in dark grey coated aluminium.
- 3.9 The building would be 6 storeys to the frontage, with a reduced height of 5 storeys to the rear elevation (in order to seek to minimise the impact on residential properties to the rear).
- 3.10 The existing mature tree to the southwest corner of the site would be retained.
- 3.11 There would be a main entrance to the frontage of the building, leading to a lift lobby and bicycle store (parking for 36 bicycles). The rear entrance to the building, accessed from the car park, would lead directly into the bicycle store. The building would be served by a single staircore and lift shaft. The three units at ground floor level would each have an individual entrance onto the street, along with some limited private garden space to the rear (accessed via bedrooms), enclosed by a 1.8m high brick wall, with indicative hedge planting behind.
- 3.12 In terms of servicing a refuse vehicle would service from the adjacent access road to the south, as is the existing situation.
- 3.13 Nine of the proposed units would be fully dual aspect, with windows to the front and rear (and some to the side also). Eight of the units would be dual aspect but with windows to the front and wide rather than from front to rear. Nine units would be single aspect (N.B. The single aspect units are all east or west facing. None would be north facing).
- 3.14 All three bed units would be dual aspect (with windows to front and rear). The single aspect units would be studios and one-bedroom flats only.
- 3.15 The proposal would provide the following accommodation:

	Type	Habitable rooms	GIA sq.m	External amenity space sq.m	Dual Aspect Rating
			Page 141		

Ground floor	3b/5p	4	96	15	Fully dual aspect
	3b/5p	4	92	15	Fully dual aspect
	3b/5p	4	92	15	Fully dual aspect
First floor	Studio	1	40	6	Single aspect
	1b/2p	2	60	6	Single aspect
	1b/2p	2	53	6	Dual aspect front and side
	2b/4p	3	72	6	Dual aspect front and side
	3b/4p	4	88	6	Fully dual aspect
Second Floor	Studio	1	40	6	Single aspect
	1b/2p	2	60	6	Single aspect
	1b/2p	2	53	6	Dual aspect front and side
	2b/4p	3	72	6	Dual aspect front and side
	3b/4p	4	88	6	Fully dual aspect
Third floor	Studio	1	40	6	Single aspect
	1b/2p	2	60	6	Single aspect
	1b/2p	2	53	6	Dual aspect front and side
	2b/4p	3	72	6	Dual aspect front and side
	3b/4p	4	88	6	Fully dual aspect
Fourth floor	Studio	1	40	6	Single aspect
	1b/2p	2	60	6	Single aspect
	1b/2p	2	53	6	Dual aspect front and side

	2b/4p	3	72	6	Dual aspect front and side
	3b/4p	4	88	6	Fully dual aspect
Fifth floor	1b/2p	2	50	6	Single aspect
	1b/2p	2	56	6	Fully dual aspect
	2b/3p	3	71	6	Fully dual aspect

Housing mix:

Studio	4
1b 2p	10
2b 3p	1
2b 4p	4
3b 4p	4
3b 5p	3

The proportional housing mix is:

Studio – 15.5%

1 bedroom – 38.5%

2 bedroom - 19%

3 bedroom – 27%

3.16 In terms of affordable housing, no affordable housing or commuted sum is offered as part of the application. The application is accompanied by a Financial Viability Statement which states that the proposed scheme is unable to support the provision of affordable housing whilst remaining deliverable.

3.17 The application is accompanied by the following supporting documents:

- Air Quality Assessment
- Arboricultural Report
- Daylight and Sunlight Report (amended 15/07/2020)
- Design & Access Statement
- Energy and Sustainability Statement
- Environmental Noise Survey and Acoustic Design Statement Report
- Financial Viability Assessment
- Flood Risk Assessment
- Basement Impact Assessment and Ground Investigation Report
- Planning Statement
- Structural Engineering Report and Outline Construction Method Statement
- SuDS Report
- Transport Statement

3.18 It should be noted that the scheme has been amended (13/07/2020) and originally proposed an alternative, curved roof form, with different elevational treatment. The layout of communal amenity space, parking and refuse storage has also been amended. The number of units has reduced from 30 to 26, with more three bedrooms proposed as a result.

4. PLANNING HISTORY

4.1 There is extensive planning history on the site, albeit the majority is not relevant to the current proposal. The most relevant history is summarised as follows:

4.2 Offices and bank approved in 1980's (multiple applications)

4.1 **Opposite the application site at the High Path estate:**

4.4 High Path Estate, South Wimbledon, SW19 2TG:

4.5 17/P1721 - OUTLINE PLANNING APPLICATION (WITH ALL MATTERS RESERVED, EXCEPT IN RELATION TO PARAMETER PLANS) FOR THE COMPREHENSIVE PHASED REGENERATION OF HIGH PATH ESTATE COMPRISING DEMOLITION OF ALL EXISTING BUILDINGS AND STRUCTURES; ERECTION OF NEW BUILDINGS RANGING FROM 1 TO 10 STOREYS MAX, PROVIDING UP TO 1570 RESIDENTIAL UNITS (C3 USE CLASS); PROVISION OF UP TO 9,900 SQM OF COMMERCIAL AND COMMUNITY FLOORSPACE (INC REPLACEMENT AND NEW FLOORSPACE, COMPRISING: UP TO 2,700 SQM OF USE CLASS A1 AND/OR A2, AND/OR A3 AND/OR A4 FLOORSPACE, UP TO 4,100 SQM OF USE CLASS B1 (OFFICE) FLOORSPACE, UP TO 1,250 SQM OF FLEXIBLE WORK UNITS (USE CLASS B1), UP TO 1,250 SQM OF USE CLASS D1 (COMMUNITY) FLOORSPACE; UP TO 600 SQM OF USE CLASS D2 (GYM) FLOORSPACE); PROVISION OF NEW NEIGHBOURHOOD PARK AND OTHER COMMUNAL AMENITY SPACES, INCL. CHILDREN'S PLAY SPACE; PUBLIC REALM, LANDSCAPING, LIGHTING; CYCLE PARKING (INCL VISITOR CYCLE PARKING) AND CAR PARKING (INC WITHIN GROUND LEVEL PODIUMS), ASSOCIATED HIGHWAYS AND UTILITIES WORKS. Grant Outline Planning Permission subject to S.106 Obligation. 29-04-2019.

5. Relevant policies.

5.1 The key policies of most relevance to this proposal are as follows:

5.2 **National Planning Policy Framework (2019)**

5. Delivering a sufficient supply of homes
6. Building a strong, competitive economy
7. Ensuring the vitality of town centres
8. Promoting healthy and safe communities
9. Promoting sustainable transport
11. Making effective use of land
12. Achieving well-designed places
14. Meeting the challenge of climate change, flooding and coastal change

5.3 **London Plan (2016)**

Relevant policies include:

- 2.6 Outer London: Vision and strategy
- 2.8 Outer London: Transport
- 3.3 Increasing housing supply
- 3.4 Optimising housing potential
- 3.5 Quality and design of housing developments
- 3.8 Housing choice

- 3.9 Mixed and balanced communities
- 3.10 Definition of affordable housing
- 3.11 Affordable housing targets
- 3.12 Negotiating affordable housing on individual private residential and mixed use schemes.
- 3.13 Affordable housing thresholds
- 5.1 Climate change mitigation
- 5.2 Minimising carbon dioxide emissions
- 5.3 Sustainable design and construction
- 5.10 Urban greening
- 5.11 Green roofs and development site environs
- 5.13 Sustainable drainage
- 5.14 Water quality and wastewater infrastructure
- 5.15 Water use and supplies
- 5.17 Waste capacity
- 5.21 Contaminated land
- 6.3 Assessing the effects of development on transport capacity
- 6.5 Funding Crossrail and other strategically important transport infrastructure
- 6.9 Cycling
- 6.10 Walking
- 6.11 Smoothing traffic flow and easing congestion
- 6.13 Parking
- 7.2 An Inclusive environment
- 7.3 Designing out crime
- 7.4 Local character
- 7.5 Public realm
- 7.6 Architecture
- 7.14 Improving air quality
- 7.15 Reducing and managing noise, improving and enhancing the acoustic environment and promoting appropriate soundscapes
- 8.2 Planning obligations
- 8.3 Community Infrastructure Levy

5.4 **Merton Local Development Framework Core Strategy – 2011 (Core Strategy)**

Relevant policies include:

- CS 8 Housing choice
- CS 9 Housing provision
- CS 11 Infrastructure
- CS 13 Open space, leisure and nature conservation
- CS 14 Design
- CS 15 Climate change
- CS 17 Waste management
- CS 18 Transport
- CS 19 Public transport
- CS 20 Parking servicing and delivery

5.5 **Merton Sites and Policies Plan – 2014 (SPP)**

Relevant policies include:

- DM H2 Housing mix
- DM H3 Support for affordable housing
- DM O2 Nature conservation, Trees, hedges and landscape features
- DM D1 Urban Design
- DM D2 Design considerations
- DM EP2 Reducing and mitigating noise
- DM EP3 Allowable solutions

DM EP4 Pollutants
DM F2 Sustainable urban drainage systems (SuDS) and; Wastewater and Water Infrastructure
DM T2 Transport impacts of development
DM T3 Car parking and servicing standards
DM T4 Transport infrastructure

5.6 **Supplementary planning considerations**

London Housing SPG 2016

DCLG - Technical housing standards – nationally described space standard 2015

London Affordable Housing and Viability SPG – 2017

London Character and Context SPG 2014

Merton Estates Local Plan 2018

Draft London and Local Plans

6. CONSULTATION

- 6.1 Press Notice, Standard 21-day site notice procedure and individual letters to neighbouring occupiers. Representations have been received from 6 individuals, raising objection on the following grounds:

Originally submitted scheme:

- Location of the refuse bins.
- Location of air source heat pumps.
- Overlooking to house to the rear.
- Loss of light to residential properties.
- Concerns that mature tree on site could not be retained.
- Consider removing the parking behind Barclays altogether and replacing it with a child play area.
- Noise disturbance from use of amenity space and air source heat pumps.
- Consider bringing design in line with Spur House.
- Adverse impact on the character of the area.
- We do not want six storey plus buildings becoming a common feature of South Wimbledon.
- Not clear whether the refuse store would be fully enclosed or not.
- Concern that outbuildings would allow for access for intruders to neighbouring gardens.
- Query whether hostile ground floor is suitable for a residential use.
- The proposed development would have a dominant and detrimental impact on the occupiers of Falcon House in terms of overlooking and loss of light to windows, many of which serve habitable rooms.
- The development would also be significantly out of scale with Falcon House due to this height difference.
- Significant potential for overlooking of windows in both the side and front elevations of Falcon House.
- The development would be contrary to Policy CS8 of the Merton Development Plan which requires at least 40% of dwellings to be Affordable Housing. Queries raised in relation to soundness/inflated costs of viability assessment.
- The development only has one staircase which would be a significant problem in the event of a fire.

- This would inevitably lead to residents and visitors parking in the car park for Falcon House, to the inconvenience and detriment of residents. No provision has been made in the application as to how this would be prevented. Would residents of the new development be subject to a S106 Agreement to ensure that they cannot apply for residents parking permits in the Borough?
- Additional congestion.
- Query whether applicant has a right to use the access road to gain entry to the site.

6.2 Since the application was amended on 13/07/2020, a further 4 objections have been received, objecting on the following grounds (in total, objections have been received from 9 address points):

- Changes do not overcome concerns.
- Overlooking, loss of light to windows at Falcon House.
- Increased noise levels.
- Significantly out of scale with Falcon House.
- Only one staircase is proposed, which would be a problem in the event of a fire.
- Insufficient car parking.
- Concern that windows may over-sail land under the ownership of Falcon House.
- Concern that large mature tree on site would not be retained.
- Overlooking and loss of privacy to houses to the rear.
- Concerns remain as to whether the applicant has a legal access to use the access road to the site.
- The area is already overcrowded.

6.3 1 general comment was received, which expressed support for on- street tree planting and suggested that the tree planting be continued to the frontage of Spur house to assist with road noise reduction.

6.4 **Internal consultees:**

6.5 **LBM Environmental Health Officer:**

No objection subject to conditions relating to noise levels and noise mitigation measures, external lighting, a Demolition and Construction Method Statement

6.6 **LBM Highway Officer:**

No objection subject to conditions, relating to Construction vehicles, Delivery and Servicing Plan and Construction Logistics Plan.

6.7 **LBM Transport Officer:**

No objection subject to conditions relating to the provision of vehicle parking, including passive charging points, provision of cycle parking, a Demolition and Construction Logistics Plan.

In addition, the applicant should enter in a Unilateral Undertaking which would restrict future occupiers of the units from obtaining an on-street residential parking permit to park in the surrounding controlled parking

zones to be secured by via S106 legal agreement and to provide free car club membership for all new residents for a period of three years.

6.8 LBM Flood Risk Engineer:

No objection raised subject to conditions relating to a detailed proposal of how drainage and groundwater will be managed and mitigated during and post construction (permanent phase), a detailed scheme for the provision of surface and foul water drainage.

5.8 LBM Climate Change Officer:

Specific technical queries raised in relation to SAP compliance reports and worksheets, “be lean” calculations and why a decentralised continuous mechanical extract ventilation system has been recommended.

Confirmed that the proposal would meet 105litres per person per day.

5.9 LBM Structural Engineer:

The submitted documents demonstrate that the proposed basement works can be undertaken safely without adversely affecting the surrounding built and natural environment.

Conditions are recommended in relation to:

- a Detailed Demolition Method Statement,
- a Detailed Construction Method Statement produced by the Contractors appointed for the piling, excavation and construction of the basement,
- Structural drawings of the secant piled retaining wall and construction sequence drawings of the temporary works,
- Design calculations of the secant piled and temporary works,
- Movement monitoring report produced by specialist surveyors appointed to install monitoring gauges to detect any movement of the highway/neighbouring properties from pre-construction to completion of the project works. a Detailed Demolition Method Statement,
- a Detailed Construction Method Statement produced by the Contractors appointed for the piling, excavation and construction of the basement,
- Structural drawings of the secant piled retaining wall and construction sequence drawings of the temporary works,
- Design calculations of the secant piled and temporary works,
- Movement monitoring report produced by specialist surveyors appointed to install monitoring gauges to detect any movement of the highway/neighbouring properties from pre-construction to completion of the project works.

5.10 LBM Tree and Landscape Officer:

No objection.

Conditions are recommended in relation to tree protection.

5.11 LBM Urban Design Officer (comments in relation to originally submitted scheme):

- The appearance and architecture in my mind cannot be described as high quality. It is unambitious, dated and does not derive from any good local context.
- The architecture of the emerging High Path regeneration can be viewed as a good emerging contextual clue.
- The curved roof is a discordant feature and is failing in any attempt at distinctiveness, which is wholly unnecessary in this regard. It is a particularly dated element.
- The positioning of the lift shaft at the front of the building creates a dead frontage and is a poor architectural feature that imbalances and rhythm such a building should have in its local context. It is also a dated typology.
- The ground floor of the building will have a poor residential environment and outlook on a busy and polluted road.
- Ground floor units have the private amenity space off the living area at the front of the building, which is unlikely to be used. There is rear amenity space, but this is inconveniently and inappropriately accessed through the private bedrooms and thus is also unsatisfactory.

Officer comment:

The concerns expressed in relation to the original scheme are noted. The applicant has amended the scheme and it is considered that the issues raised are sufficiently overcome to warrant the granting of planning permission. These matters are explained in more detail later in this report.

5.11 External consultees:

5.12 Design Review Panel – 30/01/2020 (an earlier version of the scheme)- The current scheme has not gone before the DRP

The Panel had a number of concerns regarding the design of this building and felt more work was required to ensure a quality building was built on this site.

It was felt there was a clear lack of a design narrative for the proposal, which should inform the design and appearance of the building. This was evident in the roof form and range of proposed materials. The appearance of the building was likened to a poor 1990s development. Regarding the roof, a range of poor quality examples were given, from non-contextual locations, whereas good quality examples from the locality should have been identified to inform an appropriate design response. The skyline was considered unsatisfactory, mostly due to the curved roof. The Panel felt that the materials palette was too varied and needed to be far more restrained – and again – be more contextual.

The Panel were concerned about having residential use on the ground floor, as this was a hostile environment for this use. Whilst stopping short of expressly stating this was inappropriate, they suggested that if this use was retained, some changes were required. It was suggested a deeper defensible space was needed and that a more solid acoustic barrier was

needed – a wall rather than vegetation. It was also felt the bedrooms would be better located at the rear.

The south elevation was also considered problematic as it has a bedroom window directly facing onto a secluded, publicly accessible space. It was felt this was susceptible to anti-social behaviour and a better solution was required. This led to further highlighting of the difficulty of creating successful ground floor residential use and a suggestion that the southern part of the ground floor at least, should be considered for non-residential use.

Another key element of the design the Panel were concerned about was the high proportion of single aspect units – 21 out of a total of 30 units. It was felt this was poor design and a long way from being in accordance with London Plan policy D4 E. Internally the party walls and layouts had scope for simplification, notably regarding internal party walls, to create more regular shaped flats. There was also a lack of a coherent narrative on the approach to meeting sustainability requirements and this needed more work.

It was also felt that the car park at rear of the building presented a harsh area of tarmac, which could also attract antisocial behaviour. It was suggested that the car park could be re-arranged to provide some ground level communal open space and provide more conveniently located bin storage. It was also suggested that the basement could be expanded slightly to accommodate cycle parking and provide bulky storage for flats.

The Panel noted the applicant's approach to the building alignment, but remained uncomfortable with bringing the elevation forward from that of the adjacent Spur House. This was particularly so regarding the need to maximise defensible space for ground floor residential units and also reducing the depth of the building. This was related to the single aspect units and the depth of the kitchen areas which it was felt would not receive much light and require artificial lighting.

The Panel were also unconvinced by the positioning of the lift shaft as it presented a blank frontage to the street. If this was reversed with the stair well a more attractive solution with windows could be presented to the street. The Panel also felt that more could be made of the entrance, expanding it to occupy one bay of the building in terms of its architectural expression. Overall the Panel felt a significant amount of development and revision was required to make the proposal acceptable.

VERDICT: RED

Officer comment:

The concerns expressed in relation to the original scheme are noted. The applicant has amended the scheme and it is considered that the issues raised are sufficiently overcome to warrant the granting of planning permission. These matters are explained in more detail later in this report.

5.13 TfL (original proposal):

TfL have the following comments.

1. The site of the proposed development is on the A129 Morden Road, which forms part of the Strategic Road Network (SRN). TfL has a duty under the Traffic Management Act 2004 to ensure that any development does not have an adverse impact on the SRN.

2. The site of the proposed development has a Public Transport Access Level (PTAL) of 6a on a scale ranging from 0 to 6b, where 6b represents the greatest level of access to public transport services. The site is served by 7 high frequency bus services within walking distance of the site, along with London Underground Services from South Wimbledon and Tramlink Services from Morden Road.

3. The applicant is proposing to provide 4 blue badge car parking spaces on site. In line with draft London plan standards, 3% of residential units should be provided with blue badge spaces, rising to 10% if the need arises. Therefore, the applicant should reduce the blue badge car parking spaces provided initially to one space, with information provided showing how a further 2 blue badge spaces could be provided in the future.

a. Due to the small number of car parking spaces provided on site, all spaces should be equipped with active electric charging provision.

b. All residents of the proposed residential units should be exempt from obtaining parking permits for local Controlled Parking Zones.

4. In line with draft London Plan policy T5, the applicant should provide a total of 36 long stay cycle parking spaces and 2 short stay spaces.

a. Further information regarding the design and layout should be provided regarding the cycle stores.

b. All cycle parking provision should be provided in line with the London Cycle Design Standards.

5. A full Construction Logistics Plan (CLP) should be provided to and discharged in conjunction with the LB Merton and TfL prior to any works taking place on site. TfL guidance should be followed when producing the CLP, and can be assessed here;

<http://content.tfl.gov.uk/constructionlogistics-plan-guidance.pdf> .

6. A full Delivery and Servicing Plan (DSP) should be provided to and discharged in conjunction with the LB Merton and TfL prior to the occupation of the proposed development.

TfL requests additional information is provided as outlined above prior to being supportive of the application.

Officer comment: The amendments made to the application are considered to have overcome the concerns set out. (This matter is addressed later in this report).

5.14 Metropolitan Police – Designing out Crime Officer:

Comments relating to the need for controlled access doors, cycle storage, defensible space around windows, design of air source heat pump enclosure (to eliminate misuse by climbing), the need for a CCTV system and lighting.

Conditions recommended in relation to security measures and a Secured by Design final certificate.

5.15 Merton Green Party:

Policy CS8 in the council's core planning strategy sets a borough-wide affordable housing target of 40% for developments of 10 or more units. The applicant's planning statement states (paragraph 6.23) that none of the 30 units will be affordable housing. We ask the Council to require that its 40% target be met.

5.16 External Financial Viability Consultant (Summary of comments):

We have considered the assumptions used in the Applicant's Residual Land Value calculation and how they compare to industry benchmarks and current economic factors and evidence. We have made appropriate adjustments and conclude the scheme cannot viability provide 40% affordable housing. Even based on 100% private tenure, the scheme is not viable.

5.17 Wimbledon Swift Group:

Advise that the development include Swift friendly features.

6. POLICY CONTEXT

6.1 National Planning Policy Framework (2019)

5. Delivering a sufficient supply of homes
6. Building a strong, competitive economy
7. Ensuring the vitality of town centres
8. Promoting healthy and safe communities
9. Promoting sustainable transport
11. Making effective use of land
12. Achieving well-designed places
14. Meeting the challenge of climate change, flooding and coastal change

6.2 London Plan (2016)

- 3.3 Increasing housing supply
- 3.4 Optimising housing potential
- 3.5 Quality and design of housing developments
- 3.6 Children and young people's play and informal recreation facilities
- 3.8 Housing choice
- 3.9 Mixed and balanced communities
- 3.10 Definition of affordable housing
- 3.11 Affordable housing targets
- 3.12 Negotiating affordable housing on individual private residential and mixed use schemes.
- 3.13 Affordable housing thresholds
- 4.1 Developing London's economy
- 4.7 Retail and town centre development
- 4.8 Supporting a successful and diverse retail sector and related facilities and services
- 5.1 Climate change mitigation
- 5.2 Minimising carbon dioxide emissions
- 5.3 Sustainable design and construction
- 5.7 Renewable energy
- 5.10 Urban greening
- 5.11 Green roofs and development site environs
- 5.13 Sustainable drainage

- 5.15 Water use and supplies
 - 5.17 waste capacity
 - 6.3 Assessing effects of development on transport capacity
 - 6.9 Cycling
 - 6.10 Walking
 - 6.13 Parking
 - 7.2 An inclusive environment
 - 7.3 Designing out crime
 - 7.4 Local character
 - 7.5 Public realm
 - 7.6 Architecture
 - 7.8 Heritage assets and archaeology
 - 7.14 Improving air quality
 - 7.15 Reducing and managing noise, improving and enhancing the acoustic environment and promoting appropriate soundscapes.
 - 7.21 Trees and woodland
 - 8.2 Planning obligations
 - 8.3 Community Infrastructure Levy
- 6.3 Merton Core Planning Strategy (July 2011)
- CS8 Housing Choice
 - CS9 Housing Provision
 - CS11 Infrastructure
 - CS12 Economic Development
 - CS13 Open Space, Nature Conservation, Leisure and Culture
 - CS14 Design
 - CS15 Climate Change
 - CS16 Flood Risk Management
 - CS17 Waste Management
 - CS18 Active Transport
 - CS19 Public Transport
 - CS20 Parking, Servicing and Delivery
- 6.4 Sites and Policies Plan and Policies Map (July 2014)
- DM H2 Housing mix
 - DM H3 Support for affordable housing
 - DM D1 Urban design and the public realm
 - DM D2 Design considerations in all developments
 - DM D7 Shop front design and signage
 - DM E1 Employment Areas in Merton
 - DM E3 Protection of scattered employment sites
 - DM E4 Local employment opportunities
 - DM O2 Nature Conservation, trees, hedges and landscape features
 - DM EP2 Reducing and mitigating noise
 - DM EP3 Allowable solutions
 - DM F2 Sustainable urban drainage systems (SuDS) and; Wastewater and Water Infrastructure
 - DM T1 Support for sustainable transport and active travel
 - DM T2 Transport impacts of development
 - DM T3 Car parking and servicing standards
 - DM T5 Access to the road network
- 6.5 Supplementary planning guidance.
- London Housing SPG – 2016
 - London Town Centres SPG – 2014
 - London Affordable Housing and Viability SPG – 2017
 - London Play and Informal Recreation SPG – 2012

London Sustainable Design and Construction - SPG 2014
London Character and Context SPG - 2014
GLA Guidance on preparing energy assessments - 2018
DCLG: Technical housing standards - nationally described space standard
March 2015
Merton's Design SPG 2004

7. **PLANNING CONSIDERATIONS**

7.1 Key Issues for consideration

7.1.1 The key issues in the assessment of this planning application are:

- Principle of development
- Need for additional housing, residential density and housing mix
- Affordable Housing
- Design and impact upon the character and appearance of the area
- Impact on trees
- Impact on neighbouring amenity
- Standard of accommodation
- Transport, highway network, parking and sustainable travel
- Sustainability
- Air quality and potentially contaminated land
- Basement considerations
- Flooding and site drainage
- S.106 requirements/planning obligations
- Response to issues raised in objection letters

7.2 Principle of development

7.2.1 The site is currently mixed use, comprising a vacant bank use on ground floor and ancillary office space above (previously known as A2 uses). It should be noted that as of 1st September 2020 the use as a bank and offices would comprise the new Class E (commercial).

7.2.2 There are no specific adopted policies which seek to protect A2 land uses. The site does not form part of a local centre or town centre where policies may seek to protect or in the event of redevelopment, promote non-residential uses at ground floor level.

7.2.3 The planning policy context is currently undergoing review and as the relevant stages in Plan preparation and assessment are reached greater weight will be accorded to the policies in the emerging revised local plan. For the moment weight should be properly accorded to the current Development Plan.

7.2.4 The National Planning Policy Framework 2018, London Plan 2016 Policy 3.3 and the Council's Core Strategy Policy CS9 all seek to increase sustainable housing provision where it can be shown that an acceptable standard of accommodation will also provide a mix of dwelling types dwellings at locations with good public transport accessibility. The site has a PTAL rating of 6a which is considered to be excellent.

7.2.5 The proposal would result in the provision of 26 residential units. Policy CS.9 of the Core Planning Strategy 2011 states that the Council will work with housing providers to facilitate the provision of a minimum of 4,800

additional homes for the period 2011-2026. The proposals would make a meaningful contribution to this target.

7.2.6 The site is considered to be underutilised and suitable for redevelopment; A2 uses are not specifically protected; the proposals would meet NPPF and London Plan objectives by contributing towards London Plan housing targets. Given the above, and having regard to the current policy circumstances, the principle of a fully residential scheme is considered to be acceptable.

7.2.7 Therefore, officers consider that the principle of development is acceptable, subject to the suitable resolution of design and technical considerations.

7.2.8 Notwithstanding this advice, it is important to note that Merton's emerging Local Plan proposes a new Local Centre at the heart of South Wimbledon focussed around the underground station and junction. The application site would fall within the proposed Local Centre.

7.2.9 The function of the Local Centre is, amongst other things, to

- Support shops, services and businesses commensurate with a local centre, particularly those that serve the day-to-day needs of local residents;
- Support measures to enhance the streetscene public realm in South Wimbledon, particularly along the main roads where most travel takes place;

7.2.10 The changing policy landscape is emerging and it is not yet clear what the status of the application site will be when the emerging Local Plan is adopted. However, on balance, officers consider that a fully residential scheme would be acceptable in principle, as the site does not currently sit within a Local Centre and therefore there is no policy basis to resist the principle of development.

7.3 Need for additional housing, residential density and housing mix

7.3.1 The National Planning Policy Framework (March 2018) requires Councils to identify a supply of specific 'deliverable' sites sufficient to provide five years' worth of housing with an additional buffer of 5% to provide choice and competition.

7.3.2 Policy 3.3 of the London Plan states that the Council will work with housing providers to provide a minimum of 4,107 additional homes in the borough between 2015 and 2025. Within this figure of 4,107 new homes, the policy states that a minimum of 411 new dwellings should be provided annually. This is an increase from the 320 dwellings annually that was set out in the earlier London Plan and in Policy CS9 of the Core Strategy. The policy also states that development plan policies should seek to identify new sources of land for residential development including intensification of housing provision through development at higher densities.

7.3.3 The draft London Plan is likely to significantly increase this figure to around 918 new homes annually. Therefore officers consider that significant weight should be given to optimising the housing output from this site. The scheme would make a valuable contribution towards the Council's housing stock.

- 7.3.4 Table 3.2 of the London Plan identifies appropriate density ranges based on a site's setting and PTAL rating. The proposed development would have a density of 337 dwellings per hectare and 870 habitable rooms per hectare.
- 7.3.5 The proposed density is higher than the maximum relevant density range (200-700 habitable rooms per hectare 70-260 dwellings per hectare) as set out in Table 3.2 for the setting (Urban) and PTAL 6A.
- 7.3.6 The numerical density is not the only determining factor but serves as an indication that the design merits of the proposals require rigorous and careful scrutiny if the Council is to be persuaded that the quantum of development is appropriate. In this case, it is noted that the bulk and massing would be lower than the adjacent Spur House and officers consider that the overall bulk and massing would be acceptable.
- 7.3.7 Introduction of non-residential floorspace on the ground floor would bring the density closer to the maximum of the relevant density range given the site's accessibility. However, such an adjustment would not in itself alter the bulk and massing of the scheme and may result in empty floorspace and dead frontage. Given the likely step change in housing targets officers consider that density guidelines should not be applied slavishly in such an accessible location and in proximity to taller buildings also in residential use.
- 7.3.8 In terms of housing mix, the scheme provides 7 x three bedroom units (27% of the overall provision).
- 7.3.9 Policy DM H2 sets out that residential development proposals will be considered favourably where they contribute to meeting the needs of different households such as families with children, single person households and older people by providing a mix of dwelling sizes, taking account of the borough level indicative proportions concerning housing mix.
- 7.3.10 The supporting text to the policy explains that research in London and in Merton shows that there is an overwhelming need in London and in Merton for all types and sizes of new homes, which must be balanced against the need for supporting infrastructure. Assessment of historical provision in the borough indicates a disproportionately greater delivery of smaller homes compared to larger homes: 84% of dwellings completed in the borough between April 2000 and March 2011 consisted of 1 or 2 bedroom units.
- 7.3.11 In assessing development proposals the council will take account of Merton's Housing Strategy (2011-2015) borough level indicative proportions which are set out as follows:

Number of bedrooms	Percentage of units
One	33%
Two	32%
Three +	35%

- 7.3.12 The borough level indicative proportions concerning housing mix will be applied having regard to relevant factors including individual site

circumstances, site location, identified local needs, economics of provision such as financial viability and other planning contributions. Where a developer considers a site unsuitable to apply the borough level indicative housing mix, set out above, the developer will be responsible for demonstrating why this is the case.

7.3.13 The proposed development has incorporated family sized units in the form of three bedroom flats and whilst it does not directly meet the indicative borough mix proportions, the provision of family sized units is welcomed by officers. Importantly, all of the ground floor units would provide for family housing, with access to some extent of external amenity space.

7.3.14 The proposed development is considered to be acceptable in terms of density and housing mix.

7.4 Affordable Housing

7.4.1 The Council's policy on affordable housing is set out in the Core Planning Strategy, Policy CS8. For schemes providing over ten units, the affordable housing target is 40% (of which 60% should be social rented and 40% intermediate), which should be provided on-site.

7.4.2 The Mayor's SPG on affordable housing and viability (Homes for Londoners) 2017 sets out that:

"Applications that meet or exceed 35 per cent affordable housing provision (by habitable rooms) without public subsidy, provide affordable housing on-site, meet the specified tenure mix, and meet other planning requirements and obligations to the satisfaction of the LPA and the Mayor where relevant, are not required to submit viability information. Such schemes will be subject to an early viability review, but this is only triggered if an agreed level of progress is not made within two years of planning permission being granted (or a timeframe agreed by the LPA and set out within the S106 agreement)...

... Schemes which do not meet the 35 per cent affordable housing threshold, or require public subsidy to do so, will be required to submit detailed viability information (in the form set out in Part three) which will be scrutinised by the Local Planning Authority (LPA)."

7.4.3 If the proposal does not meet this 35% provision, it will be necessary to submit a full viability assessment in order to demonstrate that the scheme is delivering as much affordable housing as is financially viable.

7.4.4 The current scheme offers no affordable housing whatsoever, on the basis that it is not financially viable to do so.

7.4.5 The Council has employed an external financial viability consultant who has considered the evidence put forward by the applicant and concludes that the scheme is not able to provide any contribution towards affordable housing. Whilst this is disappointing, the information has been reviewed by the external financial viability consultant and as such, it would not be reasonable to resist the application on this basis. A legal agreement is recommended to ensure that a review mechanism is included to capture any potential uplift in profit.

7.5 Design and impact upon the character and appearance of the area

- 7.5.1 The National Planning Policy Framework (NPPF) states that planning should always seek to secure high quality design and a good standard of amenity for all existing and future occupants of land and buildings. London-wide planning policy advice in relation to design is found in the London Plan (2015), in Policy 7.4 - Local Character and 7.6 - Architecture. These policies state that Local Authorities should seek to ensure that developments promote high quality inclusive design, enhance the public realm, and seek to ensure that development promotes world class architecture and design.
- 7.5.2 Policy DM D2 seeks to ensure a high quality of design in all development, which relates positively and appropriately to the siting, rhythm, scale, density, proportions, height, materials and massing of surrounding buildings and existing street patterns, historic context, urban layout and landscape features of the surrounding area. Core Planning Policy CS14 supports these SPP Policies.
- 7.5.3 The current built form on site represents an under development of the site. The buildings along Morden Road in this location vary in height from 4 storeys to 9 storeys, with buildings permitted up to 9 storeys opposite the site at the High Path Estate and, therefore, officers consider that a building of the height proposed could be supported.
- 7.5.4 The site stands in a transition area, in terms of building heights. To the immediate north is Spur House, a nine storey flatted block; to the south is a four storey flatted block, with more traditional two-storey residential dwellings beyond. Officers consider that the scale and design of the building sufficiently takes account of this transition and responds positively to it.
- 7.5.5 The redevelopment of the site provides an opportunity to enhance the streetscape and whilst a more active use at ground floor level may appear more animated, the impact on the streetscene would be improved and is considered to be acceptable.
- 7.5.6 The design and form of the proposed building has gone through a number of changes throughout the pre-application and Design Review Panel process and amended plans have been submitted throughout the course of this application in response to officer comments/concerns.
- 7.5.7 The visual impact of the proposed building is considered to relate positively to the streetscene in terms of form and design. The proposed building would not appear out of keeping with the existing neighbouring buildings and would not appear out of keeping with the new development being brought forward at the High Path Estate, opposite the site.
- 7.5.8 Whilst overall, the visual impact of the building is considered to be acceptable, as a matter of judgement it is considered that the southern elevation fails to take full advantage of the opportunity to enhance the streetscape and could be considered to present a large, somewhat inactive edge when approaching from the south. However, there is some articulation through the fenestration and, on balance, the quality or otherwise of this element of the scheme is not considered to warrant a refusal on the basis of design.
- 7.5.9 Whilst concerns were initially raised in relation to the lack of a convincing design rationale for curved copper roof, this element of the design has now

been omitted, along with the cumbersome projecting out-shot to house the lift shaft and the current proposal is considered to respond reasonably well to the rhythm and context of the existing streetscene.

7.5.10 The submission indicates that the partly recessed balconies to the front elevation would be part enclosed winter gardens. The use of winter gardens, and recessed balconies, as opposed to projecting balconies, is considered to be suitable along this busy road.

7.5.11 The set back at ground floor level, with an area of soft landscaping for defensible space, is considered to be a suitable design response to a fully residential scheme.

7.5.12 In terms of internal layout, the provision of individual entrances to ground floor units is supported as this would create some activity and natural surveillance at ground floor level.

7.5.13 The proposed layout of amenity space to the rear of the building has been amended since the scheme's initial conception and now includes a regular shaped area of land that would be enclosed by the proposed bin store/air source heat pump building, whilst being overlooked by the proposed development and surrounding properties. This solution to providing both car parking and amenity space is considered to be a good use of the available space on the site.

7.5.14 The scheme shows a row of street trees to the frontage of the site, which would be on land owned by the applicant. There is an intention that this street undergo additional street planting and it is not clear at this stage how the proposed planting in the application would relate to the street tree planting scheme. However, this is a matter that can be addressed through conditions and would not affect the overall acceptability of this proposal.

7.5.15 The proposal is considered to provide a suitable transition between the taller buildings to the north and the lower, more residential scale buildings to the south. The proposal would not be out of keeping with the scheme to redevelop the High Path Estate. The proposal is considered to be acceptable in terms of its impact on the character and appearance of the area.

Response to DRP Comments:

7.5.17 It is noted that the proposal has been amended substantially following concerns raised by the DRP. The copper curved roof has been omitted from the design, along with the lift out-shot and the currently proposed building is considered to provide a less conspicuous skyline, with a better contextual response.

7.5.18 The layout has been amended to provide the main living space of all ground floor units to the front of the building (as opposed to some units originally presenting bedrooms to the front only), following concerns raised by the DRP due to and it is considered that this would present a more active frontage to Morden Road. Whilst the ground floor residential use would still about a somewhat hostile environment, the setback and use of screening is considered to mitigate the impact sufficiently. The south facing bedroom window has also been omitted following concerns raised by the DRP.

- 7.5.19 In terms of single aspect units, this part of the scheme has been amended and now allows for a greater proportion of dual aspect units (this is discussed in more detail later in this report under 'Standard of Accommodation').
- 7.5.20 The arrangement of the rear amenity space, bin storage and parking has been amended following the suggestion of the DRP and now provides for a more safe, secure and defensible space.
- 7.5.21 The building no longer stands forward of Spur House in response to the concern raised by the DRP.
- 7.5.22 The projecting lift shaft out-shot to the front elevation, initially proposed, has been omitted from the scheme, in favour of a more suitable elevational treatment.
- 7.5.23 Officers consider that the applicant's revised scheme has responded well to the comments of the DRP in terms of its form and design. In terms of the ground floor residential use, as explained earlier in this report, officers consider that there is not a sufficient policy basis to resist the application on this ground.
- 7.5.24 The design of the building has responded positively to comments made by the Urban Design Officer (as noted above these comments need tempering) and the Design Review Panel and officers consider that the proposal would make a more positive contribution to the streetscene.

7.6 Impact on trees

- 7.6.1 There is a significant mature Lime tree to the rear of the site standing on a grassed area adjacent to the access road. Officers consider that this tree has substantial visual public amenity value. The proposed works would not interfere with this grassed area and the tree would be retained throughout and following the development.
- 7.6.2 The Tree and Landscape Officer has reviewed the submitted arboricultural report and raises no objection subject to a safeguarding condition.
- 7.6.3 The proposal is considered to be acceptable in terms of its impact on trees.

7.6 Impact on neighbouring amenity

- 7.6.1 Policy DM D2 seeks to ensure that development does not adversely impact on the amenity of nearby residential properties.
- 7.6.2 The scheme proposes a substantial increase in height of the building in comparison to the existing (the existing building is 11.2m in height, with the proposed building being a maximum of 20.7m in height, to the top of the lift shaft and 16.5m to the top of the parapet to the rear elevation). It is noted that the original scheme has been amended to include less bulk, massing and height to the rear part of the proposed building.
- 7.6.3 The site is surrounded on three sides by residential properties. To the immediate rear of the site is a row of terraced dwellings (22-24A Morden Road), which face towards the application site. To the north is Spur House, part eight, part nine storey building, with a lower out-shot to the rear, (part

three, part storeys). To the south, to the other side of the access road is 26 Morden Road (Falcon House), a four storey flatted block.

7.6.4 *22-24A Morden Road – row of terraced dwellings to the rear:*

7.6.5 The existing building on site is three storeys and therefore the houses to the rear currently enjoy a relatively unimpeded outlook to the front. The proposed development would present a rear elevation to a height of 16.5m, separated from the houses to the rear by 19.5m. The rear wall of the existing building stands at a height of 8.5m-11.2m.

7.6.6 The applicant's Daylight and Sunlight Report concludes that the windows to the dwellings to the rear would be affected to some limited degree but argue that ample light would be provided to the ground floor living area due to the dual aspect layout of the ground floor of the terraced houses. The bedrooms at first floor level would also experience a minor loss of light but this impact is limited and the relationship created is not considered to be unacceptable in this urban context.

7.6.7 Whilst officers note that there would be some limited loss of light and outlook to properties to the rear, the separation distance is considered sufficient to avoid a materially harmful impact.

7.6.8 In terms of overlooking, the separation distance of 19.5m is considered to be sufficient to avoid a loss of privacy and in line with frequently applied separation thresholds.

7.6.9 *Spur House:*

7.6.10 The part eight, part nine storey element of Spur House would not be adversely affected as it stands in line with the proposed building and would form part of a continuous street frontage. The three storey element to the rear has the potential to be affected. Similarly with the impact on the terraced dwellings to the rear, there would be some change to outlook and some marginal loss of light. However, the relationship created would not be unusual in this urban context.

7.6.11 There would be the opportunity for some oblique overlooking from the rear facing windows of the proposed building to the side facing windows of the rear part of Spur House. However, this arrangement is not dissimilar to the existing relationship between Spur House and the terraced dwellings to the rear of the site and whilst there would be significantly more bulk and massing in close proximity to these existing units, due to the oblique positioning, the impact is not considered to result in a material loss of privacy.

7.6.12 The separation distance to residential properties, such as Falcon House to the south, 7 Milner Road to the northwest and the flatted blocks opposite at the High Path estate is considered to be sufficient to avoid a materially harmful impact.

7.6.13 It is noted that the scheme has been amended since its initial conception, with a reduced height to the rear to seek to minimise the impact on neighbouring amenity. Officers acknowledge that the increase in bulk and massing would result in some limited harm to the outlook and light of neighbouring properties to the rear and side, however, officers conclude

that this relationship would not result in material harm to residential amenity and would not be unusual in this urban context.

7.7 Standard of Accommodation

- 7.7.1 The detailed design of the proposed development should have regard to the requirements of the London Plan (2016) in terms of unit and room sizes and provision of external amenity space. The requirements of SPP Policy DM D2 will also be relevant in relation to the provision of amenity space (see paragraph 6.17 of the supporting text).
- 7.7.2 The proposed units would meet or exceed the minimum GIA set out in the London Plan.
- 7.7.3 The amount of private external amenity space provided would meet the minimum requirements of the London Plan and no objection is raised in this regard.
- 7.7.5 The provision of external amenity space is considered to be acceptable. The provision of amenity space to the rear, would provide areas that are well over-looked and secure and would provide high quality amenity space for future residents.
- 7.7.6 Officers advise that a scheme for landscaping and to secure the provision of suitable play equipment and ongoing maintenance should be controlled by way of planning condition in the event that planning permission is granted.
- 7.7.7 At least 10% of units should be wheelchair accessible. In addition, Standard 18 of the Mayor's SPG on Housing sets out that each designated wheelchair accessible dwelling should have a car parking space that complies with Building Regulations Part M4(3). The plans show there to be 3 wheelchair accessible units with 3 disabled parking spaces and therefore no objection is raised in this regard.
- 7.7.8 The standard of accommodation is considered to be acceptable.

7.8 Transport, highway network, parking and sustainable travel

- 7.8.1 Policy 6.1 of the London Plan (2016) states that the Mayor will support developments, which generate high levels of trips at locations with high levels of public transport accessibility and which improves the capacity and accessibility of public transport, walking and cycling. At a local level Policy CS.19 of the Core Planning Strategy states that the council will ensure that all major development demonstrates the public transport impact through transport assessments. Travel plans will also be required to accompany all major developments. Policy CS.18 promotes active transport and encourages design that provides attractive, safe, covered cycle storage, cycle parking and other facilities (such as showers, bike cages and lockers).
- 7.8.2 There are double yellow lines on the road immediately outside the site on both sides of the road, along with a designated bus lane running in a northerly direction. There is no parking permitted on Morden Road.
- 7.8.3 The site is located within Controlled Parking Zone (CPZ), Subzone S1, where restrictions operate between 08:30 and 18:30, Monday to Saturday. Milner Road nearby provides a mix of permit holder bays and pay & display.

7.8.4 The site is within a high PTAL area and therefore it is appropriate that car parking on site is limited to disabled users only. It will be necessary for the applicant to enter into a s.106 to restrict the issuing of parking permits, to future occupants, so as to avoid undue additional pressure on kerbside parking locally. In addition, passive electrical charging should be provided, which can be secured by way of condition.

7.8.5 The scheme would meet London Plan requirements in terms of cycle parking and no objection is raised on this basis.

7.8.6 In terms of refuse collection, there would be adequate space to accommodate the refuse storage requirements for the development and no objection is raised in this regard. The Council's Transport Planner has confirmed that the proposed development would be serviced by London Borough of Merton refuse vehicles and the proposed arrangements are acceptable.

7.8.7 Subject to legal agreement and conditions, the proposed development is considered to be acceptable in term of transport and highway impacts.

7.10 Sustainability

7.10.1 Policy 5.2 Minimising Carbon Dioxide Emissions of the London Plan requires that development proposals should make the fullest contribution to minimising carbon dioxide emissions in accordance with the Mayor's energy hierarchy. Merton's Core Planning Strategy Policy CS15 Climate Change (parts a-d) requires new developments to make effective use of resources and materials, minimise water use and CO2 emissions.

7.10.2 An Energy & Sustainability Statement has been submitted with the application. This statement sets out that in order to comply with policy 5.2 of the London Plan a reduction in CO2 emissions of 35.25% has been achieved against the Baseline Emission Rate, including through the use of air source heat pumps on site.

7.10.3 The Council's Climate Change Officer has reviewed the submission and confirms that the scheme would meet the limit of 105 litres per person per day water usage. However, the officer has raised queries on specific, technical matters relating to energy usage. However, these are matters that can be reasonably addressed by way of condition, as it is considered that the scheme has provided detail on the sustainability credentials, including the incorporation of air source heat pumps and therefore these matters will be considered in the detailed design and construction of the building.

7.10.4 Subject to condition, the proposal is considered to be acceptable in terms of sustainability and climate change considerations.

7.11 Air quality and potentially contaminated land

7.11.1 The whole of Merton is an Air Quality Management Area (AQMA).

7.11.2 The application is supported by an air quality assessment, which concludes that there would be a reduction in traffic related emissions due to the decrease in vehicle movements (over and above the lawful use of the site). Officers note that only limited car parking has been provided, which is positive in terms of air quality. Subject to suitable conditions to control the construction process (demolition and construction method statement and a

limit on noise levels from plant/machinery), it is considered that the proposed development would be acceptable in terms of its impact on air quality.

7.11.3 In addition, conditions would be imposed relating to any potential contamination of the land on the site, to include remediation measures if necessary.

7.12 Basement considerations

7.12.1 The proposed development includes a basement and whilst the construction of basements is largely addressed under Building regulations, in accordance with the requirements of Policy DMD2 the applicant has provided a Structural Engineering Report and Outline Construction Method Statement detailing how the basement could be constructed to pose no significant threat to the structural stability of adjoining properties.

7.12.2 The Council's Structural Engineer has reviewed the submitted documents and raises no objection subject to suitable conditions.

7.13 Flooding and site drainage

7.13.1 London Plan policies 5.12 and 5.13, CS policy CS16 and SPP policies DM F1 and DM F2 seek to minimise the impact of flooding on residents and the environment and promote the use of sustainable drainage systems to reduce the overall amount of rainfall being discharged into the drainage system and reduce the borough's susceptibility to surface water flooding.

7.13.2 The site is within Flood Zone 1 (low probability of flooding) and is not within a critical drainage area. However, notwithstanding that, the scheme includes details of a Sustainable Urban Drainage System and demonstrate a sustainable approach to the management of surface water on site.

7.13.3 The Council's Flood Risk Officer have raised no objection and the proposal is considered to be acceptable in terms of surface water runoff and flooding considerations.

7.14 S.106 requirements/planning obligations

7.14.1 It will be necessary for the development to be parking permit free and to provide three years free car club membership, by way of legal agreement.

7.14.2 A clause in the legal agreement is recommended to ensure that a review mechanism is included to capture any potential uplift in profit, as affordable housing contributions.

7.14.3 The proposed development would be subject to the Community Infrastructure Levy (CIL). This would require a contribution of £220 per additional square metre of floor space to be paid to Merton Council and an additional £60 per additional square meter to be paid to the Mayor. Further information on this can be found at:
<http://www.merton.gov.uk/environment/planning/cil.htm>

7.15 Response to issues raised in objection letters

The majority of issues raised by objectors are addressed in the body of this report and a number of issues relate to the original application scheme,

rather than the amended scheme. However, in addition, the following comments are provided:

- Any noise disturbance from air source heat pumps would be minimal as they are to be entirely enclosed. Use of the amenity area would not amount to material harm to residential amenity.
- The refuse and air source heat pump enclosure would be fully roofed.
- The amended positioning of the proposed refuse and air source heat pump enclosure would not give rise to increased opportunity for burglaries (it is now moved further from the direct boundary with residential properties).
- Issues of fire safety would be primarily addressed at the Building regulations stage of the development, as opposed to the planning stage. However, the emerging London Plan includes requirements relating to fire safety and as such a condition is recommended in relation to fire safety measures, (which may include measures such as 'fire evacuation lifts', fire assembly points, fire alarm systems, passive and active fire safety measures, means of escape and associated evacuation strategy).
- Issues relating to the use of the access road to the site are not a material planning consideration – planning permission does not convey an ultimate right to develop land and if there are other legal obstacles, the granting of planning permission would not overcome these legal obstacles.

8. Conclusion

- 8.1 The principle of a residential use on this site, including at ground level, is considered to be acceptable. The scheme would provide a range of unit sizes, including family sized units with private external amenity space, in addition to communal amenity space.
- 8.2 The proposal, as a result of the increased height over the existing, would result in some limited impact on properties to the side and rear of the site. However, as explained in this report, the impact is considered to be minimal and would not warrant a reason for refusal in this urban context, whereby there is a reasonable expectation that a building fronting a main road such as this would be enlarged.
- 8.3 The applicant has gone to effort to seek to overcome the concerns initially raised by the Council's Urban Design Officer and the Design Review Panel and it is concluded that the proposed building would be of an acceptable architectural quality and would complement this part of the streetscene creating a suitable transition between Spur house and the lower buildings to the south.
- 8.4 Whilst officers are disappointed at the lack of affordable housing provided, this matter has been reviewed by an external expert who concludes that due to the residual value of the existing site, the proposed development could not yield any affordable housing contribution, and, therefore, this matter could not reasonably form a reason for refusal. However, the legal agreement includes a clause to ensure that a further financial viability assessment is carried out at the implementation stage to identify whether

the financial viability of the scheme has altered, which may then allow for any uplift in profit to be captured as affordable housing contributions.

- 8.5 Officers consider that the proposal is acceptable in planning terms, subject to conditions and a legal agreement and therefore the recommendation is for approval.

9. RECOMMENDATION

Grant planning permission subject to s106 agreement securing the following:

- Restrict parking permits.
- Car club membership for all eligible adults for three years.
- Financial viability review mechanism.
- and cost to Council of all work in drafting S106 and monitoring the obligations.

And the following conditions:

1. Time limit
2. Approved Plans
3. B1 External Materials to be Approved
4. B4 Details of surface treatment
5. B6 Levels
6. C03 Obscured Glazing (Fixed Windows)
7. C07 Refuse & Recycling (Implementation)
8. C06 Waste Management Plan (Details to be Submitted)
9. C08 No Use of Flat Roof
10. C10 Balcony or External Staircase (Screening details to be provided)
11. D09 No External Lighting
12. F01 Landscaping/Planting Scheme
13. F02 Landscaping (Implementation)
14. F5 Tree Protection
15. F8 Site supervision (trees)
16. F13 Landscape Management Plan
17. H06 Cycle Parking and workshop facility - Details to be Submitted
18. H01 New Vehicle Access - Details to be submitted
19. H02 Vehicle Access to be provided
20. H04 Provision of Vehicle Parking (including disabled parking and electric vehicle charging)
21. H05 Visibility Splays
22. H08 Travel Plan
23. H10 Construction Vehicles, Washdown Facilities etc (major sites)
24. H12 Delivery and Servicing Plan
25. H13 Construction Logistics Plan to be Submitted (major development)
26. H14 Doors/Gates
27. H11 Parking Management Strategy
28. L2 Sustainability - Pre-Commencement (New build residential)
29. L6 BREEAM - Pre-Commencement (New build non-residential)
30. A Non Standard Condition: The recommendations to protect noise intrusion into the residential dwellings and plant noise criteria as specified in the Sandy Brown, Noise Impact Assessment Report 18404-R01-B, Scheme A, dated 27 March 2019 shall be implemented as a minimum standard for the development. A post

- construction noise survey shall be conducted within 3 months of occupation and any necessary remedial measures implemented should the submitted criteria fail to be achieved. The remedial measures shall be first agreed in writing by the LPA.
31. A Non Standard Condition: Noise levels, (expressed as the equivalent continuous sound level) LAeq (10 minutes), from any fixed external new plant/machinery shall not exceed LA90-10dB at the boundary with any residential property or noise sensitive premises.
 32. A Non Standard Condition: All Non-road Mobile Machinery (NRMM) used during the course of the development that is within the scope of the Greater London Authority 'Control of Dust and Emissions during Construction and Demolition' Supplementary Planning Guidance (SPG) dated July 2014, or any subsequent amendment or guidance, shall comply with the emission requirements therein.
 33. A Non Standard Condition: No development approved by this permission shall be commenced until a detailed scheme for the provision of surface and foul water drainage has been implemented in accordance with details that have been submitted to and approved in writing by the local planning authority. The drainage scheme will dispose of surface water by means of a sustainable drainage system (SuDS) via infiltration or at the agreed runoff rate (no more than 4.02l/s), in accordance with drainage hierarchy contained within the London Plan Policy (5.12, 5.13 and SPG) and the advice contained within the National SuDS Standards.
 34. A Non Standard Condition: The development hereby permitted shall incorporate security measures to minimise the risk of crime and to meet the specific security needs of the development in accordance with the principles and objectives of Secured by Design. Details of these measures shall be submitted to and approved in writing by the local planning authority prior to commencement of the development and shall be implemented in accordance with the approved details prior to occupation.
 35. A Non Standard Condition: Prior to occupation a Secured by Design final certificate shall be submitted to and approved by the Local Planning Authority.
 36. A Non Standard Condition: If, during development, contamination not previously identified is found to be present at the site then no further development (unless otherwise agreed in writing with the Local Planning Authority) shall be carried out until the developer has submitted, and obtained written approval from the Local Planning Authority for, a remediation strategy detailing how this unsuspected contamination shall be dealt with. The remediation strategy shall be implemented as approved, verified and reported to the satisfaction of the Local Planning Authority.
 37. A Non Standard Condition: The development hereby approved shall not be commenced until a scheme for the provision and management of external amenity space, to include details of children's play equipment, has been submitted to and approved in writing by the Local Planning Authority. The development shall not be occupied until the agreed facilities and management plan are implemented in accordance with the approved details.
 38. A Non Standard Condition: No development above ground level other than demolition shall take place until drawings to a scale of not less than 1:20 and samples and/or manufacturer's specifications of the design and construction details listed below have been submitted to and approved in writing by the Local Planning Authority. The

development shall thereafter be carried out solely in accordance with the approved details.

- i) metal, glass and wood work including to private amenity spaces and balconies;
- ii) all external window and door systems (including technical details, elevations, plans and cross sections showing cills and reveal depths);
- iii) copings and soffits and junctions of external materials;
- iv) rain water goods (including locations, fixings, material and colour).

39. J2 Wheelchair Accessible Homes

40. D11 Construction Times

41. A Non Standard Condition: Prior to occupation, the detailed design, specification and planting scheme for any green roof forming part of the development hereby approved shall be submitted to and approved in writing by the Local Planning Authority. The design and planting shall be carried out as approved prior to occupation of the relevant part of the development, retained and maintained in perpetuity thereafter.

42. A Non Standard Condition: [Local employment strategy] Prior to the commencement of development [including demolition] a local employment strategy shall have been submitted to and approved in writing by the Local Planning Authority setting out the measures taken to ensure that the development provides employment opportunities for residents and businesses in Merton during the construction phase.

43. Prior to the commencement of development the following documents shall be submitted to and approved in writing by the Local Planning Authority, in consultation with London Underground:

- a Detailed Demolition Method Statement,
- a Detailed Construction Method Statement produced by the Contractors appointed for the piling, excavation and construction of the basement,
- Structural drawings of the secant piled retaining wall and construction sequence drawings of the temporary works,
- Design calculations of the secant piled and temporary works,
- Movement monitoring report produced by specialist surveyors appointed to install monitoring gauges to detect any movement of the highway/neighbouring properties from pre-construction to completion of the project works. a Detailed Demolition Method Statement,
- a Detailed Construction Method Statement produced by the Contractors appointed for the piling, excavation and construction of the basement,
- Structural drawings of the secant piled retaining wall and construction sequence drawings of the temporary works,
- Design calculations of the secant piled and temporary works,
- Movement monitoring report produced by specialist surveyors appointed to install monitoring gauges to detect any movement of the highway/neighbouring properties from pre-construction to completion of the project works.

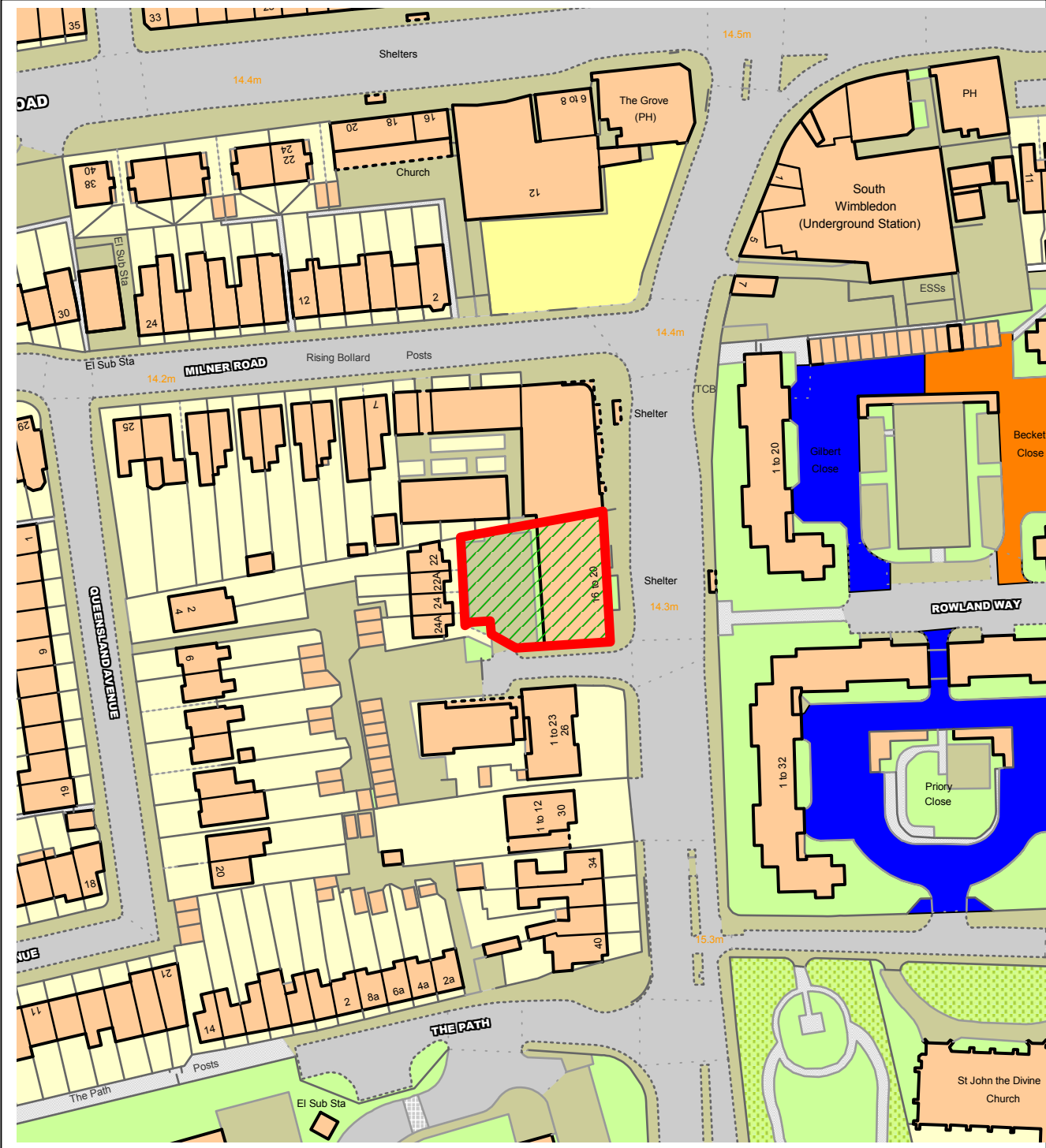
The development shall be carried out in accordance with the approved details.

44. Prior to the commencement of development a Fire Safety Strategy shall be submitted to and approved in writing by the Local Planning Authority. The applicant shall confirm in writing, to the Local Planning

Authority, prior to the first occupation that the development has been carried out in accordance with the agreed strategy.

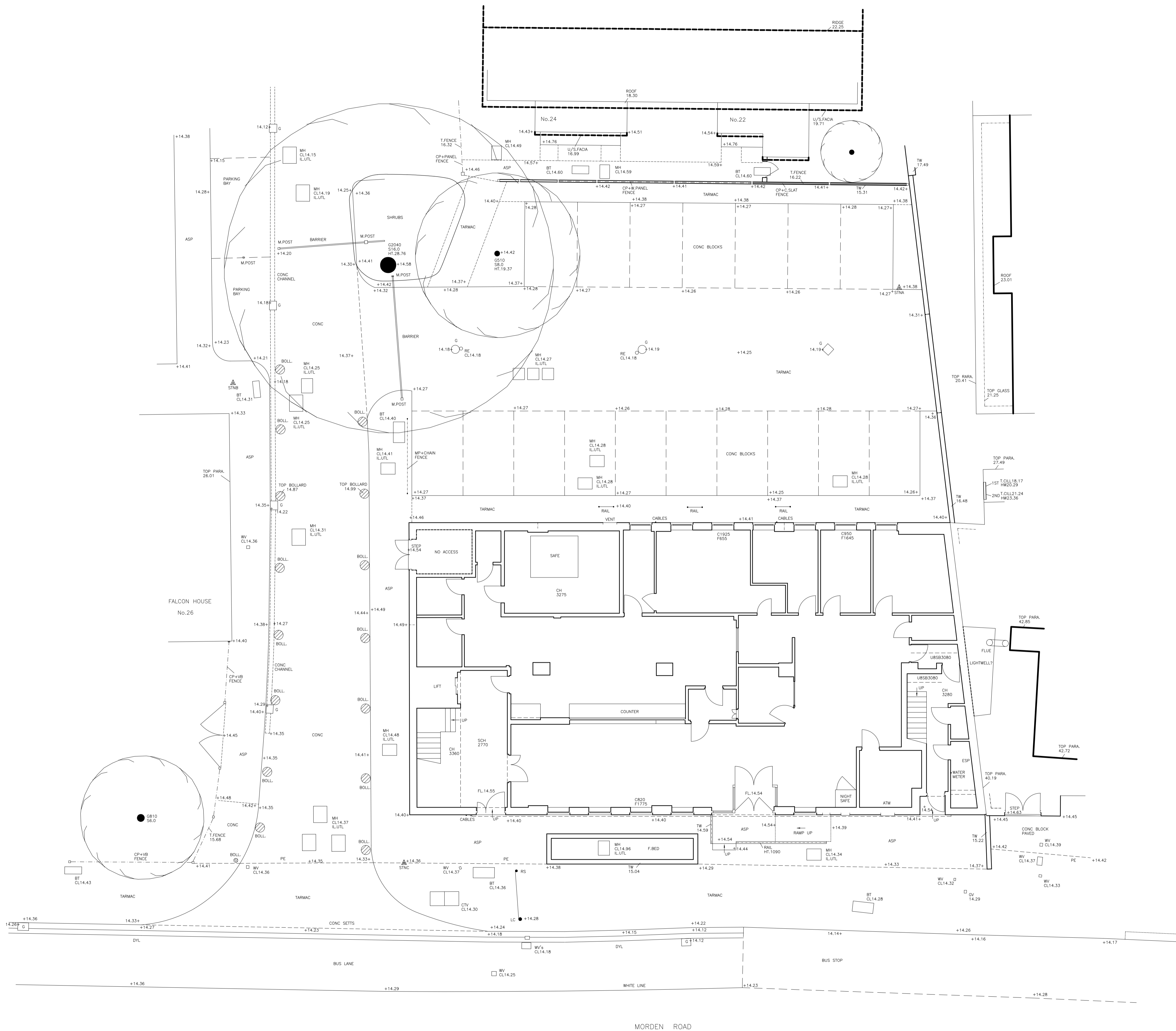
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NORTHGATE SE GIS Print Template



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W.P.G.SURVEYS
LAND SURVEYORS
P.O. BOX 10044
SURREY, SURREY, KT1 9AL
TEL: 020 8397 8361

SITE
16-20 MORDEN ROAD
SOUTH WIMBLEDON
SW19 3BN

DRAWING
SITE SURVEY & GRD
FLOOR LAYOUT PLAN

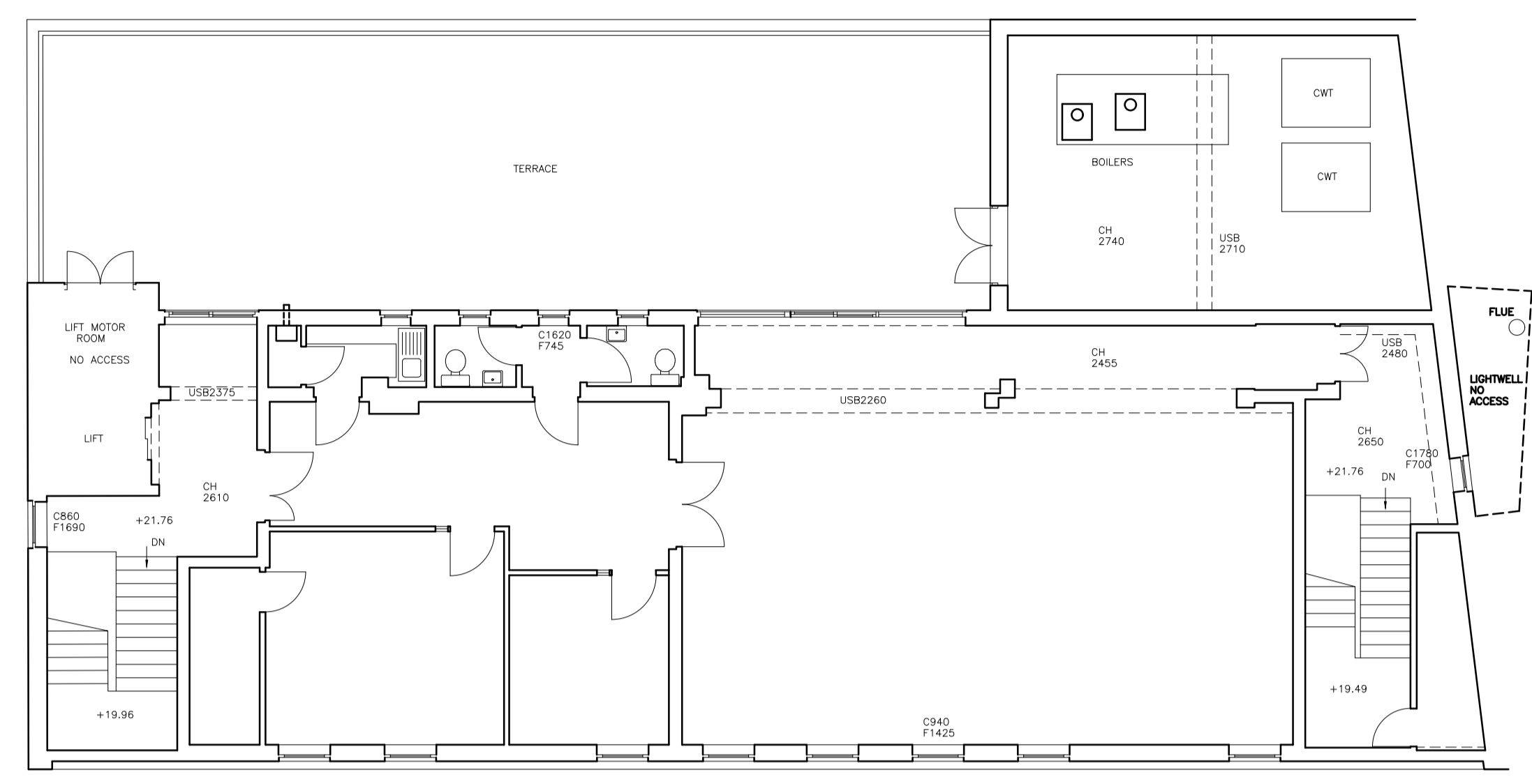
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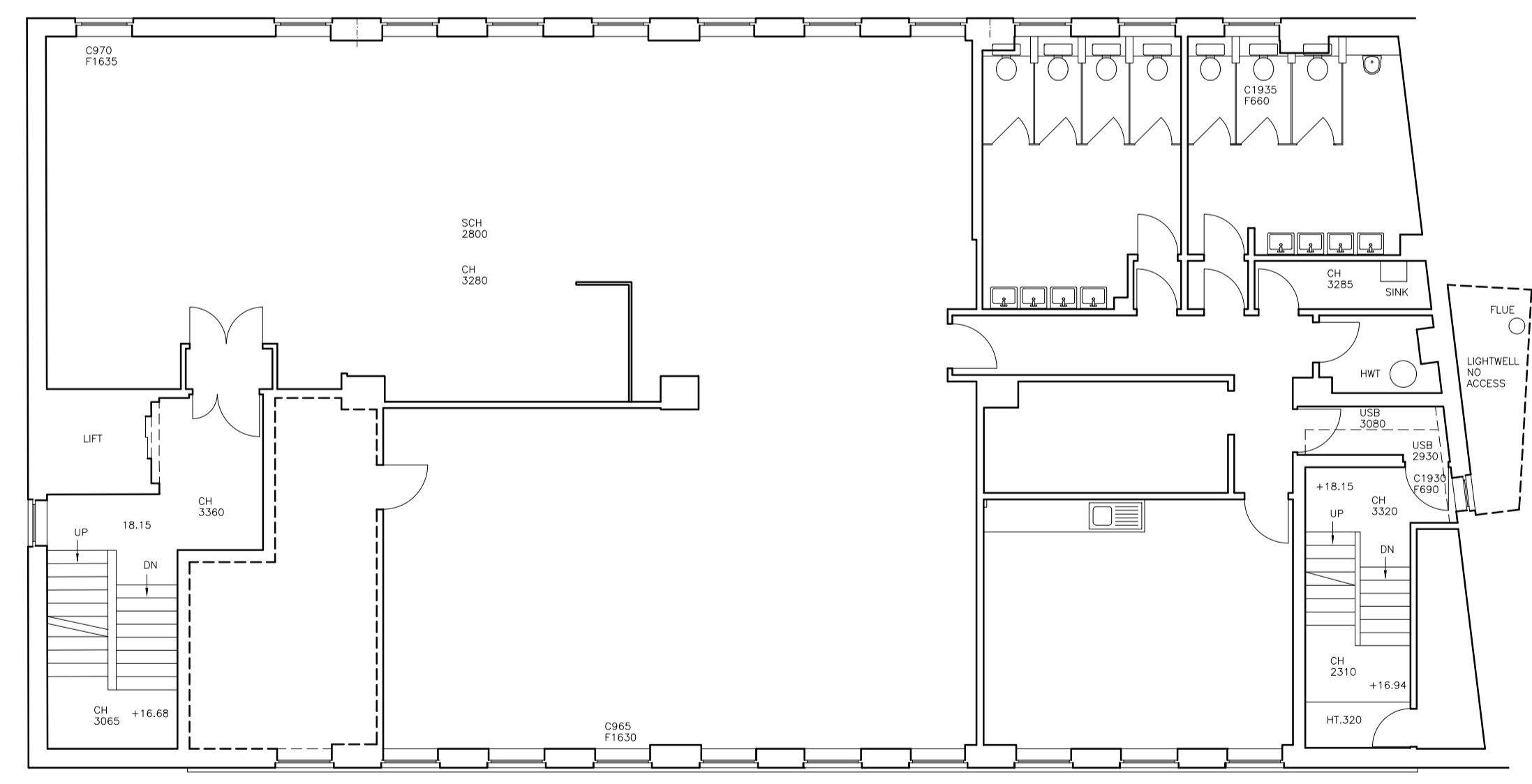
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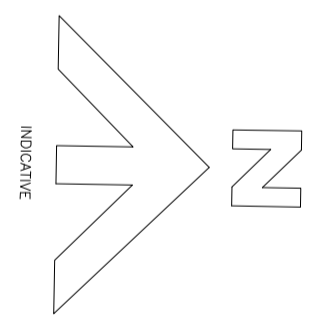
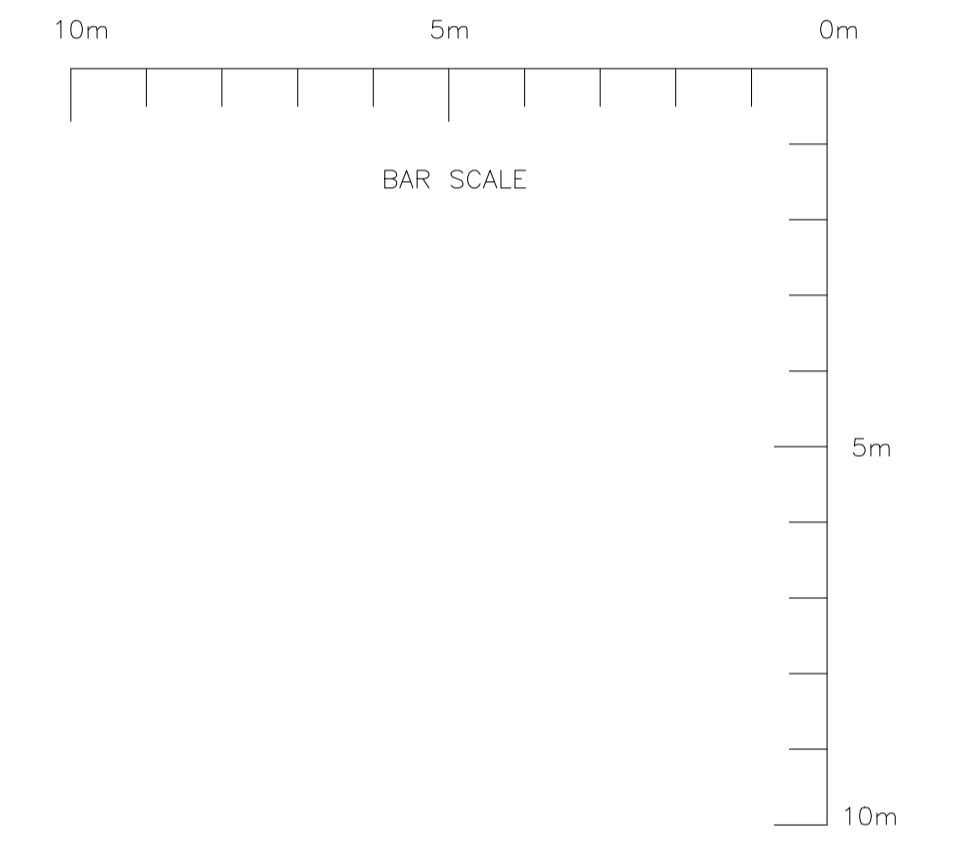
<p>LEVELS RELATE TO :- OSBM SITUATED ON NORTHSIDE EL.SUB.STA. MILNER ROAD, VALUE 14.87m</p>		<p>W.P.G.SURVEYS LAND SURVEYORS P.O. BOX 1004A SURREY, SURREY, KT1 9AL TEL: 020 8397 8361</p>	
<p>DN DETAIL SURVEY</p> <p>AB AIR BRICK AC AIR COMPRESSOR ADU AIR CONDITIONING UNIT AP ASPHALT APD ASPHALT, STONE PAVING BB BELLOWS REACH BFF BENCH B1 BENCH B2 BENCH B3 BENCH B4 BENCH B5 BENCH B6 BENCH B7 BENCH B8 BENCH B9 BENCH B10 BENCH B11 BENCH B12 BENCH B13 BENCH B14 BENCH B15 BENCH B16 BENCH B17 BENCH B18 BENCH B19 BENCH B20 BENCH B21 BENCH B22 BENCH B23 BENCH B24 BENCH B25 BENCH B26 BENCH B27 BENCH B28 BENCH B29 BENCH B30 BENCH B31 BENCH B32 BENCH B33 BENCH B34 BENCH B35 BENCH B36 BENCH B37 BENCH B38 BENCH B39 BENCH B40 BENCH B41 BENCH B42 BENCH B43 BENCH B44 BENCH B45 BENCH B46 BENCH B47 BENCH B48 BENCH B49 BENCH B50 BENCH B51 BENCH B52 BENCH B53 BENCH B54 BENCH B55 BENCH B56 BENCH B57 BENCH B58 BENCH B59 BENCH B60 BENCH B61 BENCH B62 BENCH B63 BENCH B64 BENCH B65 BENCH B66 BENCH B67 BENCH B68 BENCH B69 BENCH B70 BENCH B71 BENCH B72 BENCH B73 BENCH B74 BENCH B75 BENCH B76 BENCH B77 BENCH B78 BENCH B79 BENCH B80 BENCH B81 BENCH B82 BENCH B83 BENCH B84 BENCH B85 BENCH B86 BENCH B87 BENCH B88 BENCH B89 BENCH B90 BENCH B91 BENCH B92 BENCH B93 BENCH B94 BENCH B95 BENCH B96 BENCH B97 BENCH B98 BENCH B99 BENCH B100 BENCH</p>		<p>JB JUNCTION BOX LC LAMP COLUMN LH LAMP LJ LAMP LW LAMP LX LAMP LZ LAMP M1 METER M2 METER M3 METER M4 METER M5 METER M6 METER M7 METER M8 METER M9 METER M10 METER M11 METER M12 METER M13 METER M14 METER M15 METER M16 METER M17 METER M18 METER M19 METER M20 METER M21 METER M22 METER M23 METER M24 METER M25 METER M26 METER M27 METER M28 METER M29 METER M30 METER M31 METER M32 METER M33 METER M34 METER M35 METER M36 METER M37 METER M38 METER M39 METER M40 METER M41 METER M42 METER M43 METER M44 METER M45 METER M46 METER M47 METER M48 METER M49 METER M50 METER M51 METER M52 METER M53 METER M54 METER M55 METER M56 METER M57 METER M58 METER M59 METER M60 METER M61 METER M62 METER M63 METER M64 METER M65 METER M66 METER M67 METER M68 METER M69 METER M70 METER M71 METER M72 METER M73 METER M74 METER M75 METER M76 METER M77 METER M78 METER M79 METER M80 METER M81 METER M82 METER M83 METER M84 METER M85 METER M86 METER M87 METER M88 METER M89 METER M90 METER M91 METER M92 METER M93 METER M94 METER M95 METER M96 METER M97 METER M98 METER M99 METER M100 METER</p>	
<p>DN BUILDINGS</p> <p>AB AIR BRICK AC AIR COMPRESSOR ADU AIR CONDITIONING UNIT AP ASPHALT APD ASPHALT, STONE PAVING BB BELLOWS REACH BFF BENCH B1 BENCH B2 BENCH B3 BENCH B4 BENCH B5 BENCH B6 BENCH B7 BENCH B8 BENCH B9 BENCH B10 BENCH B11 BENCH B12 BENCH B13 BENCH B14 BENCH B15 BENCH B16 BENCH B17 BENCH B18 BENCH B19 BENCH B20 BENCH B21 BENCH B22 BENCH B23 BENCH B24 BENCH B25 BENCH B26 BENCH B27 BENCH B28 BENCH B29 BENCH B30 BENCH B31 BENCH B32 BENCH B33 BENCH B34 BENCH B35 BENCH B36 BENCH B37 BENCH B38 BENCH B39 BENCH B40 BENCH B41 BENCH B42 BENCH B43 BENCH B44 BENCH B45 BENCH B46 BENCH B47 BENCH B48 BENCH B49 BENCH B50 BENCH B51 BENCH B52 BENCH B53 BENCH B54 BENCH B55 BENCH B56 BENCH B57 BENCH B58 BENCH B59 BENCH B60 BENCH B61 BENCH B62 BENCH B63 BENCH B64 BENCH B65 BENCH B66 BENCH B67 BENCH B68 BENCH B69 BENCH B70 BENCH B71 BENCH B72 BENCH B73 BENCH B74 BENCH B75 BENCH B76 BENCH B77 BENCH B78 BENCH B79 BENCH B80 BENCH B81 BENCH B82 BENCH B83 BENCH B84 BENCH B85 BENCH B86 BENCH B87 BENCH B88 BENCH B89 BENCH B90 BENCH B91 BENCH B92 BENCH B93 BENCH B94 BENCH B95 BENCH B96 BENCH B97 BENCH B98 BENCH B99 BENCH B100 BENCH</p>		<p>FOR</p> <p>PEER SECURITIES LTD</p>	



SECOND FLOOR



FIRST FLOOR



title
16-20 MORDEN ROAD, SOUTH WIMBLEDON
LONDON SW19 3BN

client
PEER SECURITIES LTD

description
FIRST AND SECOND FLOOR PLANS
AS EXISTING

bm architects
burgess mean architects
unit 1, the warehouse
12 ravenbury terrace
london
SW18 4RL

all dimensions to be checked on site. any discrepancy between this drawing and other information is to be referred to the partner(s). this drawing is copyright.

f: 020 8944 8050 f: 020 8946 9506 e: bma@burgessmean.co.uk

date: JANUARY 2019 scale: 1:100 (A1) project: 181202 drawing: S/02 revision:

drawn by: SA checked:

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<p>LEVELS RELATE TO :- OSBM SITUATED ON NORTHSIDE EL.SUB.STA. MILNER ROAD, VALUE 14.87m</p>		<p>W.P.G.SURVEYS LAND SURVEYORS P.O. BOX 1004A SURREY, SURREY, KT1 9AL TEL: 020 8397 8361</p>	
<p>ON DETAIL SURVEY</p>		<p>SITE 16-20 MORDEN ROAD SOUTH WIMBLEDON SW19 3BN</p>	
<p>ON BUILDINGS</p>		<p>DRAWING SITE SURVEY & GRD FLOOR LAYOUT PLAN</p>	
<p>SCALE 1:100 DATE AUG 2018 JOB NO 5409</p>		<p>FOR PEER SECURITIES LTD</p>	



DATUM 10.00m EAST ELEVATION

title
16-20 MORDEN ROAD, SOUTH WIMBLEDON
LONDON SW19 3BN

client
PEER SECURITIES LTD

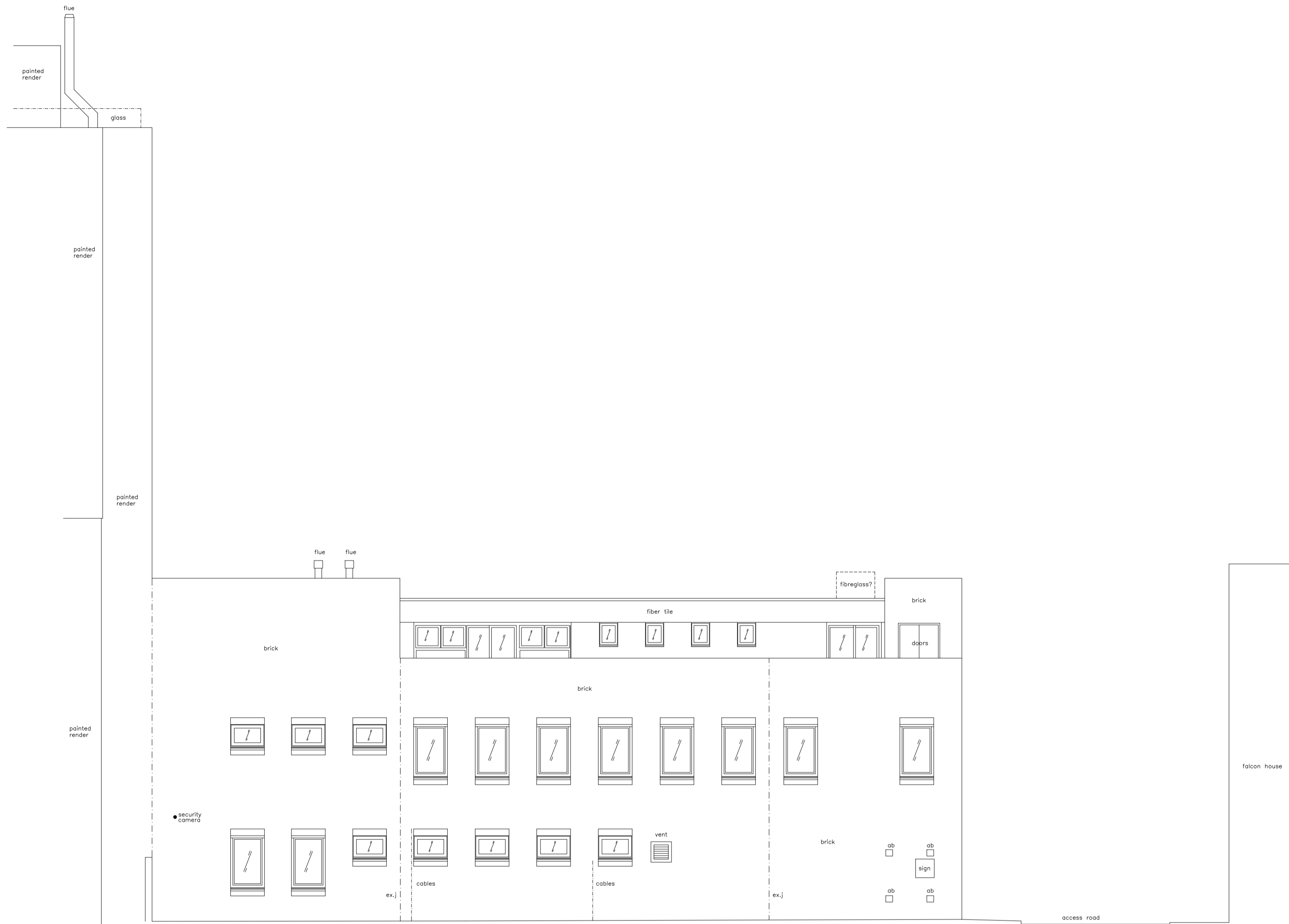
description
EAST ELEVATION
AS EXISTING

bm architects
burgess mean architects
unit 1, the warehouse
12 ravenbury terrace
london
SW18 4RL

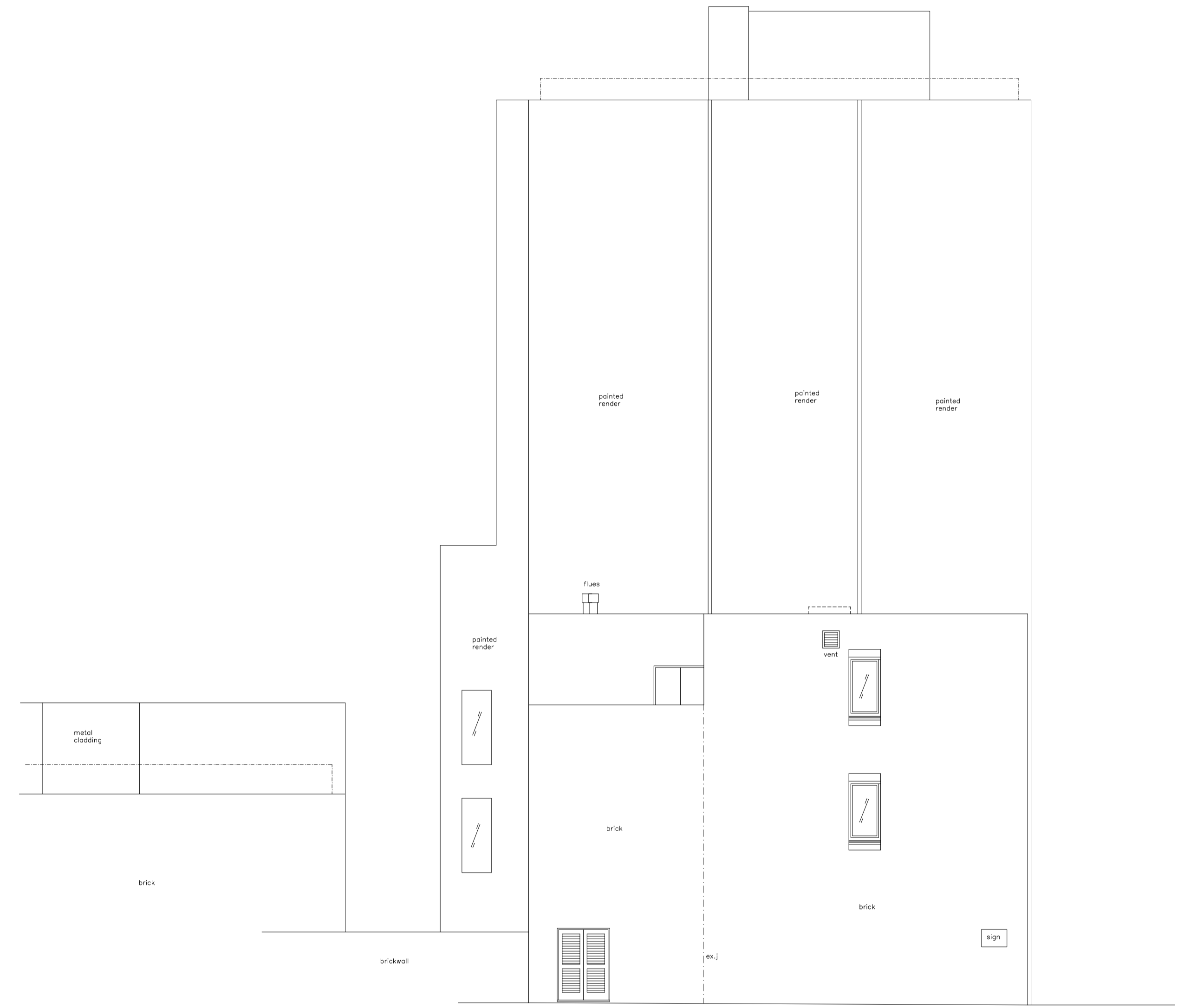
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f: 020 8944 8050 f: 020 8946 9506 e: bma@burgessmean.co.uk

date: JANUARY 2019 scale: 1:100 project: 181202 drawing: S/03 revision:
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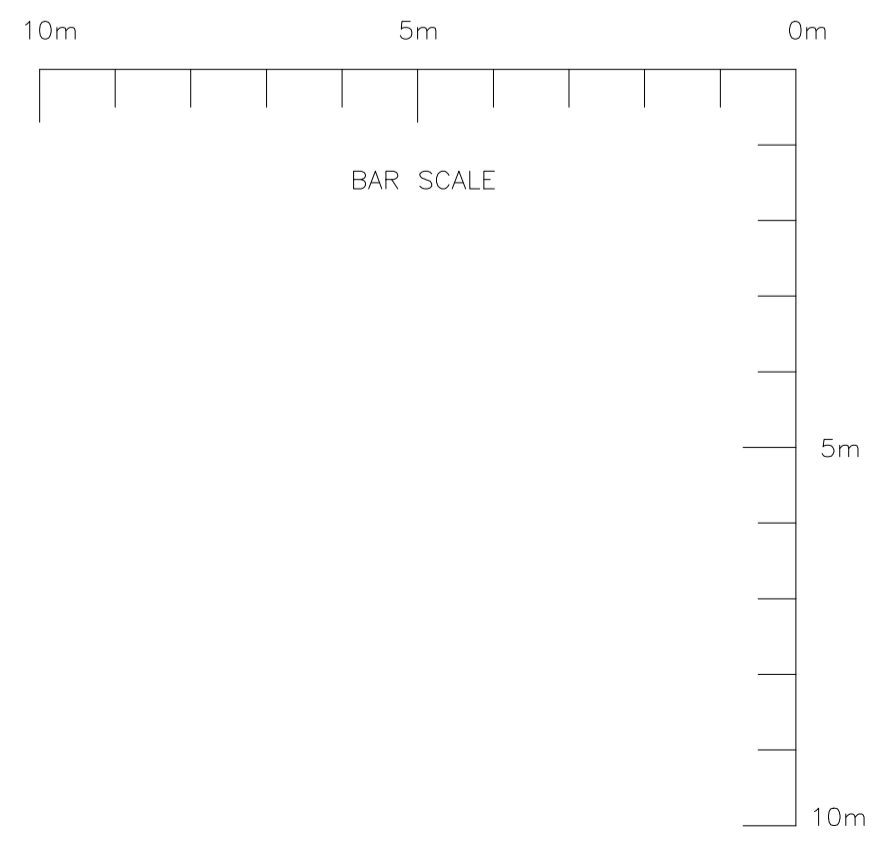


WEST ELEVATION



SOUTH ELEVATION

<p>LEVELS RELATE TO :- OSBM SITUATED ON NORTHSIDE ELSUB.STA. MILNER ROAD. VALUE 14.87m</p>		<p>W.P.G.SURVEYS LAND SURVEYORS P.O. BOX 1004A SURREY, SURREY, KT1 9AL TEL: 020 8397 8361</p>	
<p>ON DETAIL SURVEY AB ASB BRICK ACU AIR CONDITIONING UNIT ADP ARTIFICIAL STONE PAVING AE AIR LINE AF BRICK AG BRICK AH BRICK AI BRICK AJ BRICK AK BRICK AL BRICK AM BRICK AN BRICK AO BRICK AP BRICK AQ BRICK AR BRICK AS BRICK AT BRICK AU BRICK AV BRICK AW BRICK AX BRICK AY BRICK AZ BRICK BA BRICK BB BRICK BC BRICK BD BRICK BE BRICK BF BRICK BG BRICK BH BRICK BI BRICK BJ BRICK BK BRICK BL BRICK BM BRICK BN BRICK BO BRICK BP BRICK BQ BRICK BR BRICK BS BRICK BT BRICK BU BRICK BV BRICK BW BRICK BX BRICK BY BRICK BZ BRICK CA BRICK CB BRICK CC BRICK CD BRICK CE BRICK CF BRICK CG BRICK CH BRICK CI BRICK CJ BRICK CK BRICK CL BRICK CM BRICK CN BRICK CO BRICK CP BRICK CQ BRICK CR BRICK CS BRICK CT BRICK CU BRICK CV BRICK CW BRICK CX BRICK CY BRICK CZ BRICK DA BRICK DB BRICK DC BRICK DD BRICK DE BRICK DF BRICK DG BRICK DH BRICK DI BRICK DJ BRICK DK BRICK DL BRICK DM BRICK DN BRICK DO BRICK DP BRICK DQ BRICK DR BRICK DS BRICK DT BRICK DU BRICK DV BRICK DW BRICK DX BRICK DY BRICK DZ BRICK EA BRICK 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ZL BRICK ZM BRICK ZN BRICK ZO BRICK ZP BRICK ZQ BRICK ZR BRICK ZS BRICK ZT BRICK ZU BRICK ZV BRICK ZW BRICK ZX BRICK ZY BRICK ZZ BRICK</p>		<p>SITE 16-20 MORDEN ROAD SOUTH WIMBLEDON SW19 3BN</p> <p>DRAWING SITE SURVEY & GRD FLOOR LAYOUT PLAN</p> <p>SCALE 1:100 DATE AUG 2018 JOB NO 5400</p> <p>SURVEYED BY SP.CP DRAWING NO 01</p> <p>DRAWN BY SP</p> <p>FOR PEER SECURITIES LTD</p>	



title
16-20 MORDEN ROAD, SOUTH WIMBLEDON
LONDON SW19 3BN

client
PEER SECURITIES LTD

description
WEST AND SOUTH ELEVATIONS
AS EXISTING

bm architects
burgess mean architects
unit 1, the warehouse
12 ravenbury terrace
london
SW18 4RL

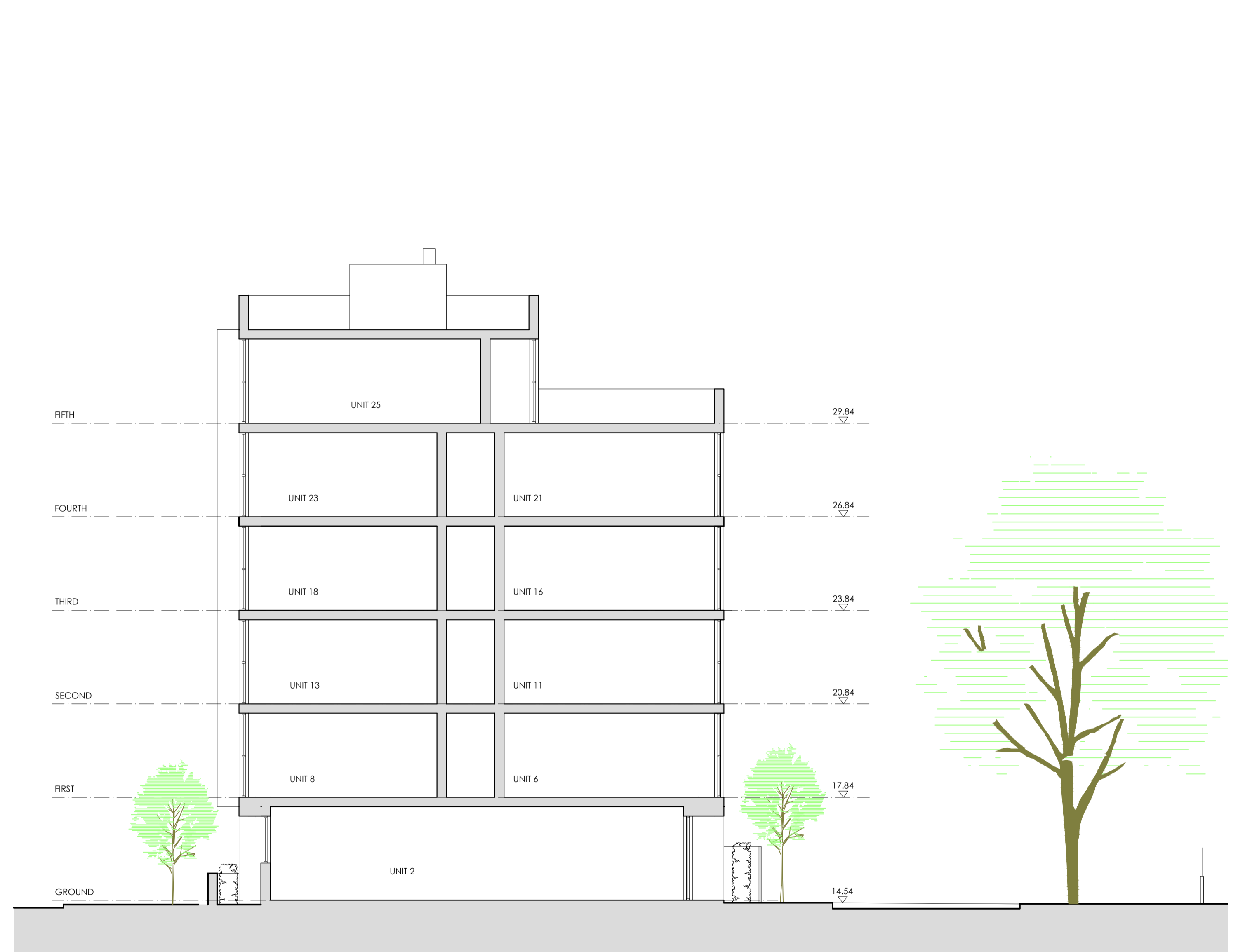
f: 020 8944 8050 f: 020 8946 9506 e: bma@burgessmean.co.uk

date: JANUARY 2019 scale: 1:100 project: drawing: revision:
drawn by: SA (A1) 181202 S/04
checked:

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FRONT (EAST) ELEVATION



SECTION AA

PLANNING DRAWING (26 FLATS)

Rev G: 10/06/2020: Building repositioned to suit planning officers' comments.
 Rev F: 19/09/2019: General update to suit planning consultants comments.
 Rev E: 13/09/2019: Updated to suit planning application.
 Rev D: 17/07/2019: Amended to suit planning pre-application report comments.

title
 16-20 MORDEN ROAD, SOUTH WIMBLEDON
 LONDON SW19 3BN

client
 PEER SECURITIES LTD

description
 FRONT (EAST) ELEVATION + SECTION AA
 AS PROPOSED

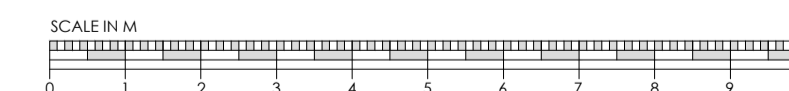


burgess mean architects
 unit 1, the warehouse
 12 ravenbury terrace
 london
 SW18 4RL

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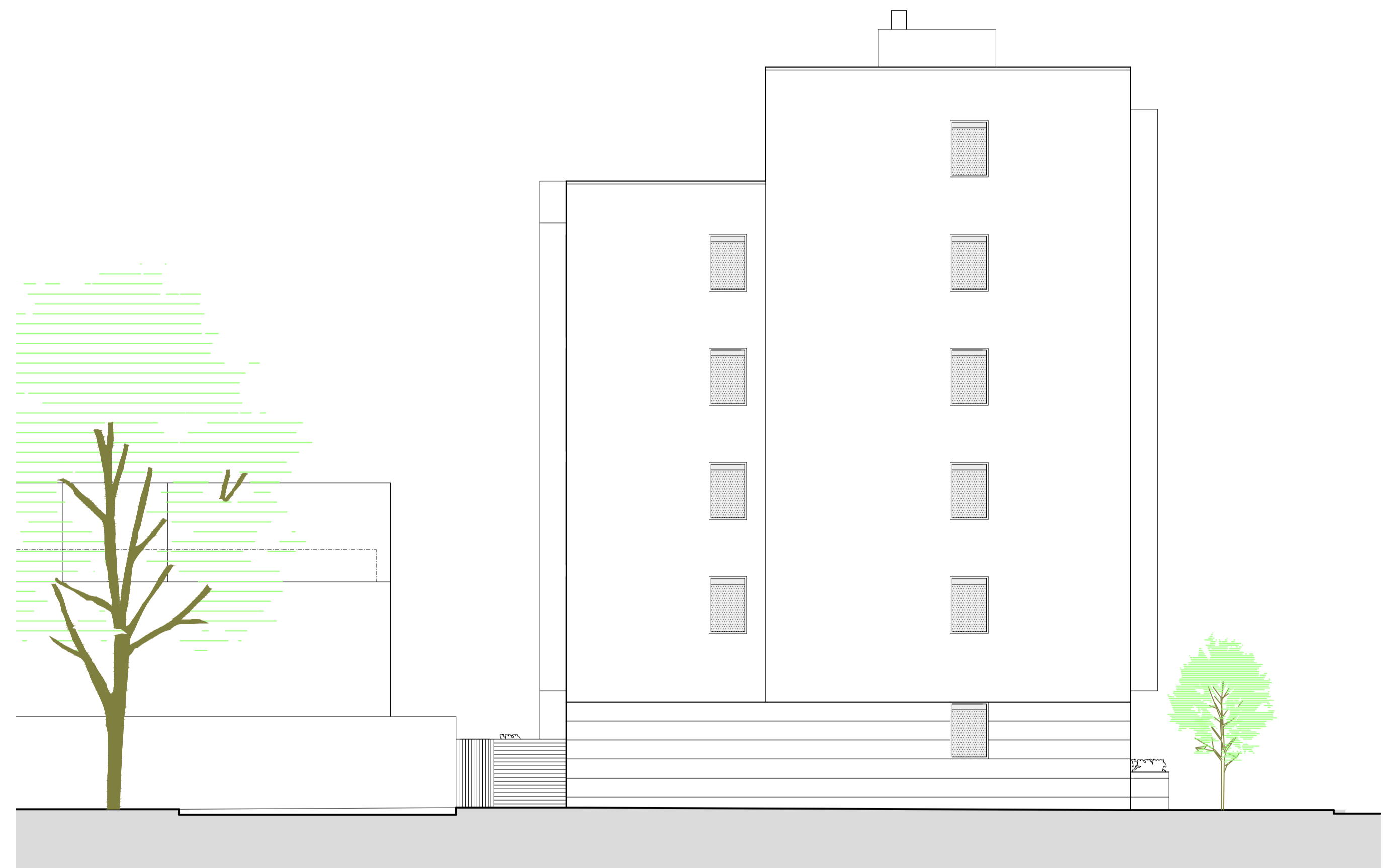
f: 020 8944 8050 f: 020 8946 9506 e: bma@burgessmean.co.uk

date: JANUARY 2019	scale: 1:100 (A1)	project: 181202	drawing: P/07	revision: G
drawn by: SA				
file name:				
checked:				





REAR (WEST) ELEVATION



SIDE (SOUTH) ELEVATION

PLANNING DRAWING (26 FLATS)

Rev G: 10/06/2020: Building repositioned to suit planning officers' comments.
 Rev F: 19/09/2019: General update to suit planning consultants comments.
 Rev E: 13/09/2019: Updated to suit planning application.
 Rev D: 17/07/2019: Amended to suit planning pre-application report comments.

title
 16-20 MORDEN ROAD, SOUTH WIMBLEDON
 LONDON SW19 3BN

client
 PEER SECURITIES LTD

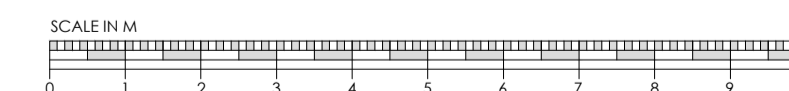
description
 REAR (WEST) + SIDE (SOUTH) ELEVATIONS
 AS PROPOSED



burgess mean architects
 unit 1, the warehouse
 12 ravenbury terrace
 london
 SW18 4RL

all dimensions to be checked on site. any discrepancy between this drawing and other information is to be referred to the partner(s). this drawing is copyright.

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date: JANUARY 2019	scale: 1:100 (A1)	project: 181202	drawing: P/08	revision: G
drawn by: SA				
file name:				
checked:				



FRONT (EAST) ELEVATION

PLANNING DRAWING (26 FLATS)

Rev C: 19/09/2019: General update to suit planning consultants comments.
Rev B: 17/07/2019: Amended to suit planning pre-application report comments.
Rev A: 29/03/2019: Amended to suit planners comments.

title
16-20 MORDEN ROAD, SOUTH WIMBLEDON
LONDON SW19 3BN

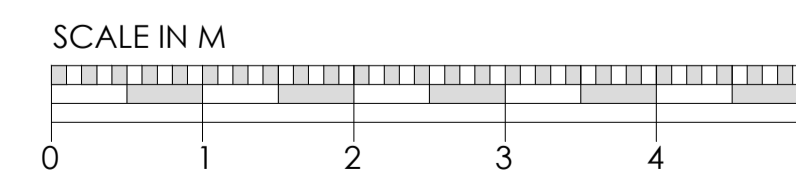
client
PEER SECURITIES LTD

description
FRONT (EAST) ELEVATION - STREET SCENE
AS PROPOSED

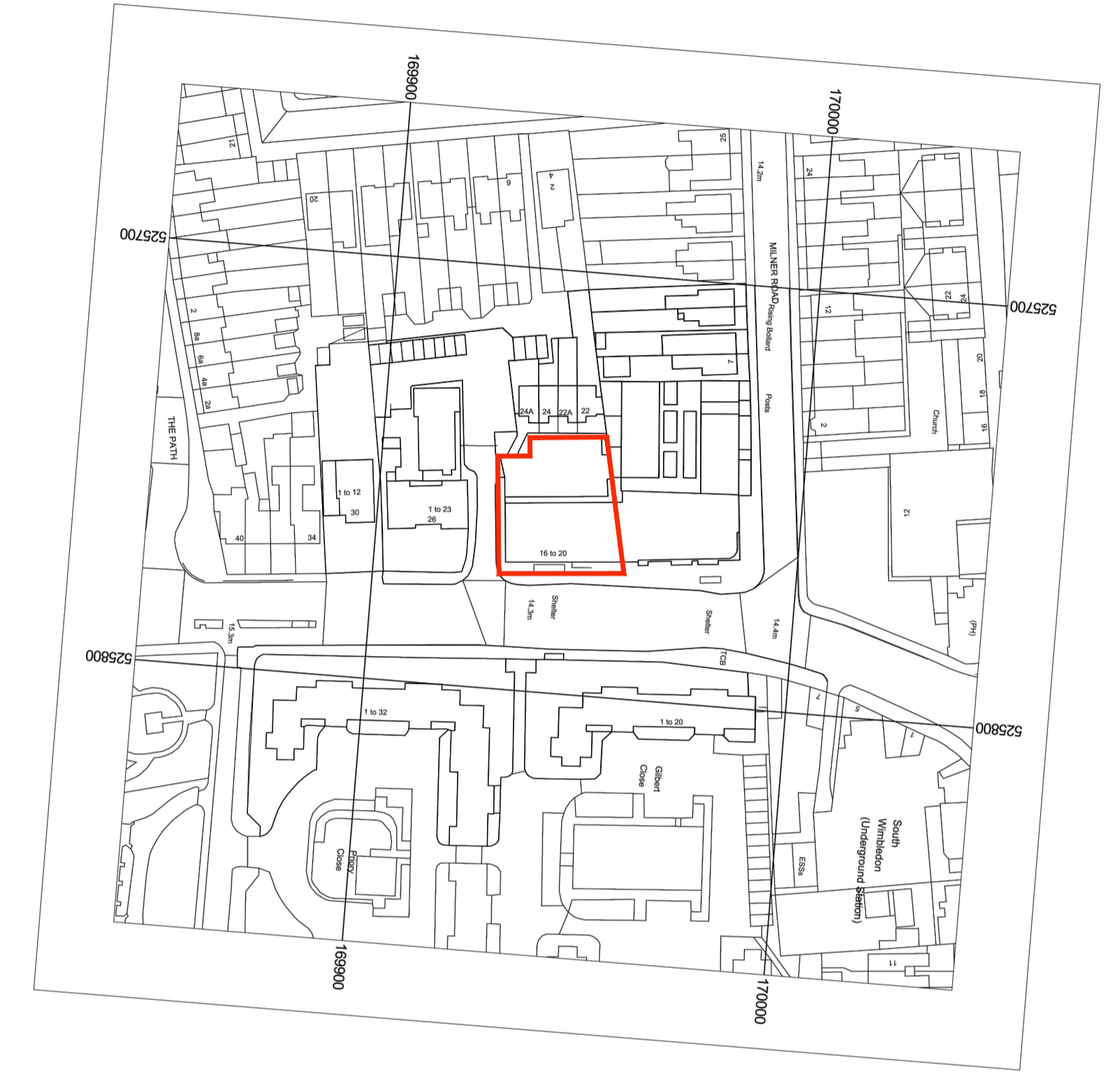
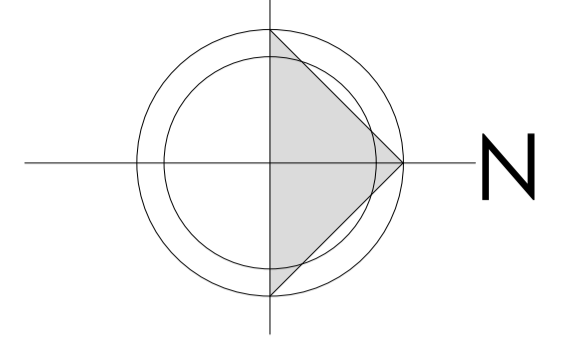


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SW18 4RL

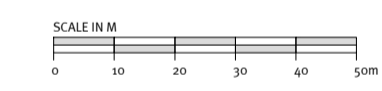
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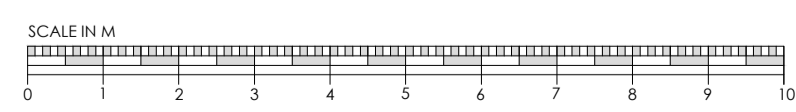
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drawn by: SA				
file name:				
checked:				



LOCATION PLAN



SITE PLAN



PLANNING DRAWING

Rev F: 10/06/2020: Building repositioned to suit planning officers' comments.
 Rev E: 19/09/2019: General update to suit planning consultants comments.
 Rev D: 13/09/2019: Updated to suit planning application.
 Rev C: 17/07/2019: Amended to suit planning pre-application report comments.

title
 16-20 MORDEN ROAD, SOUTH WIMBLEDON
 LONDON SW19 3BN

client
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description
 SITE PLAN
 AS PROPOSED



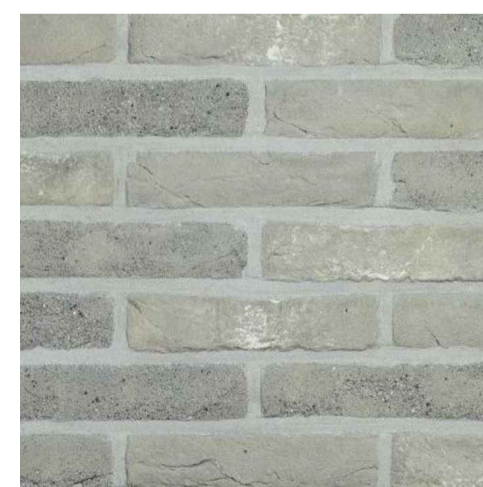
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date: JANUARY 2019	scale: 1:100 (A1)	project: 181202 P/10	drawing: F	revision:
drawn by: SA				
checked:				



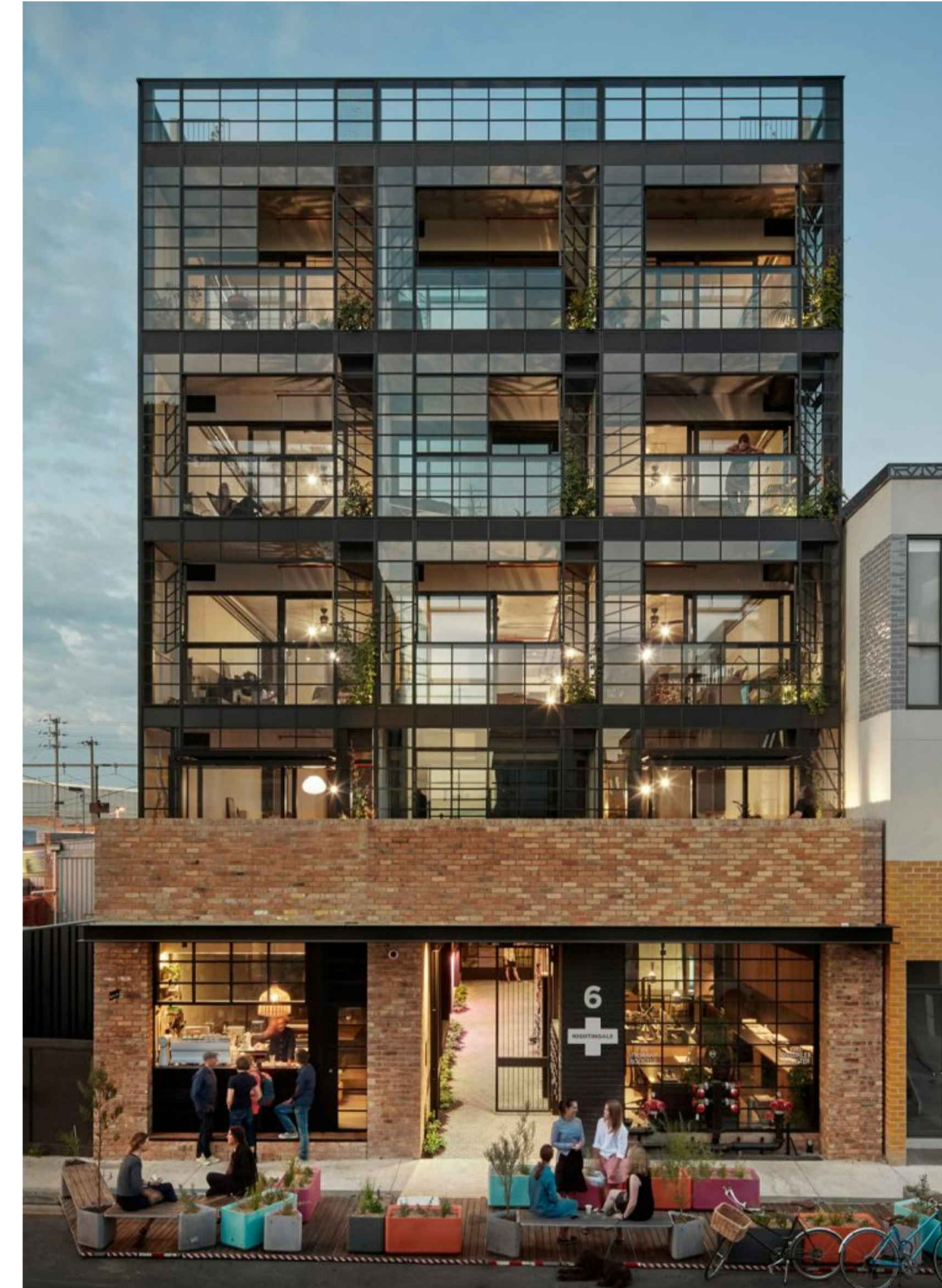
FRONT (EAST) ELEVATION



Wienerberger - Forum Smoked Branco



Wienerberger - Flashed Black



PLANNING DRAWING (26 FLATS)

Rev C: 19/09/2019: General update to suit planning consultants comments.
Rev B: 17/07/2019: Amended to suit planning pre-application report comments.
Rev A: 29/03/2019: Amended to suit planners comments.

title
16-20 MORDEN ROAD, SOUTH WIMBLEDON
LONDON SW19 3BN

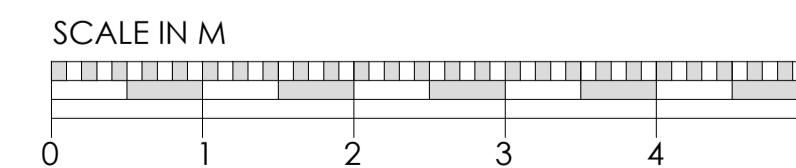
client
PEER SECURITIES LTD

description
FRONT (EAST) ELEVATION + SECTION (MATERIALS)
AS PROPOSED



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date: JANUARY 2019
drawn by: SA
file name:
checked:
scale: 1:100 (A1)
project: 181202
drawing: P/11
revision: C



REAR (WEST) ELEVATION



SIDE (SOUTH) ELEVATION

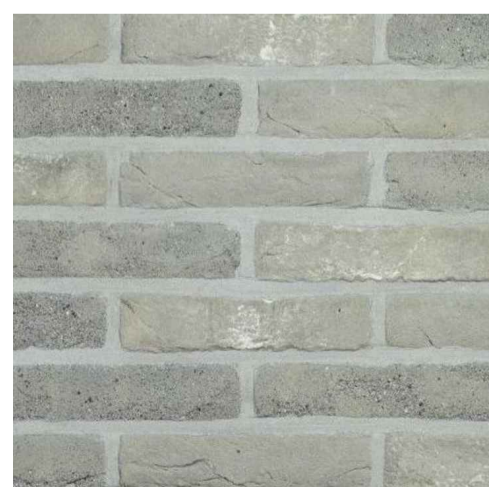
PLANNING DRAWING (26 FLATS)

Rev D: 19/09/2019: General update to suit planning consultants comments.
Rev C: 17/07/2019: Amended to suit planning pre-application report comments.
Rev B: 18/04/2019: Side elevation feature panel amended.
Rev A: 27/03/2019: Amended to suit planners comments.

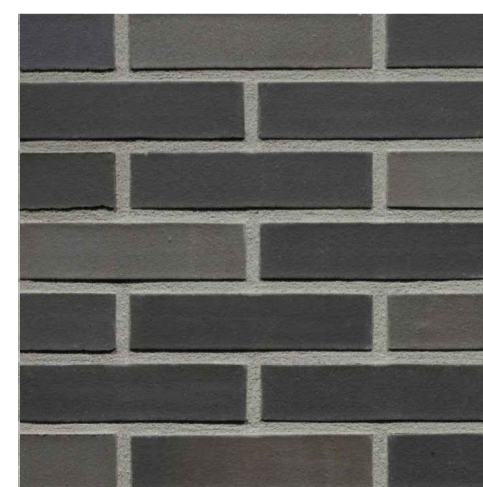
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16-20 MORDEN ROAD, SOUTH WIMBLEDON
LONDON SW19 3BN

client
PEER SECURITIES LTD

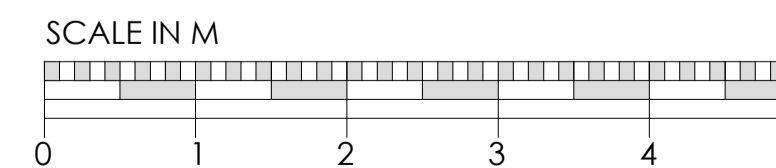
description
REAR (WEST) + SIDE (SOUTH) ELEVATIONS (MATERIALS
AS PROPOSED



Wienerberger -
Forum Smoked
Branco



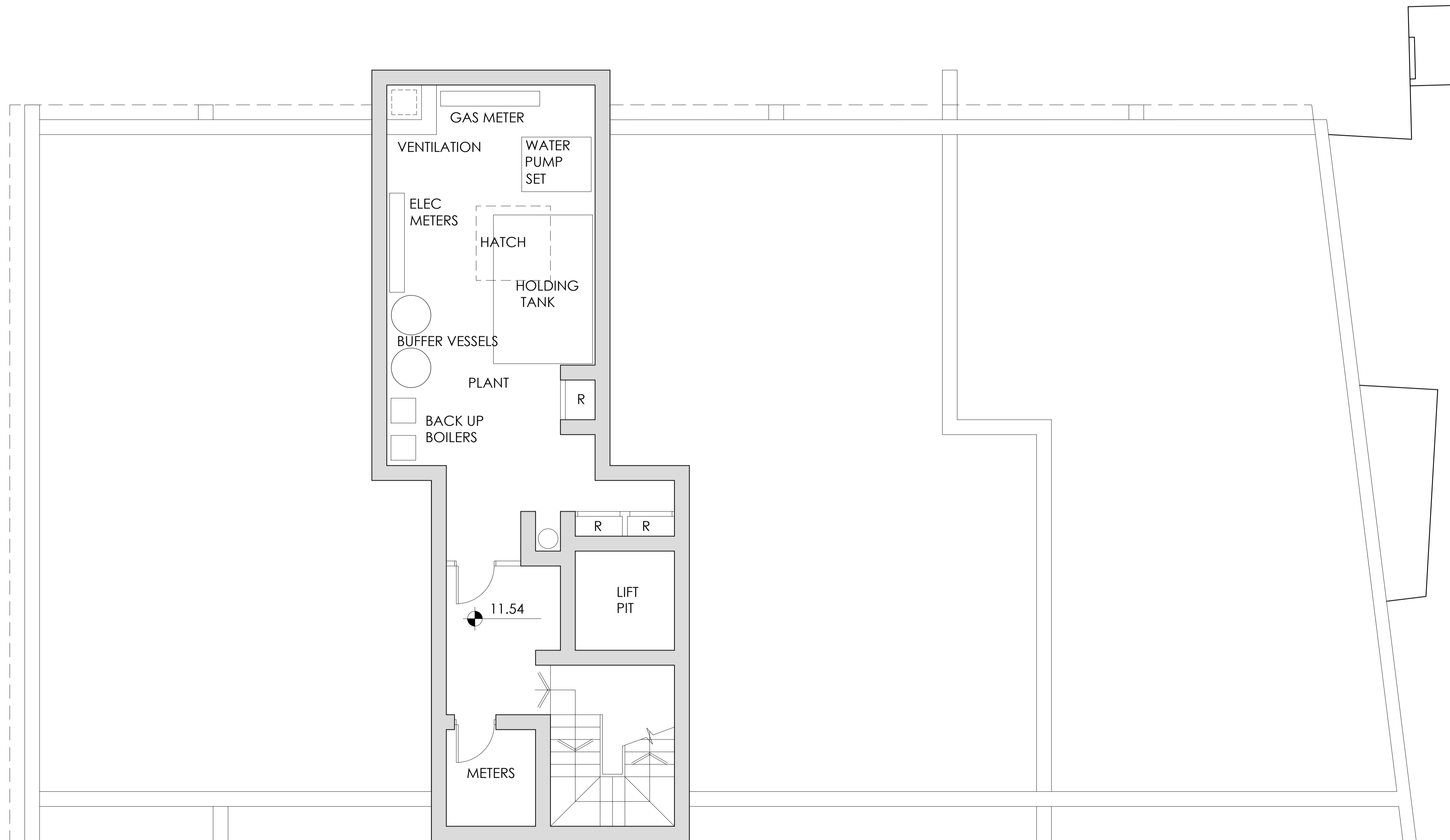
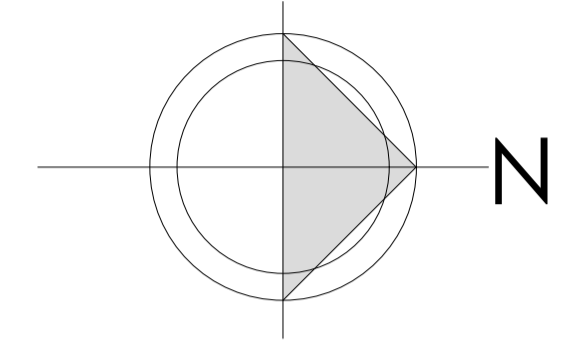
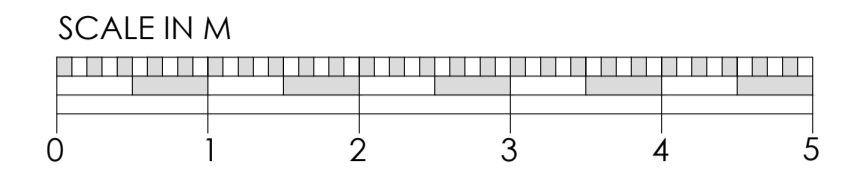
Wienerberger -
Flashed Black



burgess mean architects
unit 1, the warehouse
12 ravenbury terrace
london
SW18 4RL

f: 020 8944 8050 f: 020 8946 9506 e: bma@burgessmean.co.uk

date: JANUARY 2019 scale: 1:100 project drawing revision
drawn by: SA (A1) 181202 P/12 D
checked:



BASEMENT FLOOR PLAN

PLANNING DRAWING (26 FLATS)

Rev G: 10/06/2020: Building repositioned to suit planning officers' comments.
 Rev F: 19/09/2019: General update to suit planning consultants comments.
 Rev E: 13/09/2019: Updated to suit planning application.
 Rev D: 28/08/2019: Completely redrawn.

title
 16-20 MORDEN ROAD, SOUTH WIMBLEDON
 LONDON SW19 3BN

client
 PEER SECURITIES LTD

description
 BASEMENT FLOOR PLAN
 AS PROPOSED

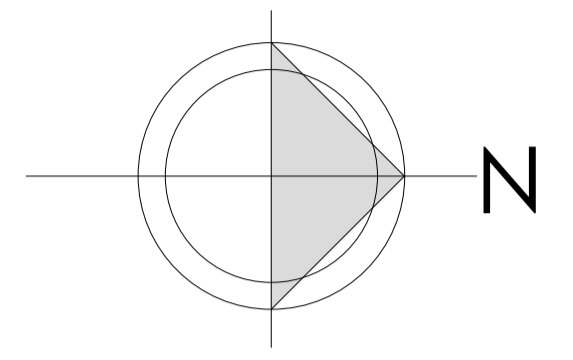
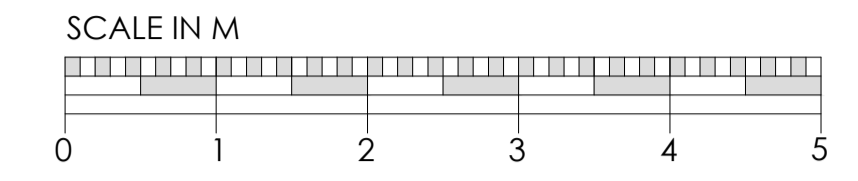


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 london
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date: JANUARY 2019	scale: 1:50 (A1)	project: 181202	drawing: P/01	revision: G
drawn by: SA				
file name:				
checked:				



PLANNING DRAWING (26 FLATS)

Rev G: 10/06/2020: Building repositioned to suit planning officers' comments.
 Rev F: 19/09/2019: General update to suit planning consultants comments.
 Rev E: 13/09/2019: Updated to suit planning application.
 Rev D: 28/08/2019: Completely redrawn.

title
 16-20 MORDEN ROAD, SOUTH WIMBLEDON
 LONDON SW19 3BN

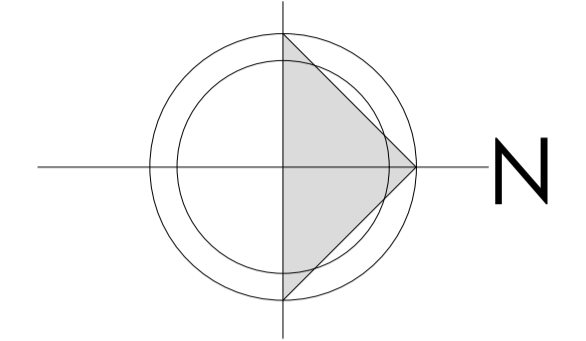
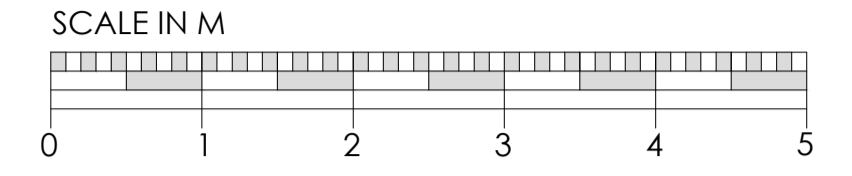
client
 PEER SECURITIES LTD

description
 GROUND FLOOR PLAN
 AS PROPOSED (OPTION B)

bm architects
 burgess mean architects
 unit 1, the warehouse
 12 ravenbury terrace
 london
 SW18 4RL

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date:	JANUARY 2019	scale:	1:50 (A1)	project:	181202 P/02	revision:	G
drawn by:	SA	checked:					
file name:							



TYPICAL (FIRST/FOURTH) FLOOR PLAN

PLANNING DRAWING (26 FLATS)

Rev G: 10/06/2020: Building repositioned to suit planning officers' comments.
Rev F: 19/09/2019: General update to suit planning consultants comments.
Rev E: 13/09/2019: Updated to suit planning application.
Rev D: 28/08/2019: Completely redrawn and drawing renumbered.

16-20 MORDEN ROAD, SOUTH WIMBLEDON
LONDON SW19 3BN

client
PEER SECURITIES LTD

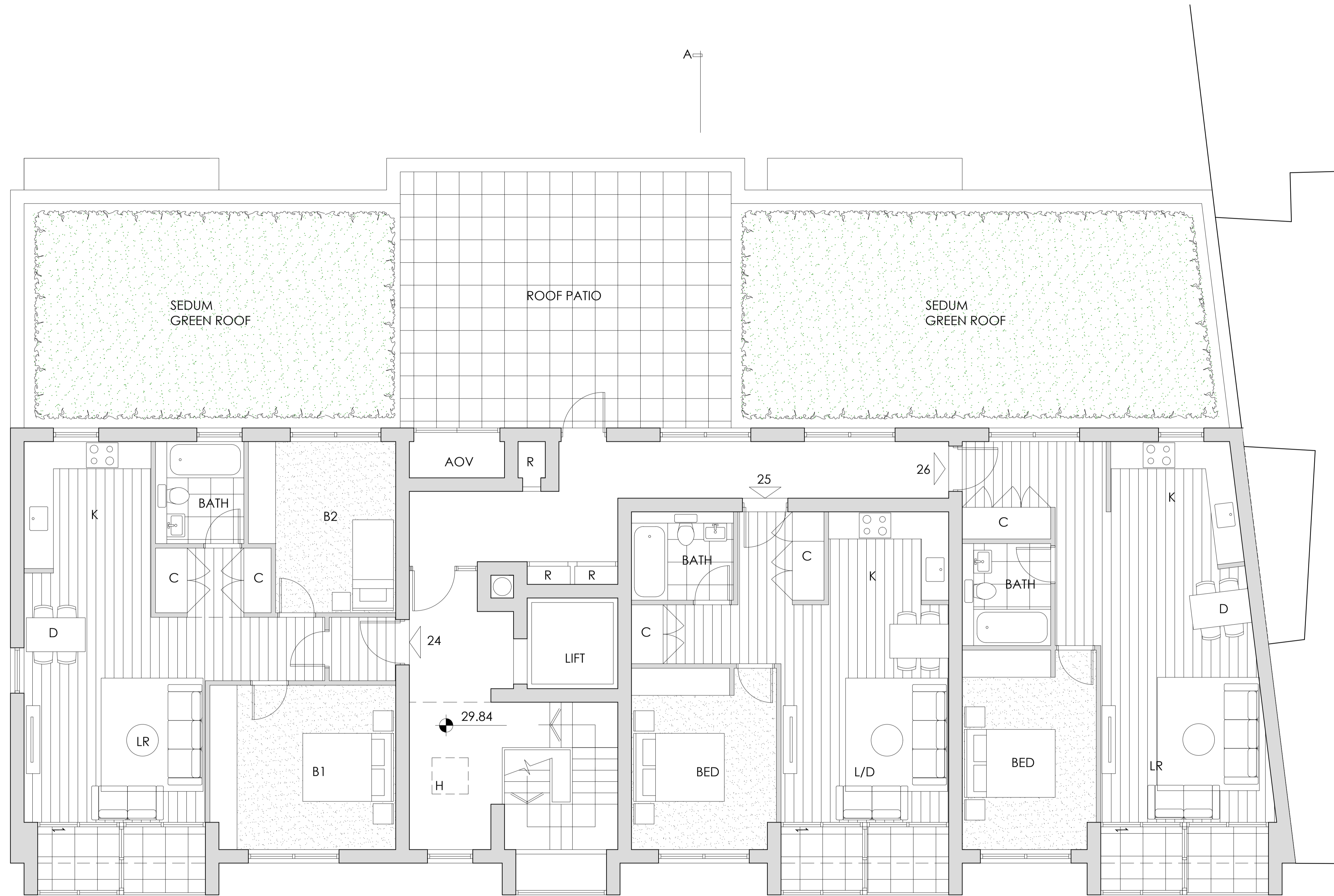
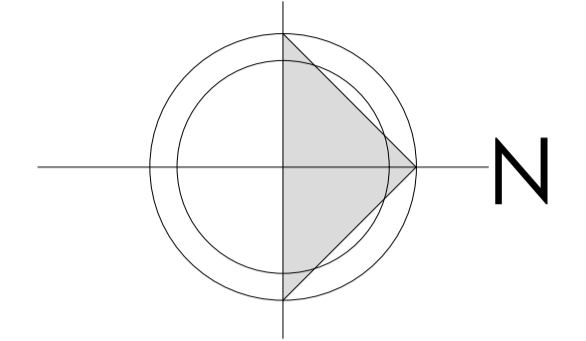
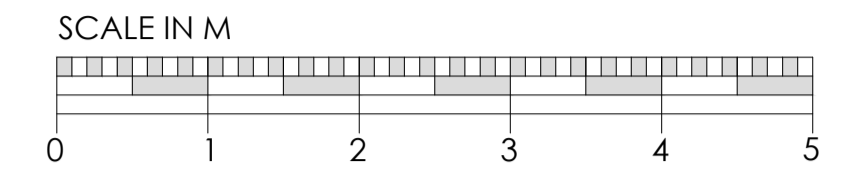
description
TYPICAL FLOOR PLAN (FIRST - FOURTH FLOORS)
AS PROPOSED



burgess mean architects
unit 1, the warehouse
12 ravenbury terrace
london
SW18 4RL

t: 020 8944 8050 f: 020 8946 9506 e: bma@burgessmean.co.uk

date: JANUARY 2019 scale: 1:50 (A1) project: 181202 drawing: P/03 revision: G
drawn by: SA
file name:
checked:



Page 190

FIFTH FLOOR PLAN

PLANNING DRAWING
(26 FLATS)

Rev G: 10/06/2020: Building repositioned to suit planning officers' comments.
 Rev F: 19/09/2019: General update to suit planning consultants comments.
 Rev E: 13/09/2019: Updated to suit planning application.
 Rev D: 28/08/2019: Completely redrawn and drawing renumbered.

16-20 MORDEN ROAD, SOUTH WIMBLEDON
 LONDON SW19 3BN

client
 PEER SECURITIES LTD

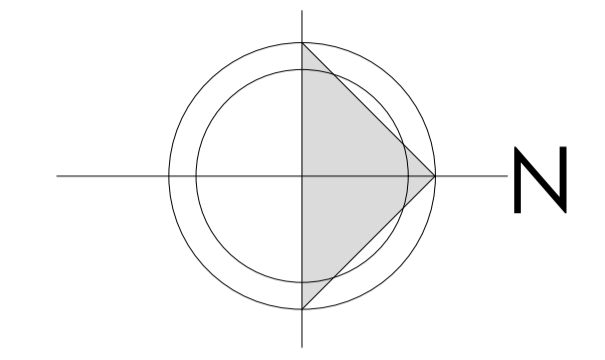
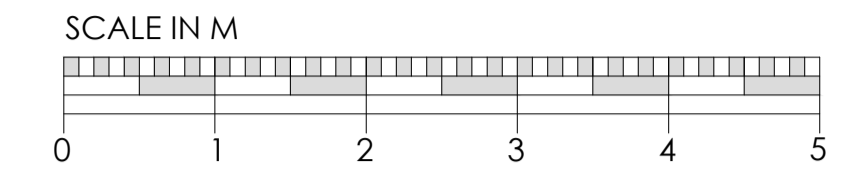
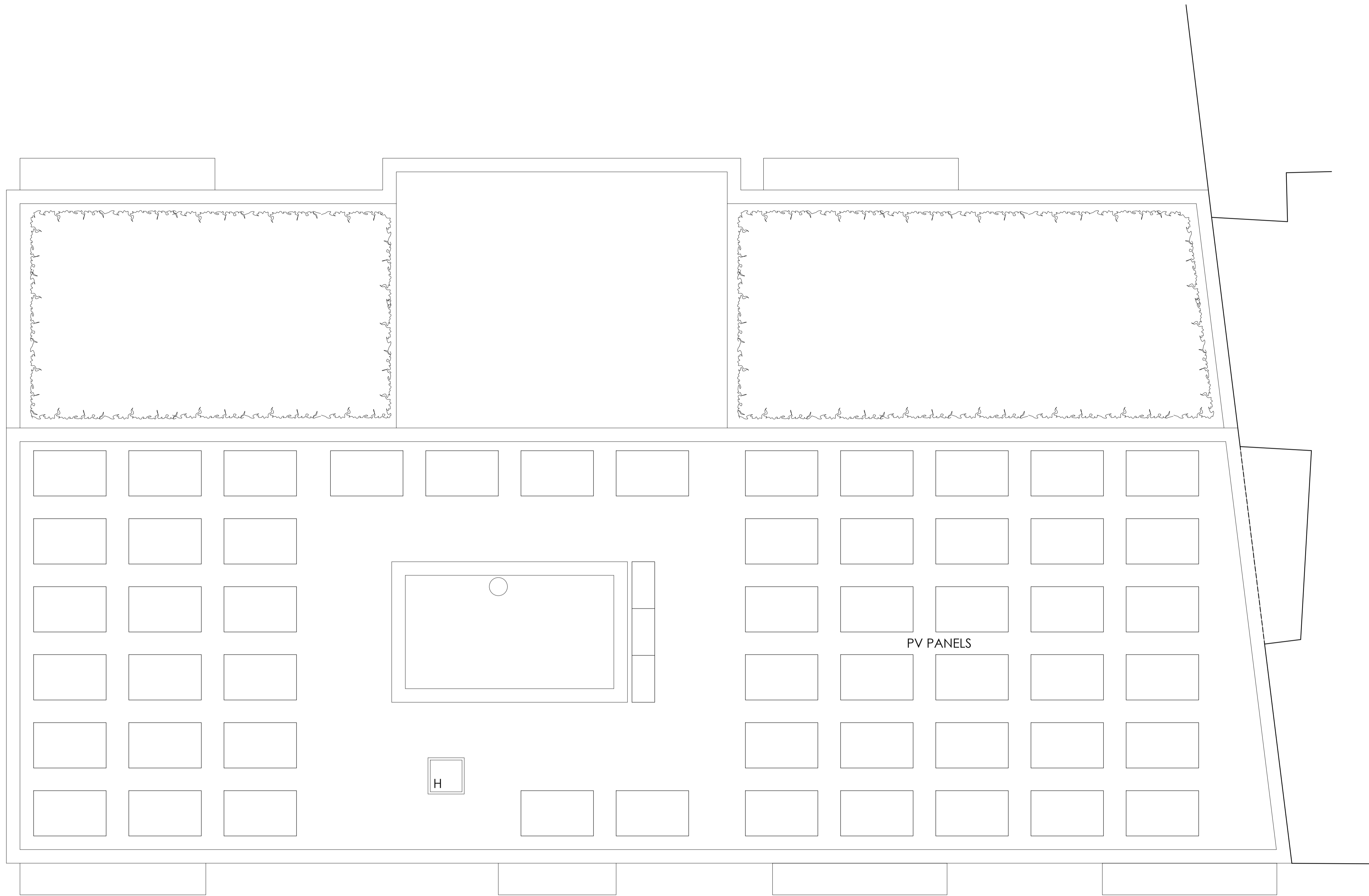
description
 FIFTH FLOOR PLAN
 AS PROPOSED



burgess mean architects
 unit 1, the warehouse
 12 ravenbury terrace
 london
 SW18 4RL

f: 020 8944 8050 f: 020 8946 9506 e: bma@burgessmean.co.uk

date: JANUARY 2019	scale: 1:50 (A1)	project: 181202	drawing: P/04	revision: G
drawn by: SA				
checked:				



ROOF PLAN

PLANNING DRAWING (26 FLATS)

Rev G: 10/06/2020: Building repositioned to suit planning officers' comments.
 Rev F: 19/09/2019: General update to suit planning consultants comments.
 Rev E: 13/09/2019: Updated to suit planning application.
 Rev D: 28/08/2019: Completely redrawn and drawing renumbered.

title
 16-20 MORDEN ROAD, SOUTH WIMBLEDON
 LONDON SW19 3BN

client
 PEER SECURITIES LTD

description
 ROOF PLAN
 AS PROPOSED



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 unit 1, the warehouse
 12 ravenbury terrace
 london
 SW18 4RL

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date: JANUARY 2019	scale: 1:50 (A1)	project: 181202	drawing: P/06	revision: G
drawn by: SA				
file name:				
checked:				

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PLANNING APPLICATIONS COMMITTEE

22 October 2020

<u>UPRN</u> 48072664	<u>APPLICATION NO.</u> 20/P1483	<u>Item No: 10</u> <u>DATE VALID</u> 04/05/2020
Address/Site:	2 Westcoombe Avenue West Wimbledon London SW20 0RQ	
Ward:	Raynes Park	
Proposal:	Erection of two storey side extension, a part-single part-two storey rear extension, front porch extension and rear roof extensions with associated facade changes and landscaping.	
Drawing No.'s:	1628/20/BR/01;1628/20/BR/02; 1628/20/BR/03; 1628/20/BR/04 & 1628/20/GA/01.	
Contact Officer:	Tony Smith (020 8545 3144)	

RECOMMENDATION

Grant planning permission subject to any resolution made by Committee pertaining to facing material and conditions.

CHECKLIST INFORMATION

- S106: No
- Is a screening opinion required: No
- Is an Environmental Statement required: No
- Has an Environmental Statement been submitted: No
- Press notice: No
- Site notice: Yes
- Design Review Panel consulted: No
- Number of neighbours consulted: 2
- External consultations: 0
- Conservation area: No, but adjacent to Westcoombe Avenue Conservation Area
- Listed building: No
- Tree protection orders: No
- Controlled Parking Zone: No
- Flood risk zone: No
- Designated Open Space: No

1. INTRODUCTION

- 1.1 This application is brought to the Planning Applications Committee for further consideration on the proposed choice of materials, following the decision at the previous meeting (September 24th 2020) to Grant Planning Permission subject to conditions as set out in the report, plus a requirement for the Committee to view the

materials to be used for the proposal. It should be noted that all other matters were considered acceptable and this report relates solely to the proposed materials of the development.

1.2 Whilst Members found the siting, form, scale and design of the proposal acceptable, there was concern that the proposed materials could result in a detrimental impact to the character and appearance of the host dwelling and surrounding area. A condition was previously recommended requiring the submission of further details on the proposed materials prior to the commencement of development, however, it was considered that members would be required to view the materials. Given meetings are being held virtually, the applicant has supplied a supplementary document for presentation to members which details exact specifications for each of the facing elements of the extensions.

1.3 The applicant has provided two options for consideration, namely, Option A and Option B. It is understood that the choices have been made with regard to the availability of materials as there has been general supply issues due to the ongoing pandemic.

1.4 Option A

Tiles – the development would utilise a brown tile to the roofs and wall hung sections to the front wall and bay section. The colour would be ‘Marley Antique Brown’. The proposed tiles would be of a similar appearance to the existing roof tiles at the host dwelling and 233 Coombe Lane in both texture and colour.

Render – the development would utilise a white render to the facing walls at the rear and sides and part of the front. The exact specification would be ‘K Rend Silicone TC10 Coloured Render White’. The proposed render would be of a similar colour and texture to existing rendered elements to the front, rear and sides, although it is acknowledged that the existing render may be discoloured due to the passage of time. The proposed render is noted as being maintenance free.

Brickwork – the development would utilise deep red/brown facing bricks to the ground floor elements within the front elevation. The exact specification would be ‘Fonterra LBC Pressed Facing Brick’. The proposed brickwork would be marginally darker than existing brickwork. It should be noted that the pointing shown within the example image is only illustrative and the wording of the condition has been amended to require pointing to match the existing.

Windows – the development would utilise Grey Aluminium Coated window frames to match the adjoining neighbour at 233 Coombe Lane. Existing windows are unpainted Grey Aluminium which have a light grey appearance.

1.5 Option B

Tiles – the development would utilise a lighter brown tile to the roofs and wall hung sections to the front wall and bay section. The colour would be ‘18 Hedgerow Brown (Granular) Concrete Tiles’.

Render – the development would utilise a white render to the facing walls at the rear and sides and part of the front. The exact specification would be ‘K Rend Silicone TC10 Coloured Render White’. The proposed render would be of a similar colour and texture to existing rendered elements to the front, rear and sides, although it is acknowledged that the existing render may be discoloured due to the passage of time. The proposed render is noted as being maintenance free.

Brickwork – the development would utilise red facing bricks to the ground floor

elements within the front elevation. The exact specification would be 'London Brick Company Facing Brick Sandfaced'. The proposed brickwork would be marginally lighter than existing brickwork. It should be noted that the pointing shown within the example image is only illustrative and the wording of the condition has been amended to require pointing to match the existing.

Windows – the development would utilise Grey Aluminium Coated window frames to match the adjoining neighbour at 233 Coombe Lane. Existing windows are unpainted Grey Aluminium which have a light grey appearance.

- 1.5 It is considered that either of the proposed material palettes would be of a similar appearance to the host building and surrounding properties, and these would serve to retain the character and appearance of the streetscene. If members agree on a particular option, a suitably worded condition can then be included requiring the development to be carried out in accordance with these details. It is also recommended to require the pointing of the brickwork to match the existing to further safeguard the appearance.
- 1.6 The proposed development would therefore accord with London Plan Policies 7.4 and 7.6, 7.8, Merton's Core Strategy Policy CS14 and Merton's Sites and Policies Plan Policies DM D2 and DM D4.

RECOMMENDATION.

Grant planning permission subject to any resolution made by Committee pertaining to facing material and the conditions specified in the report annexed below.

APPENDIX A: REPORT FROM THE PREVIOUS MEETING (SEPTEMBER 24TH 2020) WITH AMENDMENT TO MATERIALS CONDITION TO INSERT APPROVED MATERIAL OPTION.

2. SITE AND SURROUNDINGS

- 2.1 The application site comprises a roughly triangular plot located on the western side of Westcoombe Avenue, on the corner of the junction with Coombe Lane within West Wimbledon. The site features a two storey, detached, single family dwellinghouse which is set back from the street with an off-street private parking area served by a vehicle crossover to the front. Two metre high close boarded fencing surrounds the site, including to the front and the garden area lies to the rear (west) and side (south).
- 2.2 The property is of a typical 1930's design, with a two storey angled bay to the front with a small gabled roof protrusion above. The front facade is stepped with the western element set back from both the front and rear elevations resulting in a secondary hipped roof. The façades features a mixture of materials, with brickwork to the ground floor and wall hung tiles to the first floor of the main façade. The western side element features white render to ground and first floor levels and typical roof tiles are present at roof level.
- 2.3 The surrounding area is residential in character with semi-detached dwellings being the predominant housing type, however, a number of terraced and detached dwellings are present in the area. The site does not lie within a conservation area, nor is it a listed

building, however, the site is adjacent to the Westcoobme Avenue Conservation Area to the south and west. The dwelling appears to have been built as a part of and at the same time as those erected in the conservation area by Messrs Crouch, a house building company active in Merton and elsewhere in south west London in the interwar period.

- 2.4 The site has a public transport accessibility level (PTAL) of 2 which is considered poor (with 0 being the lowest and 6b being the highest).

3. CURRENT PROPOSAL

- 3.1 This application seeks planning permission for the erection of a two storey side extension, a part-single part two-storey rear extension, a front porch extension and side/rear roof extensions with associated façade changes and landscaping.

- 3.2 It is proposed to erect a two storey side extension to the southern flank of the dwelling to replace the existing addition, which would be set back from the front façade at both ground and first floor level with a hipped roof continuing from the main roof. The extension would incorporate a two storey angled bay window and roof projection to the front, and would incorporate window units to the front and rear with a sets of folding doors at rear ground level to the rear and side. The rear first floor window would be obscured glazed. The extension would utilise render and roof tiles to the bay and roof, with UPVC windows.

- 3.3 A part-single, part-two storey extension would then be constructed at the rear along the full width of the dwelling. The extension would have a hipped roof form, with a central first floor element of flat roof design. The ground floor would feature folding doors and window units with full height windows in the central element up to first floor. The extension would utilise render to elevations and roof tiles to the hipped single storey roof.

- 3.4 It is also proposed to erect a rear roof extension which would be of a flat roof dormer design. The dormer would run the width of the ridge, being set back from the hipped ends and would have a central recessed portion. Typical window units would be inserted in the rear face with the southernmost window being obscure glazed. The dormer would be clad in render.

- 3.5 A small extension to the front porch is also proposed, which would have a flat roof and use brickwork to the front façade. Landscaping would include permeable resin bound gravel to the front, with timber decking to the side and immediately to the rear. The rear garden would be soft landscaped with planting to the northern boundary. The high timber boarded fencing to the front drive would be replaced with a low level brick wall and planting, with a 2m high brickwork boundary further to the south.

- 3.6 The proposed extensions would have the following dimensions:

- Two storey side: 4.3 - 6.9m wide, 7.81m to 8.5m length, 5.7m high to the eaves and 8.5m maximum height.
- Part-single, part-two storey rear: 1.3m length, 12.8m width, 3m eaves height, 3.5m max height, 6m first floor roof height.
- Rear roof: 2.3m height, 3.1m depth, 7m width.
- Front porch: 0.5m length, 2.2m width, 3m height.

3.7 Amendments:
It should be noted that the original scheme has been amended in the following ways since submission:

- Additional wall hung tiles and ground floor facing brick to front
- Replacement of grey roof tiles to dark brown
- Obscuring of windows closest to no. 4 Westcoombe Avenue

4. PLANNING HISTORY

4.1 No recorded planning history.

5. CONSULTATION

5.1 Public consultation was undertaken by way of post sent to neighbouring properties. An additional round of consultation was carried out following the receipt of amended drawings. The outcome of the combined consultation is summarised as follows:

5.2 Representations were received from 9 individuals who raised the following concerns:

- Scale of side extension appearing as two semi-detached dwellings
- Reduction in garden space
- Overdevelopment
- Overlooking and loss of privacy from rear / side windows
- Overbearing massing
- Examples in Design & access statement are irrelevant
- Impact to Conservation Area
- Failure to respect original character of dwelling

5.3 Officer's response:

The impact of the proposal on the amenity of the neighbour's and its impact on the character and appearance of the surrounding area will be address in the relevant sections below.

6. POLICY CONTEXT

6.1 National Planning Policy Framework (2019)

12. Achieving well-designed places

16. Conserving and enhancing the historic environment

6.2 London Plan (2016)

Relevant policies include:

7.4 Local character

7.6 Architecture

6.3 Merton Local Development Framework Core Strategy – 2011 (Core Strategy)

Relevant policies include:

CS 14 Design

6.4 Merton Sites and Policies Plan – 2014 (SPP)

Relevant policies include:

DM D2 Design considerations

DM D3 Alterations and extensions to existing buildings

DM D4 Managing heritage assets

6.5 Supplementary planning considerations

London Character and Context SPG -2014

Westcoombe Avenue Conservation Area Design Guide

7. PLANNING CONSIDERATIONS

7.1 Material Considerations

The key planning considerations for the proposed development include the impact on the character and appearance of the host building, surrounding area and adjacent Conservation Area, and the impact on neighbouring amenity.

Design and impact upon the character and appearance of the area

7.2 Policy DM D2 and DM D4 of Merton's Sites and Policies Plan requires development to relate positively and appropriately to the siting, rhythm, scale, density, proportions, height, materials and massing of surrounding buildings and existing street patterns, historic context (including conservation areas), urban layout and landscape features of the surrounding area and to use appropriate architectural forms, language, detailing and materials which complement and enhance the character of the wider setting. The requirement for good quality design and protection of heritage assets is further supported by the London Plan Policies 7.4 and 7.6, 7.8 and Merton's Core Strategy Policy CS14. Policy DMD4 specifically requires developments not to adversely impact the significance of heritage assets and their settings.

7.3 Site setting

The application site is located on the junction of Westcoombe Avenue and Coombe Lane, being one of two houses on the street which are not included within the Westcoombe Avenue Conservation Area, with the other site being no. 1a on the opposite side of the street. The Westcoombe Avenue Conservation Area Design Guide notes that dwellings in the area were built in approximately 1935 and since that time two additional houses have been built within the Conservation Area, with one newer house immediately outside the boundary. Historical mapping shows that the host dwelling was in situ from at least 1953, prior to the surrounding areas designation in 1990. The Design Guide states 'the boundary line for the CA is easy to justify. It includes all of the dwellings which were built to the particular two designs, and but for the two recent infill dwellings, it excludes all other dwelling styles'. The guide goes further on to give details on the two distinct semi-detached dwelling typologies.

7.4 The host dwelling was in place at the time of the designation of the conservation area and is therefore not considered to be in keeping with the distinct building typologies of those within it. The host dwelling has clearly been built in the same style as the adjoining semi-detached pair to the north at nos. 233 & 235 Coombe Lane. Features include hanging tiles to the primary first floor facade (not exclusive to the bay window), with brickwork at ground level. The two storey side element is rendered to the front and sides and corner wraparound windows are a feature to the front elevation. No. 233 has also recently undertaken recent works to construct a two storey side extension which incorporates an oriel style window at first floor and dark framed windows. As such, it is considered that any proposals should not be limited in seeking to replicate the design features of the adjoining conservation area, given that the character of the property is substantially different, but should respect the adjoining semi-detached pair to the north and the general surrounding area.

7.5 An assessment of the various elements of the scheme in relation to the above will be detailed below.

7.6 Materials

The proposal seeks to retain the material palette to the front through the inclusion of dark brown tiles to the primary front wall and first floor bays, together with dark brown brickwork to the primary front and ground floor bay sections. White render would be continued from the existing side element to the remainder of the extension and other

facing walls. The roof would utilise red tiles as per the existing dwelling and adjoining semi-detached pair. Whilst grey window frames would be used in the development, these would be of a similar appearance to the adjoining property at no. 233 Coombe Lane. Given the above, it is considered that the material choice would be of a satisfactory appearance to respect the character of the original building and surrounding area. In order to ensure that high quality and appropriate materials are used to achieve the above, a condition is recommended requiring samples and details of materials to be agreed with the Local Planning Authority prior to construction.

7.7 Two storey side extension and front porch

The proposed two storey side extension would take a subordinate approach, retaining a set back from the front façade and incorporating a hipped roof that is set back from the main roof. It would also incorporate a two storey bay window to the front, as well as a front oriel style window and front/side wraparound window. It is noted that two storey side extensions are common in the wider area, of differing sizes. The front porch extension would be small in scale and would utilise brickwork to match the remaining. In this instance, it is considered the scale, form, design and massing of the extensions would meet a satisfactory level of subordination to the original dwelling so as to respect its original character and appearance in this regard.

7.8 Part-single, Part-two storey rear extension

The extension to the rear would incorporate a small single storey rear addition with a central, first floor rear extension. It is recognised that this element of the scheme would be somewhat more modern in appearance, however, due its siting at the rear of the property, there would be limited views from the streetscene. Given this, together with its modest scale and massing, and incorporation of a hipped roof and dark tiles to the ground floor, it is considered there would not be such a harmful impact to the character and appearance of the host dwelling or streetscene so as to warrant a refusal.

7.9 Rear roof extension

The rear roof extension would be of a dormer typical dormer style and would be sited within the hipped ends of the newly constructed roof. It would feature facing walls and a roof style to mimic that of the first floor element. The rear face would incorporate a central recessed area to reduce its bulk at roof level and give an appearance of two smaller, connected dormers. Similarly to the rear extension, given its siting to the rear of the property, together with the angle of building in relation to the street, there would only be limited views of the side of the roof extension from the streetscene which would not appear dissimilar to other roof extensions in the area. As such, it is considered the roof extension would have an acceptable impact on the character and appearance of the host building and surrounding area.

7.10 Other works

Other works to the property include the landscaping of the areas to the front, side and rear of the building, together with a change in design to the front boundary. It is considered these works would be of a satisfactory appearance.

7.11 As a whole, it is considered the proposal would not result in a detrimental impact to the character and appearance of the host building or surrounding area, and the character and appearance of the adjacent Conservation Area would be preserved. Therefore, the proposal complies with the principles of policies DMD2, DMD3, DMD4 of the Adopted SPP 2014, CS 14 of the LBM Core Strategy 2011 and 7.4 and 7.6 of the London Plan 2016.

Impact upon neighbouring amenity

- 7.12 London Plan policies 7.6 and 7.15 along with SPP policies DM D2 and DM EP2 state that proposals must be designed to ensure that they would not have an undue negative impact upon the amenity of neighbouring properties in terms of light spill/pollution, loss of light (sunlight and daylight), quality of living conditions, privacy, visual intrusion and noise.
- 7.13 Impact to no. 233 Coombe Lane
The majority of the proposal would be sited away from this neighbour, with only the rear extension and front porch extension extending beyond the building lines of the existing dwelling. Given the modest increase in depth of the building lines of these elements, it is considered there would not be a materially harmful impact in terms of visual intrusion, loss of light, shadowing or a sense of overbearing. Furthermore, given the presence of existing windows at first floor level, it is considered the proposed roof extension would not provide a greater deal of overlooking than that which already exists.
- 7.14 Impact on no. 4 Westcoombe Avenue
The proposal would result in a two storey side extension being built towards the shared boundary with no. 4 and this neighbour has objected on the grounds of visual intrusion/overbearing together with loss of privacy from additional windows. The proposed side extension would be angled relative to the boundary with this neighbour due to the splayed plots around the corner of Westcoombe Avenue. As such, the closest point of the extension would be the southern corner, with the facing walls extending away from this neighbour. It is noted that this neighbour exhibits an outbuilding along the shared boundary at this point, together with a garage fronting the street. Given the presence of these structures, together with the angled nature of the extension, it is considered the impact in terms of visual intrusion and bulk would be acceptable. In terms of loss of sunlight/daylight and shadowing, the proposed development would be sited to the north and would therefore not result in a materially harmful impact.
- 7.15 Concerns of loss of privacy and overlooking from additional windows was also raised. It is recognised that windows presently exist in the rear elevation of the host dwelling, but that a side extension would result in windows closer to the boundary. The proposed additional windows at first and roof level closest to the boundary serve non-habitable rooms and are indicated as being obscure glazed and fixed shut to 1.7m. As such, it is considered the privacy of this neighbour would be maintained and an appropriate condition is recommended to safeguard this. The proposed wraparound window to the front/side corner of the extension would be positioned as such that views would only be available towards Westcoombe Avenue, with oblique views to the front corner of this neighbours site where the garage is situated due to the angle of the plot and its position.
- 7.16 As a whole, it is considered the proposal would not result in an undue detrimental impact to neighbouring amenity. The proposal would therefore accord with the principles of policy DMD2 of the Sites and Policies Plan 2014.

8. CONCLUSION

- 8.1 Notwithstanding the somewhat modern approach to the detailed design of the remodeled and extended dwelling, officers consider the form and massing of the extensions is compatible with the host dwelling. It is considered that the proposal would not result in a detrimental impact to the character and appearance of the host building

and surrounding area or on neighbouring amenity. Given the house lies outside the Westcoombe Avenue conservation area it could appear unreasonable to require the remodeled and dwelling to slavishly replicate the detailing of the existing house or those in the conservation area in terms of fenestration and external materials. It is considered that the character and appearance of the adjacent Conservation Area would be preserved. Therefore, the proposal complies with the principles of policies DMD2, DMD3, DMD4 of the Adopted SPP 2014, CS 14 of the LBM Core Strategy 2011 and 7.4 and 7.6 of the London Plan 2016.

RECOMMENDATION

Grant planning permission subject to the following conditions:

Conditions:

- 1) Standard condition [Commencement of development]: The development to which this permission relates shall be commenced not later than the expiration of 3 years from the date of this permission.

Reason: To comply with Section 91 (as amended) of the Town & Country Planning Act 1990.

- 2) Standard condition [Approved plans]: The development hereby permitted shall be carried out in accordance with the following approved plans: [Refer to the schedule on page 1 of this report].

Reason: For the avoidance of doubt and in the interests of proper planning.

- 3) Amended standard condition [Materials]: The facing materials to be used for the development hereby permitted shall be those specified under option 'X' in the approved material schedule document titled 'Material Schedule 14.10.2020'. The pointing of the brickwork sections shall match that of the existing brickwork in both style and colour.

Reason: To ensure a satisfactory appearance of the development and to comply with the following Development Plan policies for Merton: policy 7.6 of the London Plan 2016, policy CS14 of Merton's Core Planning Strategy 2011 and policies DMD2 and DMD3 of Merton's Sites and Policies Plan 2014.

- 4) Amended standard condition [Obscure glazed windows]: Before the development hereby permitted is first occupied, the southernmost first and second floor windows in the rear elevation as shown on the approved drawings shall be glazed with obscured glass and fixed shut to 1.7m above internal floor level and shall be maintained as such thereafter.

Reason: To safeguard the amenities and privacy of the occupiers of adjoining properties and to comply with the following Development Plan policies for Merton: policy 7.6 of the London Plan 2016, policy CS14 of Merton's Core Planning Strategy 2011 and policies DM D2 and D3 of Merton's Sites and Policies Plan 2014.

- 5) Standard condition [Timing of construction]: No demolition or construction work or ancillary activities such as deliveries shall take place before 8am or after 6pm Mondays - Fridays inclusive, before 8am or after 1pm on Saturdays or at any time on Sundays or Bank Holidays.

Reason: To safeguard the amenities of the area and the occupiers of neighbouring

properties and ensure compliance with the following Development Plan policies for Merton: policy 7.15 of the London Plan 2016 and policy DM EP2 of Merton's Sites and Policies Plan 2014.

- 6) Standard condition [Hardstandings]: The hardstanding hereby permitted shall be made of porous materials, or provision made to direct surface water run-off to a permeable or porous area or surface within the application site before the development hereby permitted is first occupied or brought into use.

Reason: To reduce surface water run-off and to reduce pressure on the surrounding drainage system in accordance with the following Development Plan policies for Merton: policy 5.13 of the London Plan 2016, policy CS16 of Merton's Core Planning Strategy 2011 and policy DMF2 of Merton's Sites and Policies Plan 2014.

- 7) Standard condition [Access to flat roofs]: Access to the flat roof of the development hereby permitted shall be for maintenance or emergency purposes only, and the flat roof shall not be used as a roof garden, terrace, patio or similar amenity area.

Reason: To safeguard the amenities and privacy of the occupiers of adjoining properties and to comply with the following Development Plan policies for Merton: policy 7.6 of the London Plan 2016, policy CS14 of Merton's Core Planning Strategy 2011 and policies DM D2 and D3 of Merton's Sites and Policies Plan 2014.

- 8) Standard condition [Landscaping]: All hard and soft landscape works shall be carried out in accordance with the approved details as shown in the approved drawings. The works shall be carried out in the first available planting season following the completion of the development or prior to the occupation of any part of the development, whichever is the sooner, and any trees which die within a period of 5 years from the completion of the development, are removed or become seriously damaged or diseased or are dying, shall be replaced in the next planting season with others of same approved specification, unless the Local Planning Authority gives written consent to any variation. All hard surfacing and means of enclosure shall be completed before the development is first occupied.

Reason: To enhance the appearance of the development in the interest of the amenities of the area, to ensure the provision sustainable drainage surfaces and to comply with the following Development Plan policies for Merton: policies 5.12 and 5.13 of the London Plan 2016, policies CS13 and CS16 of Merton's Core Planning Strategy 2011 and policies DM D2, F2 and O2 of Merton's Sites and Policies Plan 2014

Informatives:

- 1) **INFORMATIVE**
In accordance with paragraph 38 of the National Planning Policy Framework 2018, The London Borough of Merton takes a positive and proactive approach to development proposals focused on solutions. The London Borough of Merton works with applicants or agents in a positive and proactive manner by suggesting solutions to secure a successful outcome; and updating applicants or agents of any issues that may arise in the processing of their application. In this instance the Planning Committee considered the application where the applicant or agent had the opportunity to speak to the committee and promote the application.

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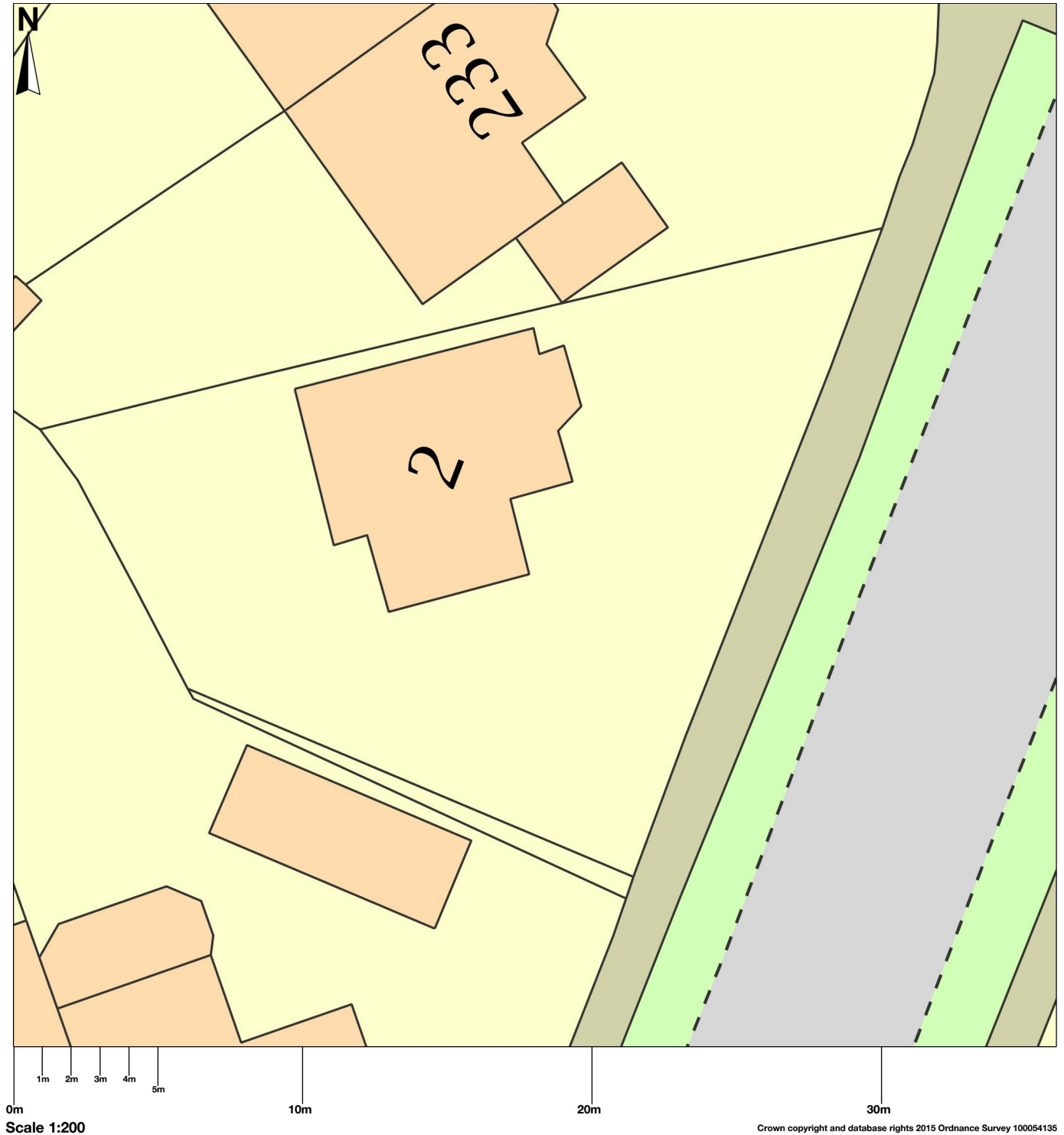
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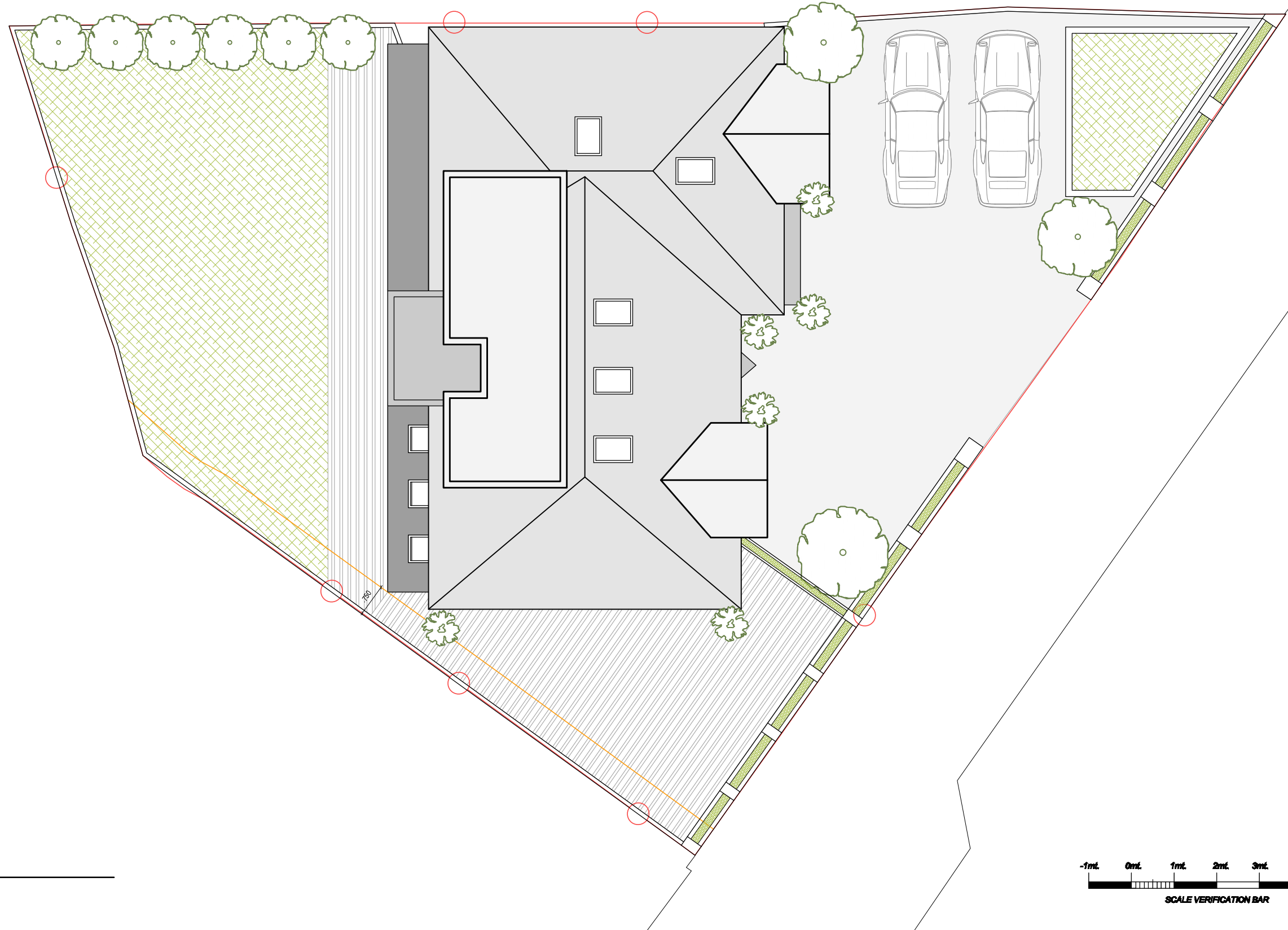
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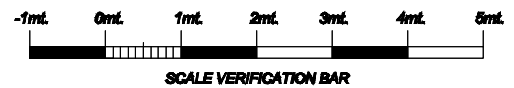


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LA-01 SCALE: 1= 100



REVISIONS	

OAKFIELD GU21 Ltd
CHRISTIAN DA SILVA

WESTCOOMBE
2 Westcoombe avenue
SW20 0EQ

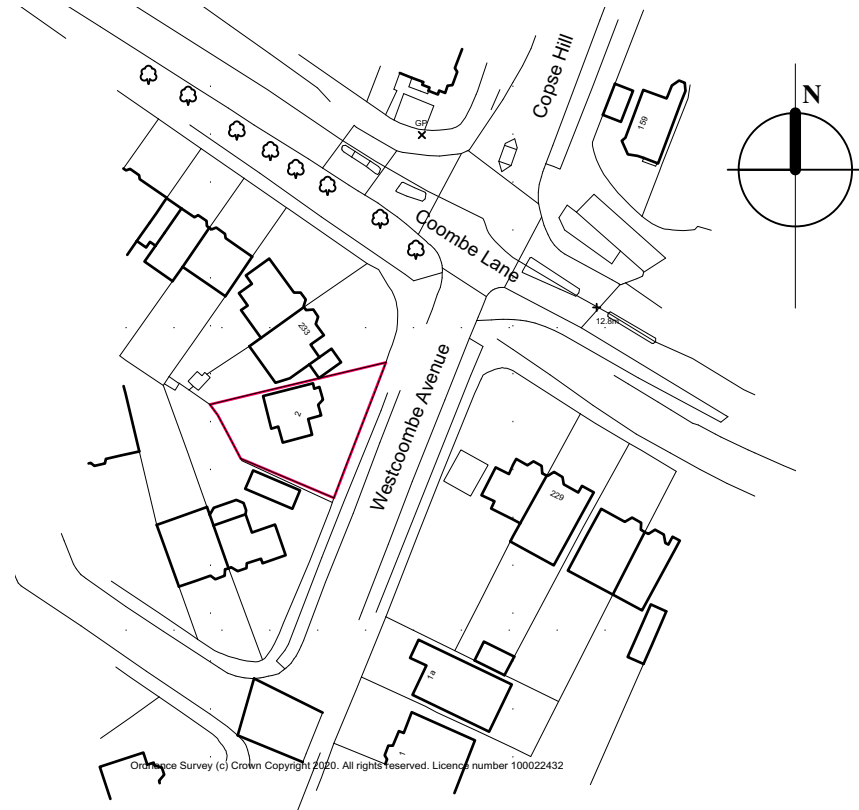
PROPOSED SITE
PLAN

3 MAY 2020
CLIENT:
MR & MRS
LOMBAARD

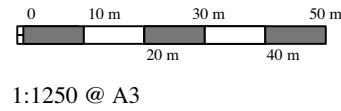
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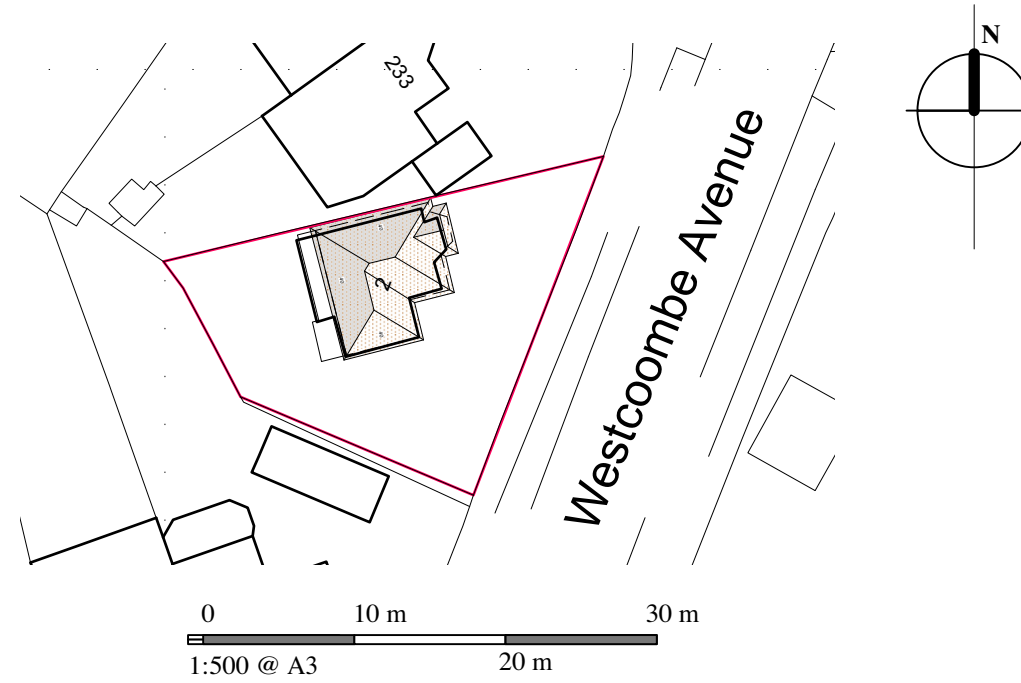


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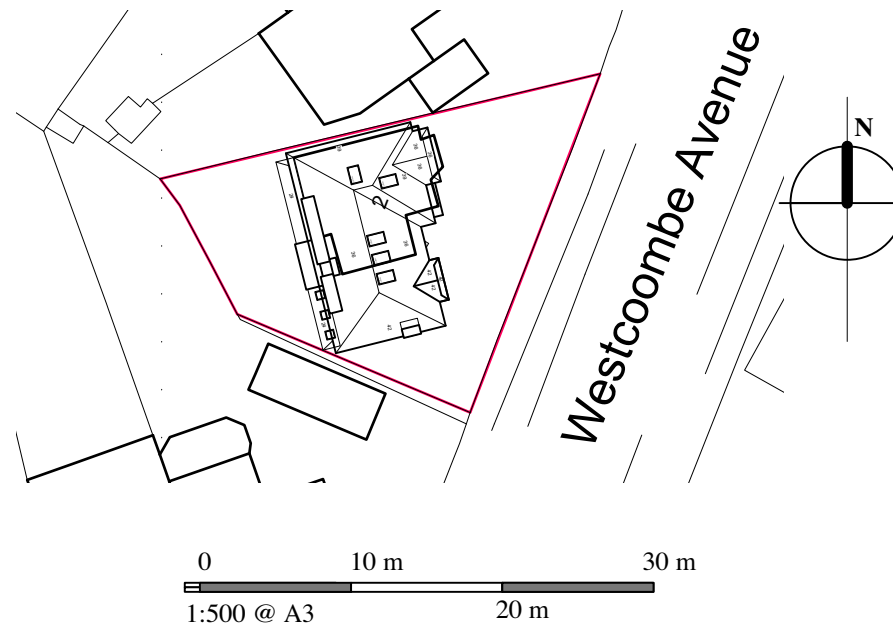


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Location Plan 1:1250

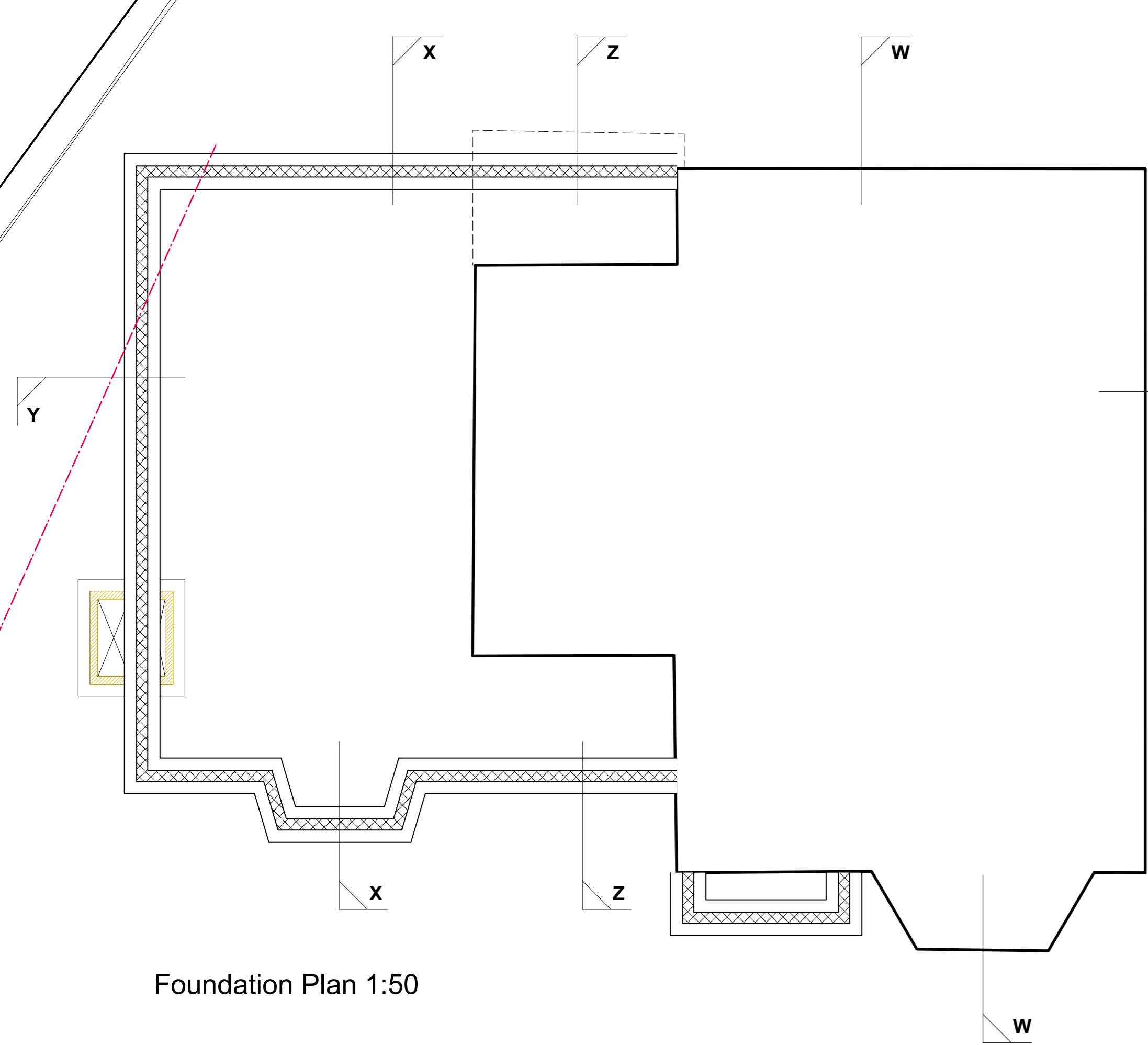


Block/Roof Plan [Existing] 1:500

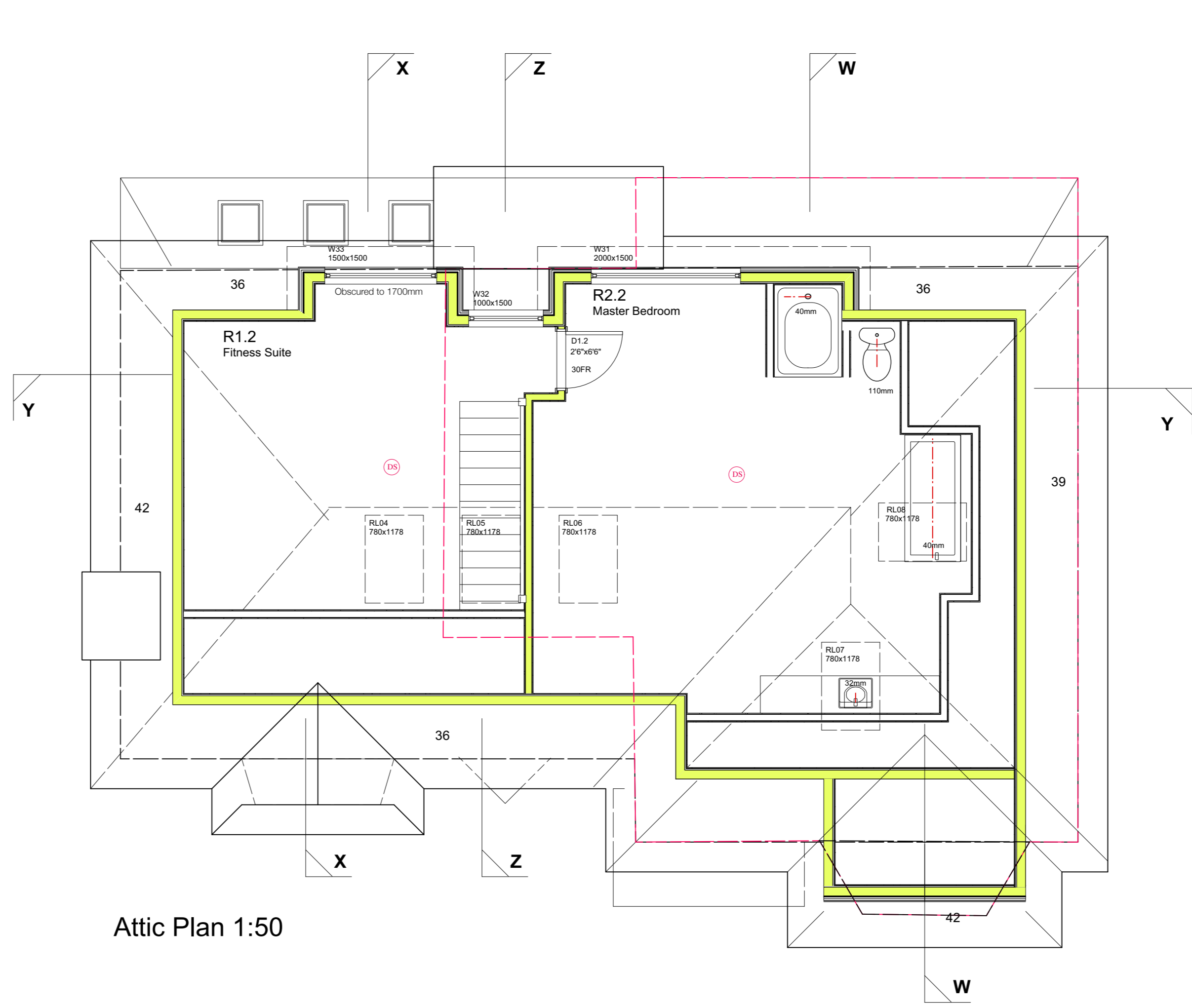


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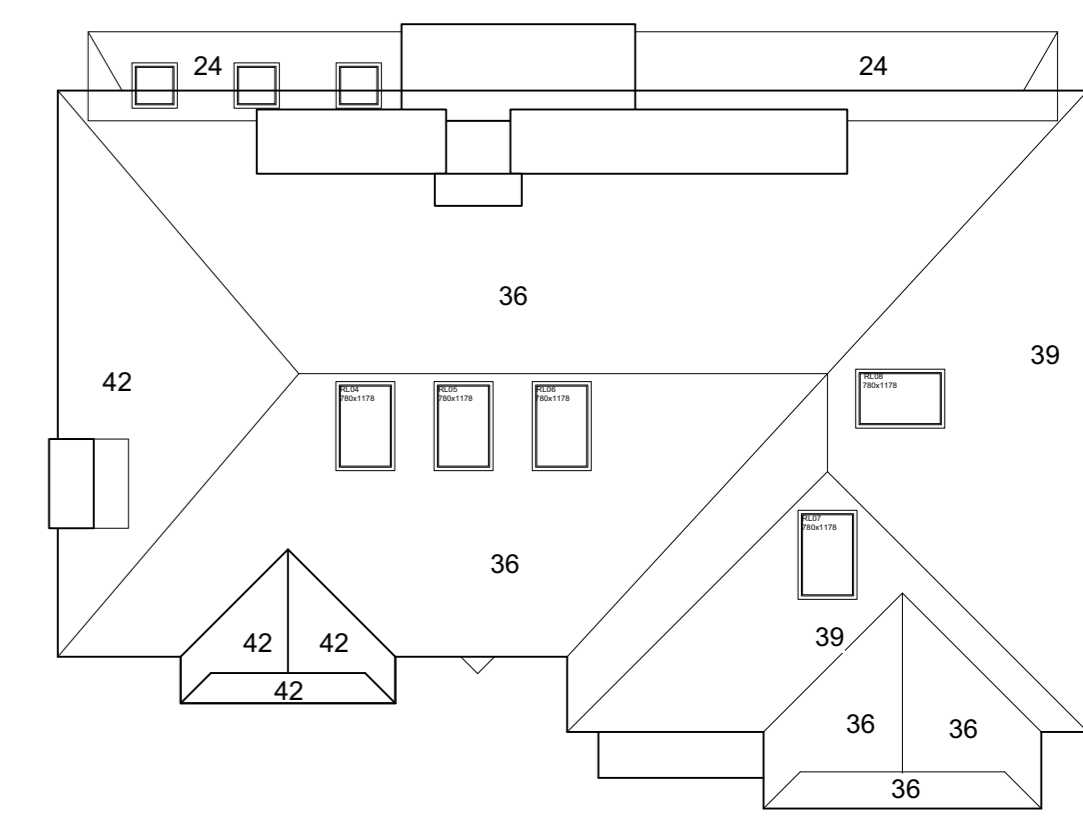
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Foundation Plan 1:50

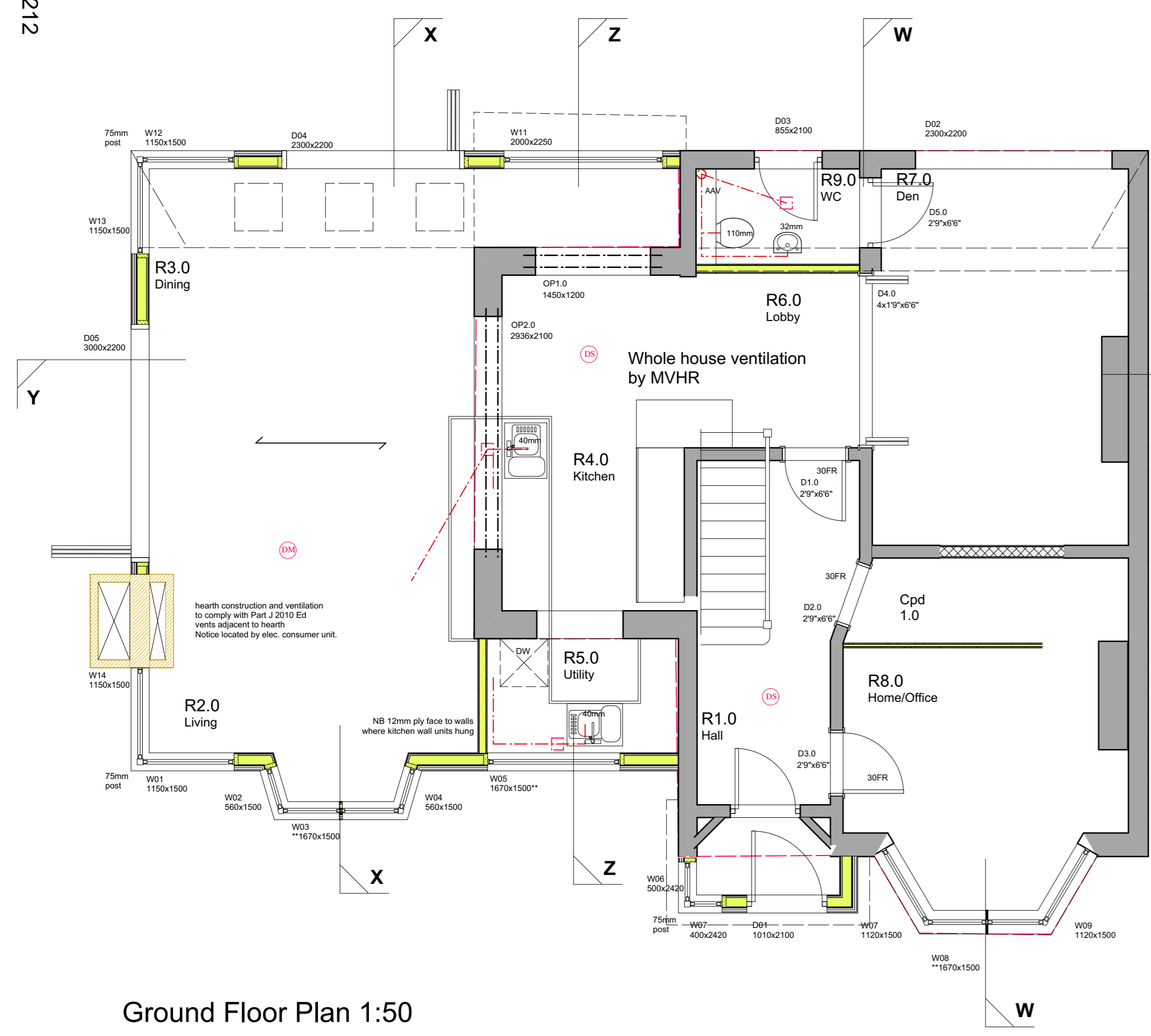


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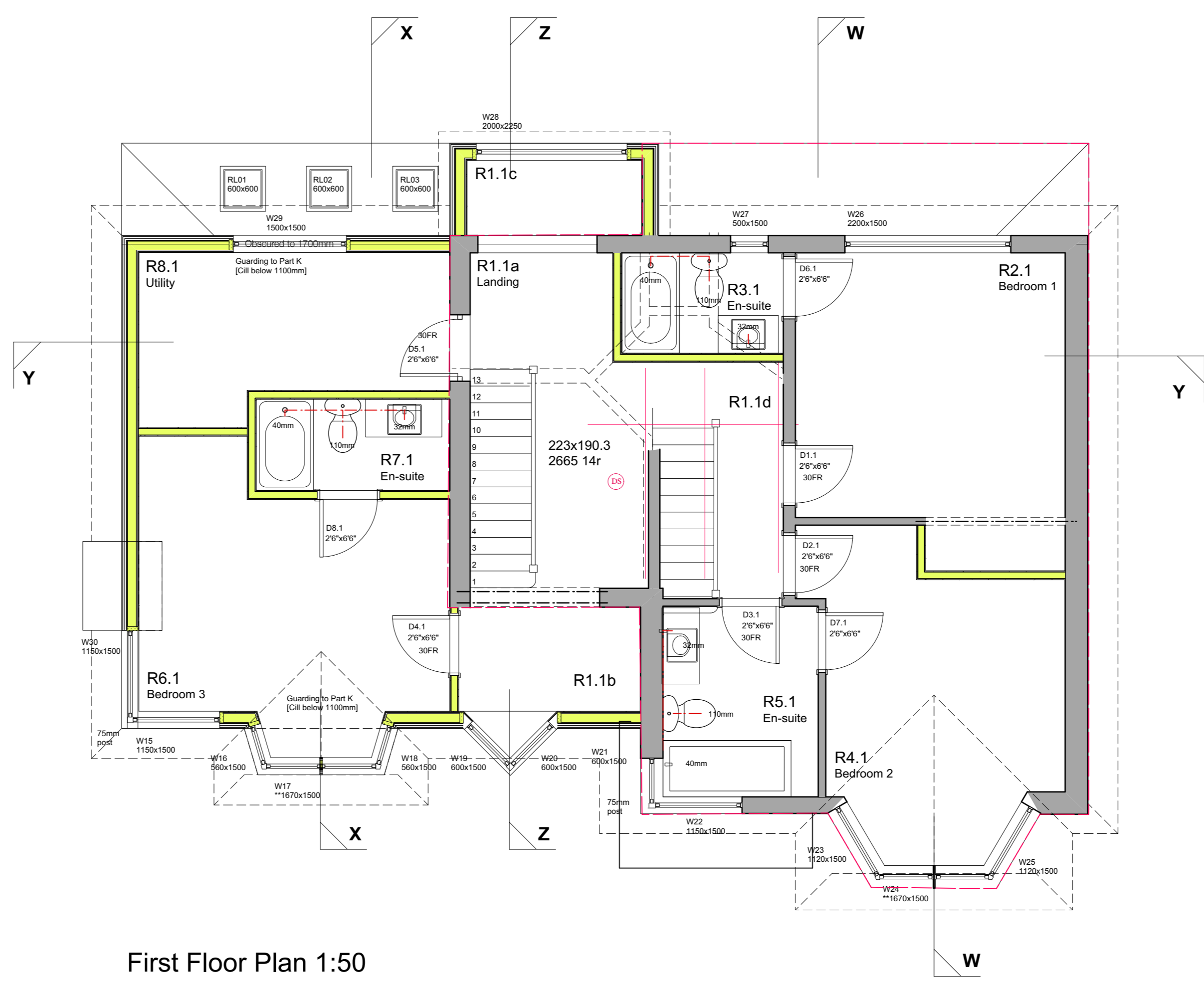


Roof Plan 1:100

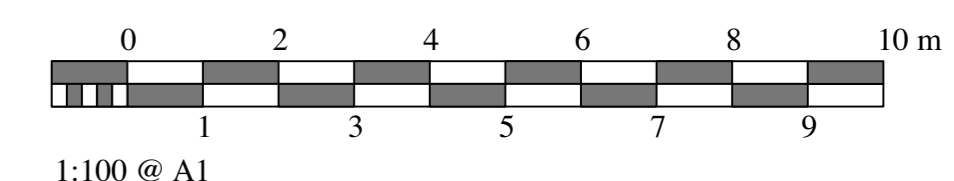
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Ground Floor Plan 1:50



First Floor Plan 1:50



1:100 @ A1

Rev Date Amend Revision
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 Client: MR & MRS P LOMBAARD

Project: PROPOSED EXTENSION & ALTERATIONS TO 2 WESTCOOMBE AVENUE LONDON SW20 ORQ

Drawing: PROPOSED FLOOR PLANS

Drg No: 1628/20/BR/02

Scales: 1:100, 1:50 Date: MAY 2020

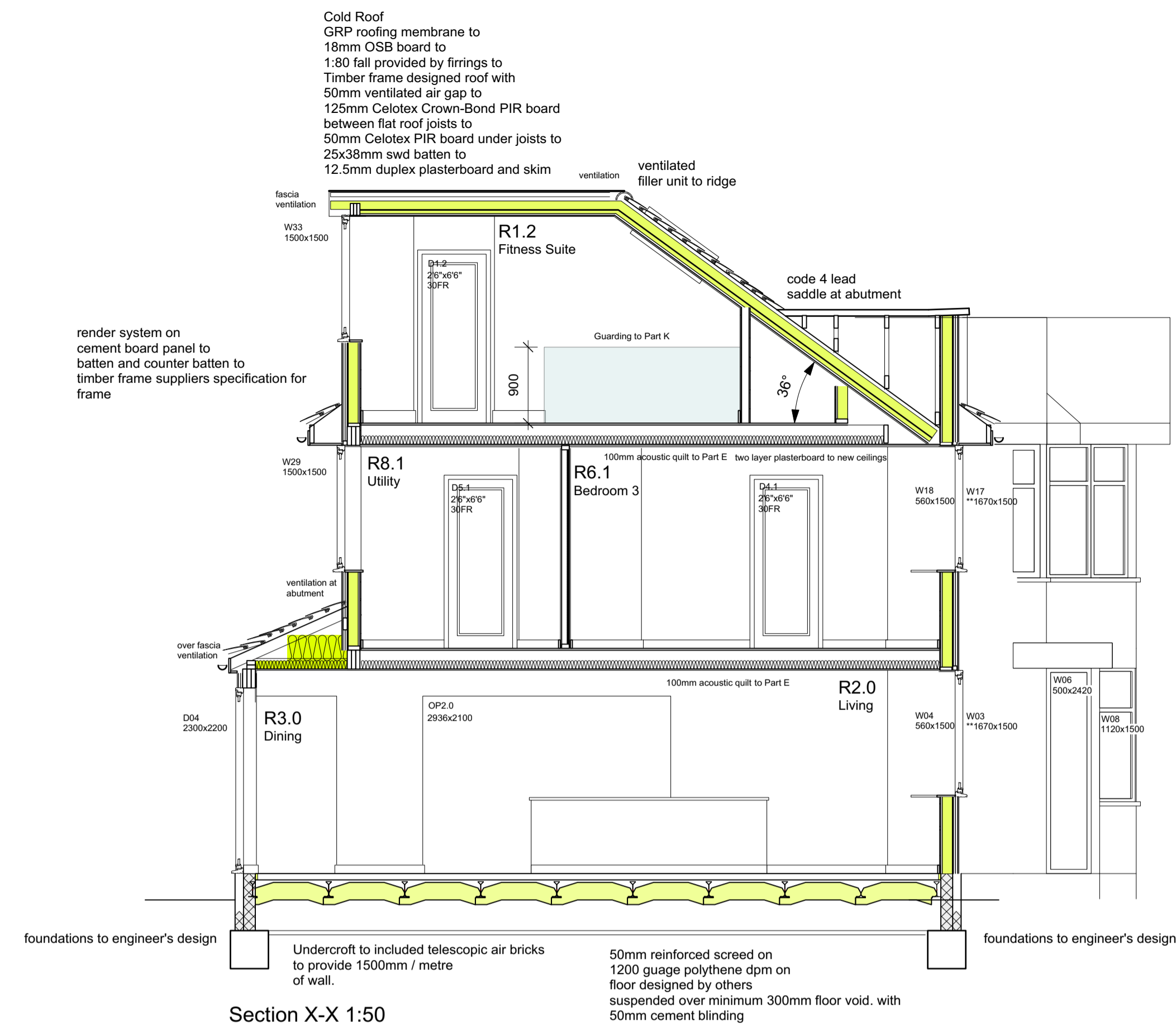
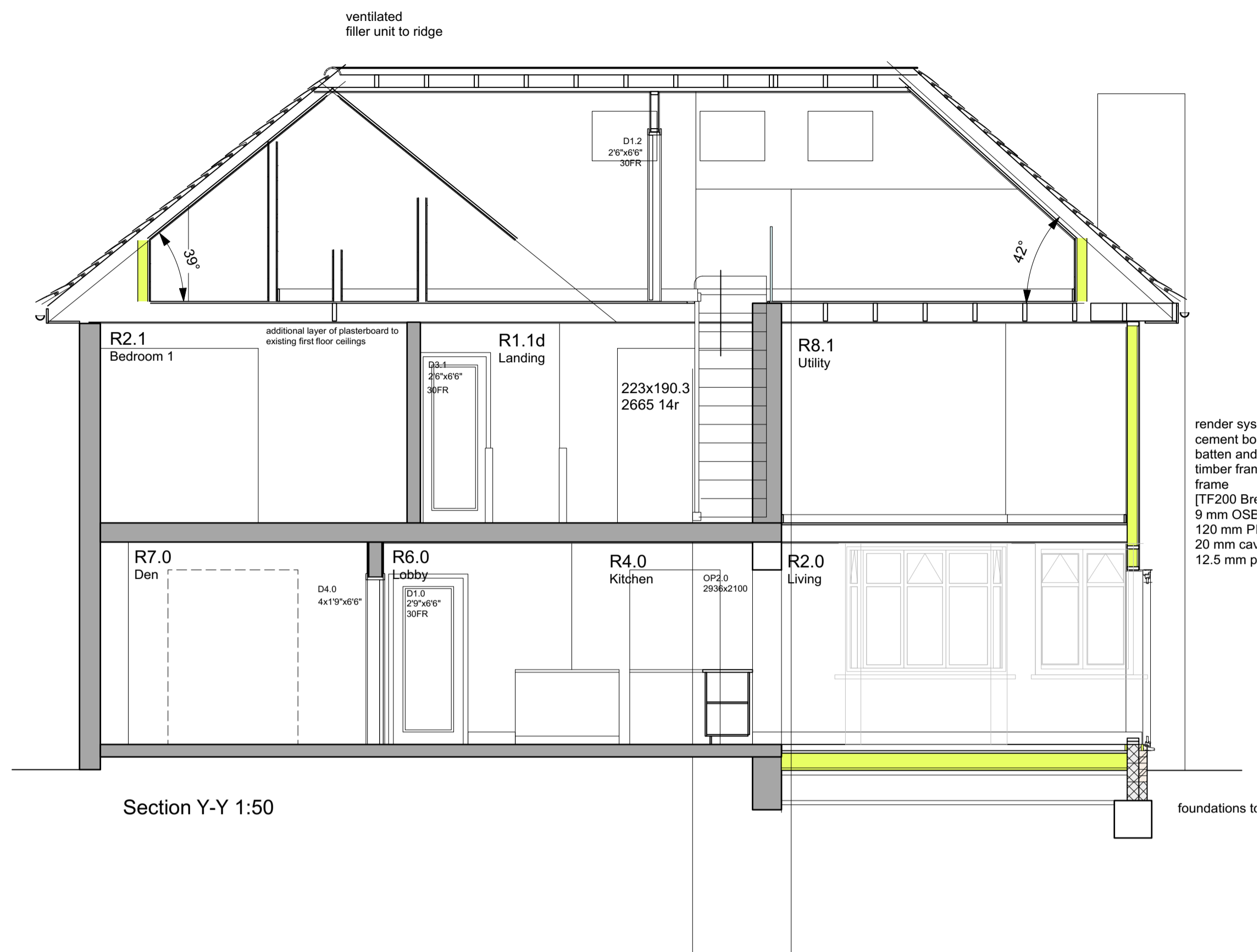
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PRELIMINARY

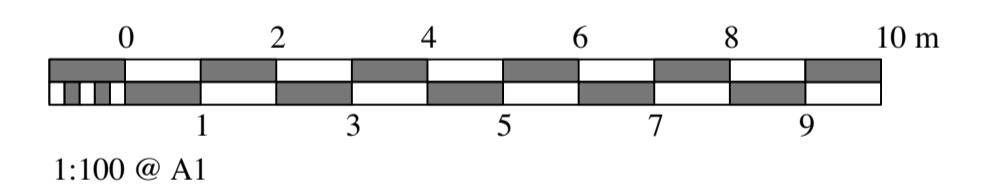
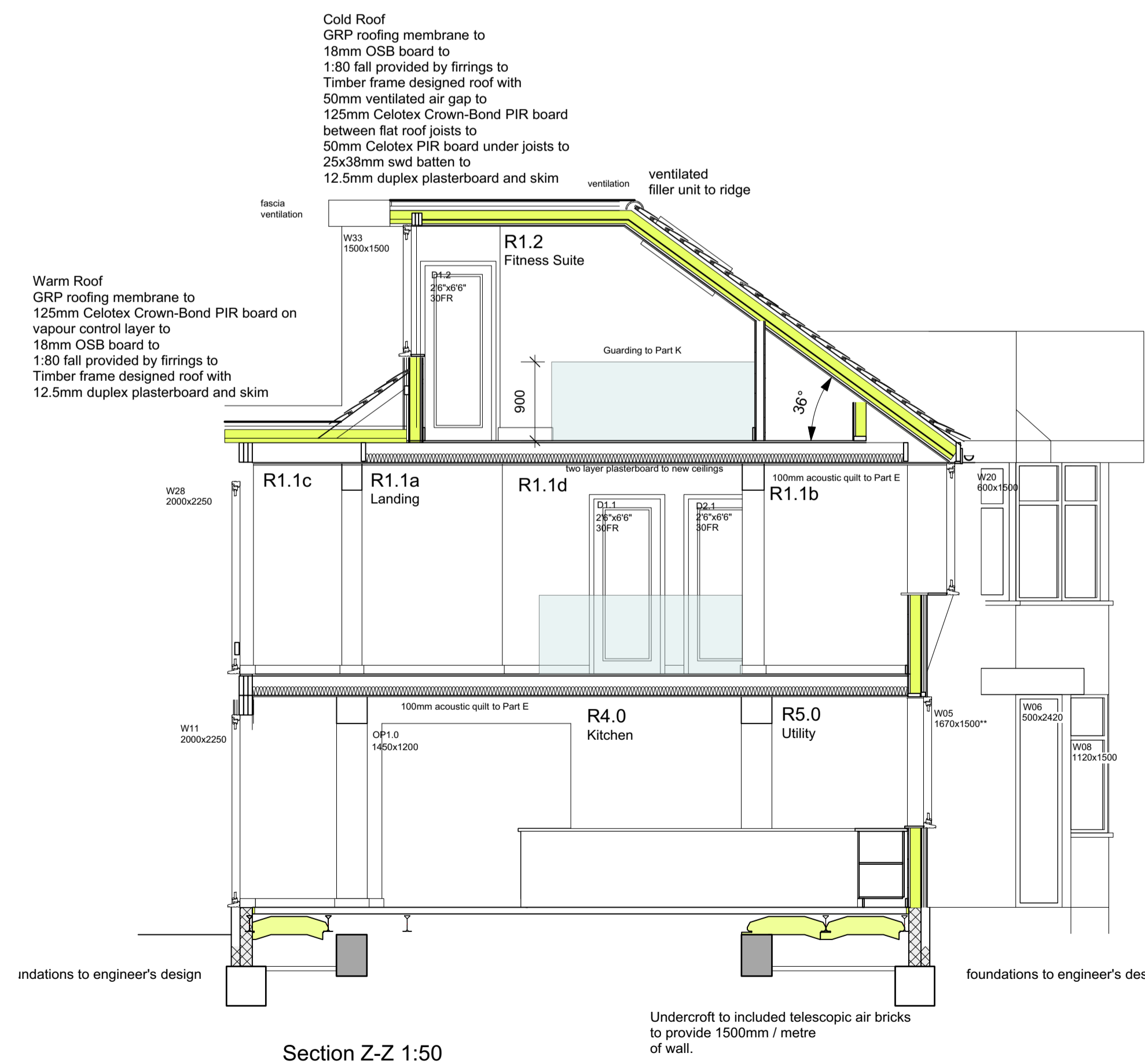
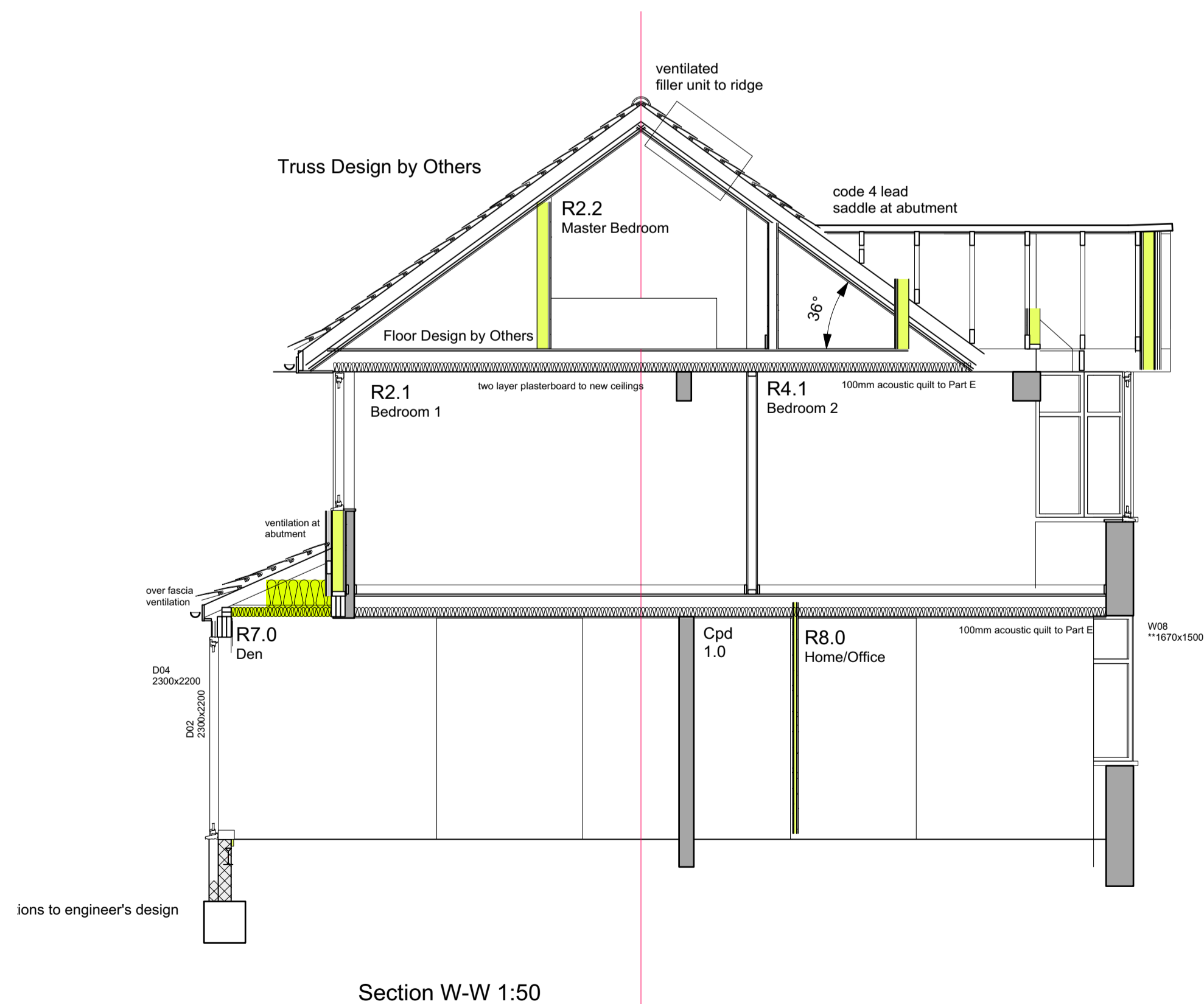
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PRELIMINARY

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Project: PROPOSED EXTENSION & ALTERATIONS TO 2 WESTCOOMBE AVENUE LONDON SW20 ORQ

Drawing: PROPOSED SECTIONS

Drg No: 1628/20/BR/03

Scales: 1:50 Date: MAY 2020

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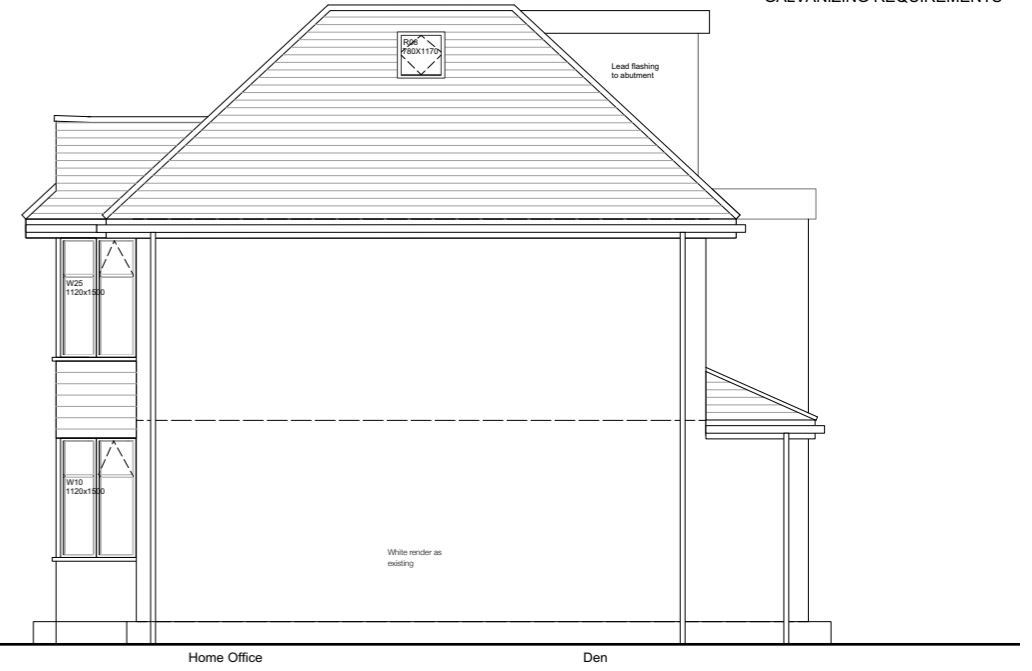
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West Elevation [Rear] 1:50



North Elevation [Side] 1:50

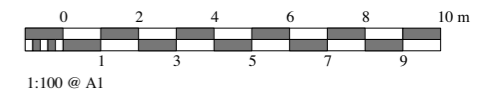
Page 214



East Elevation [Front] 1:50



South Elevation [Side] 1:50



PRELIMINARY

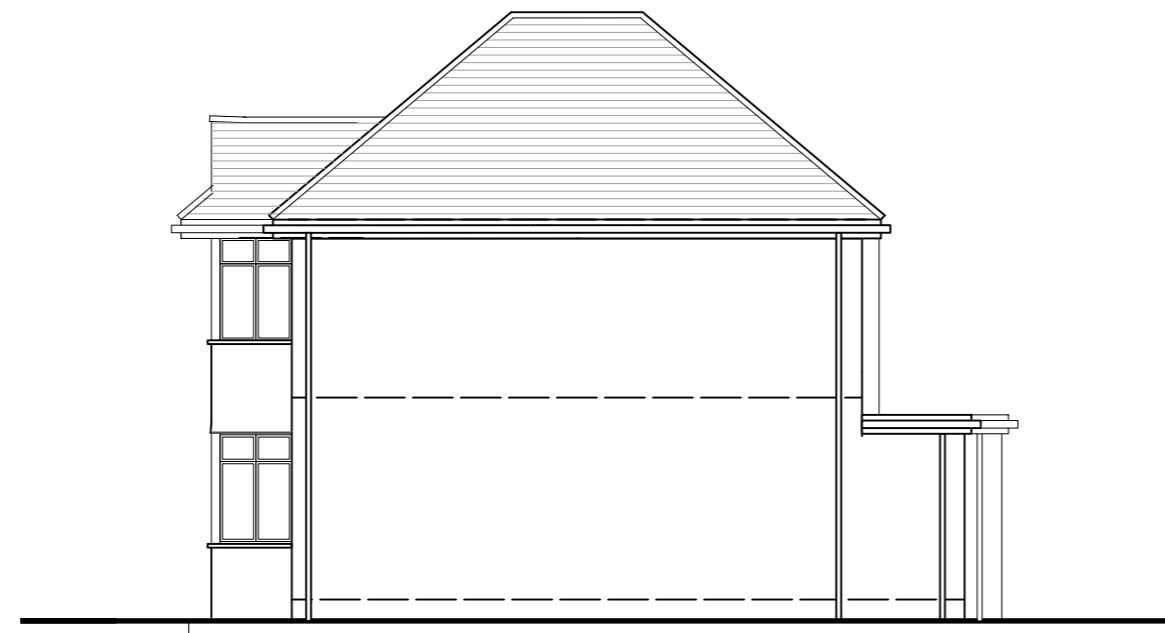
Rev Date Amend Revision
Gerald Moore RIBA. Chartered Architect
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 Client: MR & MRS P LOMBAARD
 Project: PROPOSED EXTENSION & ALTERATIONS TO 2 WESTCOOMBE AVENUE LONDON SW20 ORQ
 Drawing: PROPOSED ELEVATIONS
 Drwg No: 1628/20/BR/04
 Scales: 1:50 Date: MAY 2020
 Drawn: gm Drwg Size: A1

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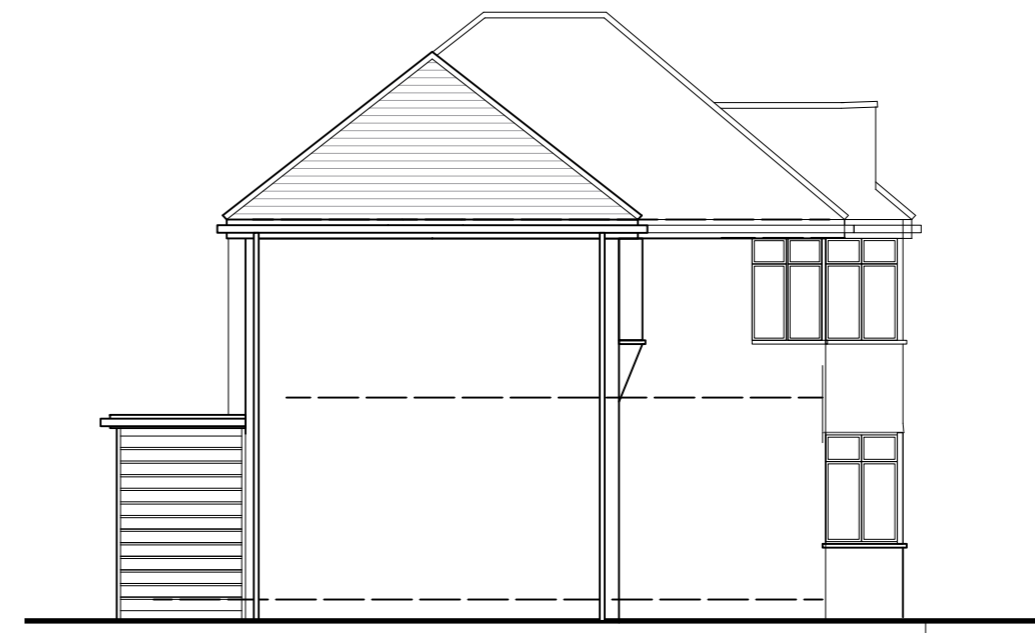
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Dining Room Utility
West Elevation [Rear] 1:100



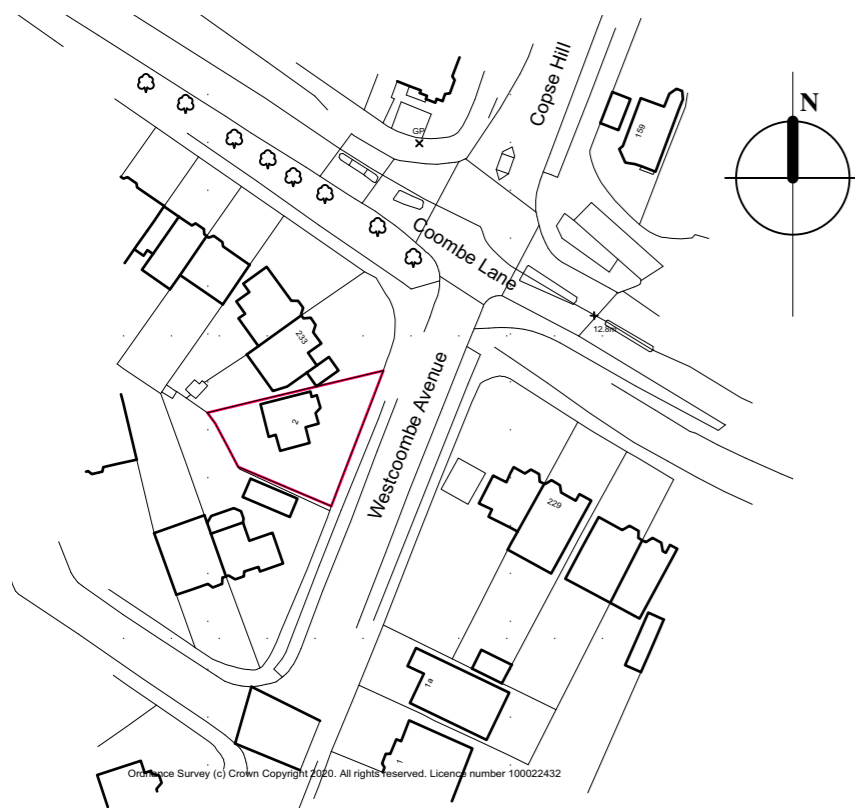
Living Dining
North Elevation [Side] 1:100



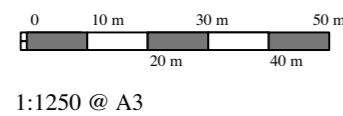
Kitchen Entrance Hall
South Elevation [Side] 1:100



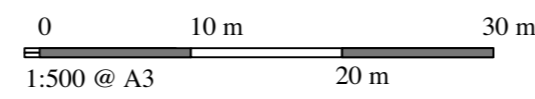
Kitchen Entrance Hall Home Office
East Elevation [Front] 1:100



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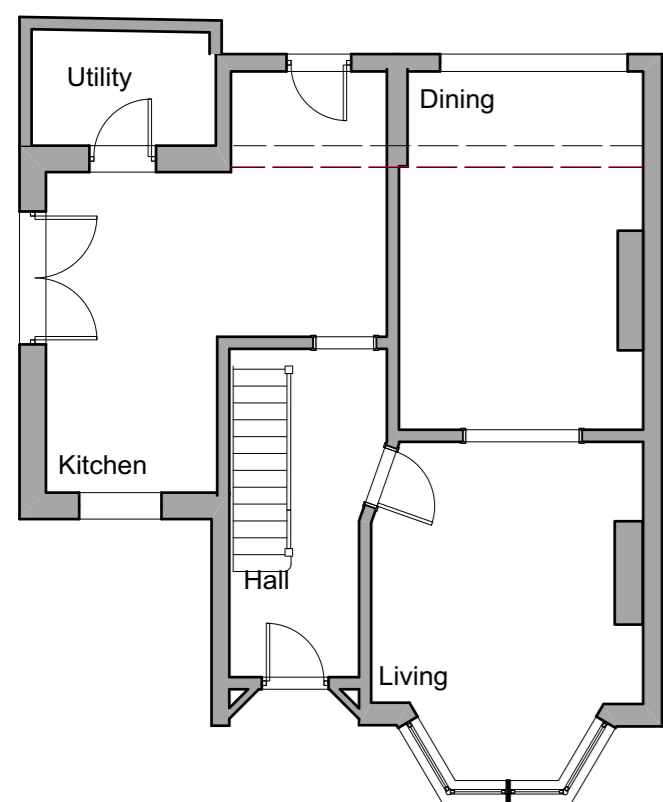
Location Plan 1:1250



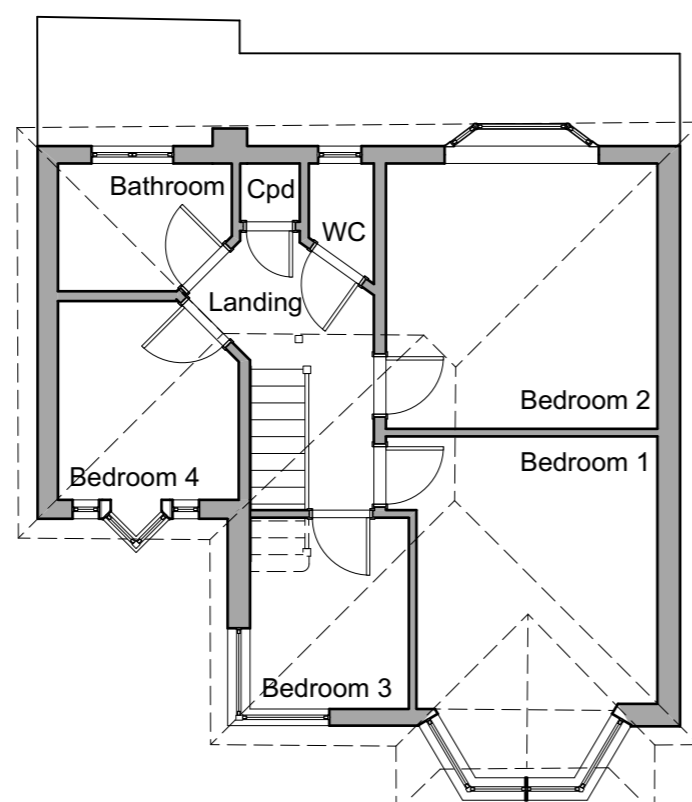
Block/Roof Plan [Existing] 1:500



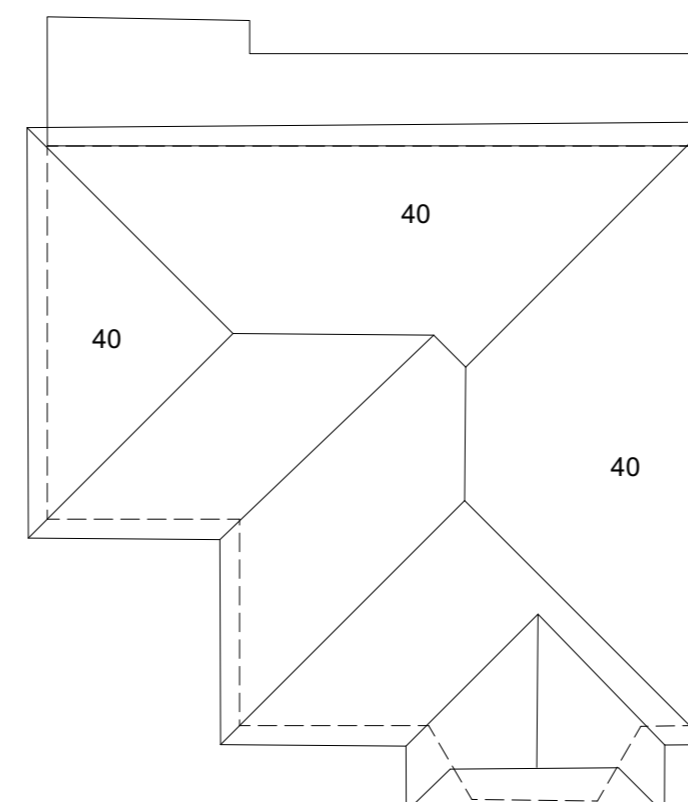
1:100 @ A2



Ground Floor Plan 1:100



First Floor Plan 1:100



Roof Plan 1:100

Rev Date Amend Revision

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Client: MR & MRS P LOMBAARD

Project: PROPOSED EXTENSION & ALTERATIONS TO 2 WESTCOOMBE AVENUE LONDON SW20 ORQ

Drawing: EXISTING

Drg No: 1628/20/BR/05

Scales: 1:1250, 1:500, 1:100

Date: JUNE 2020

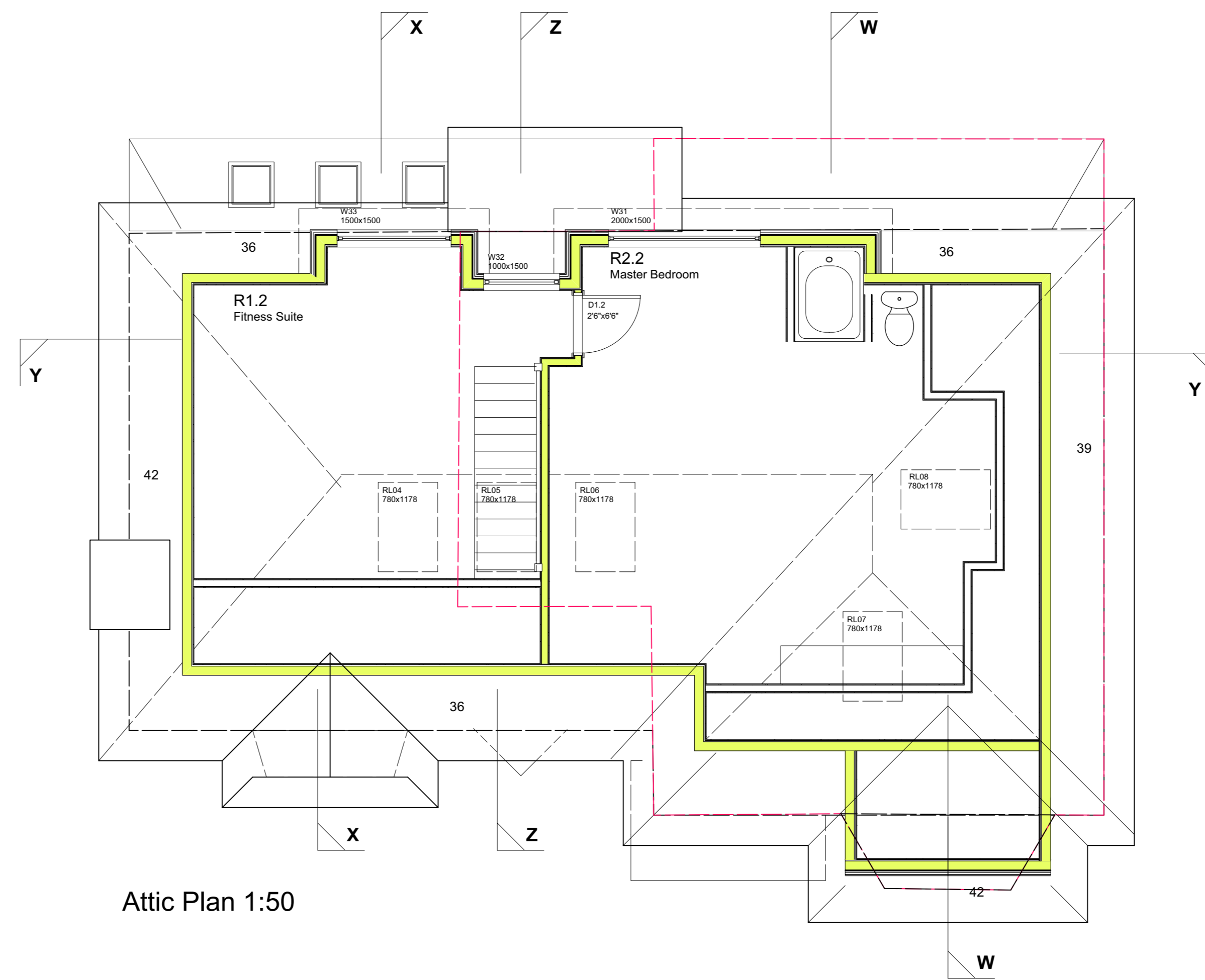
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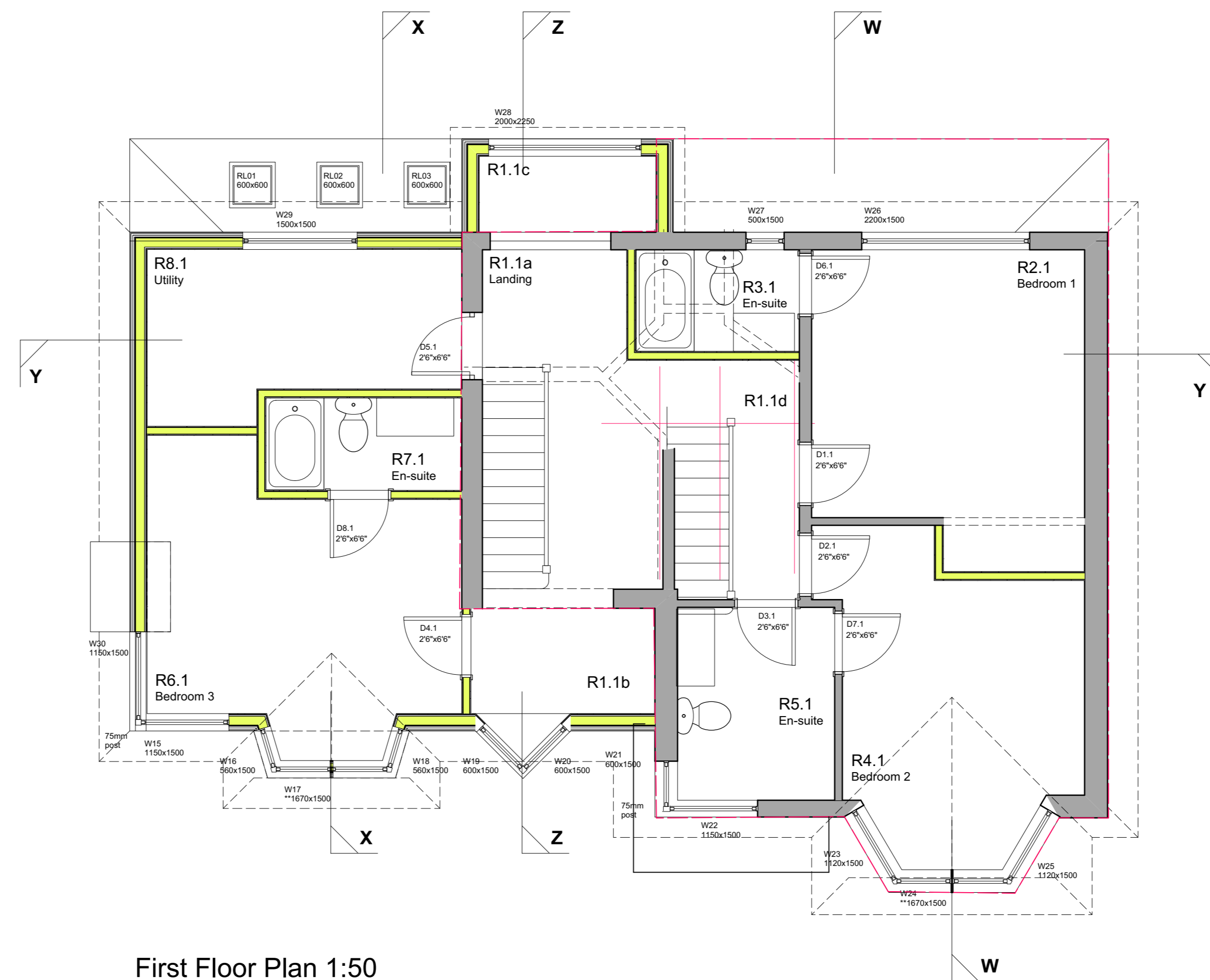
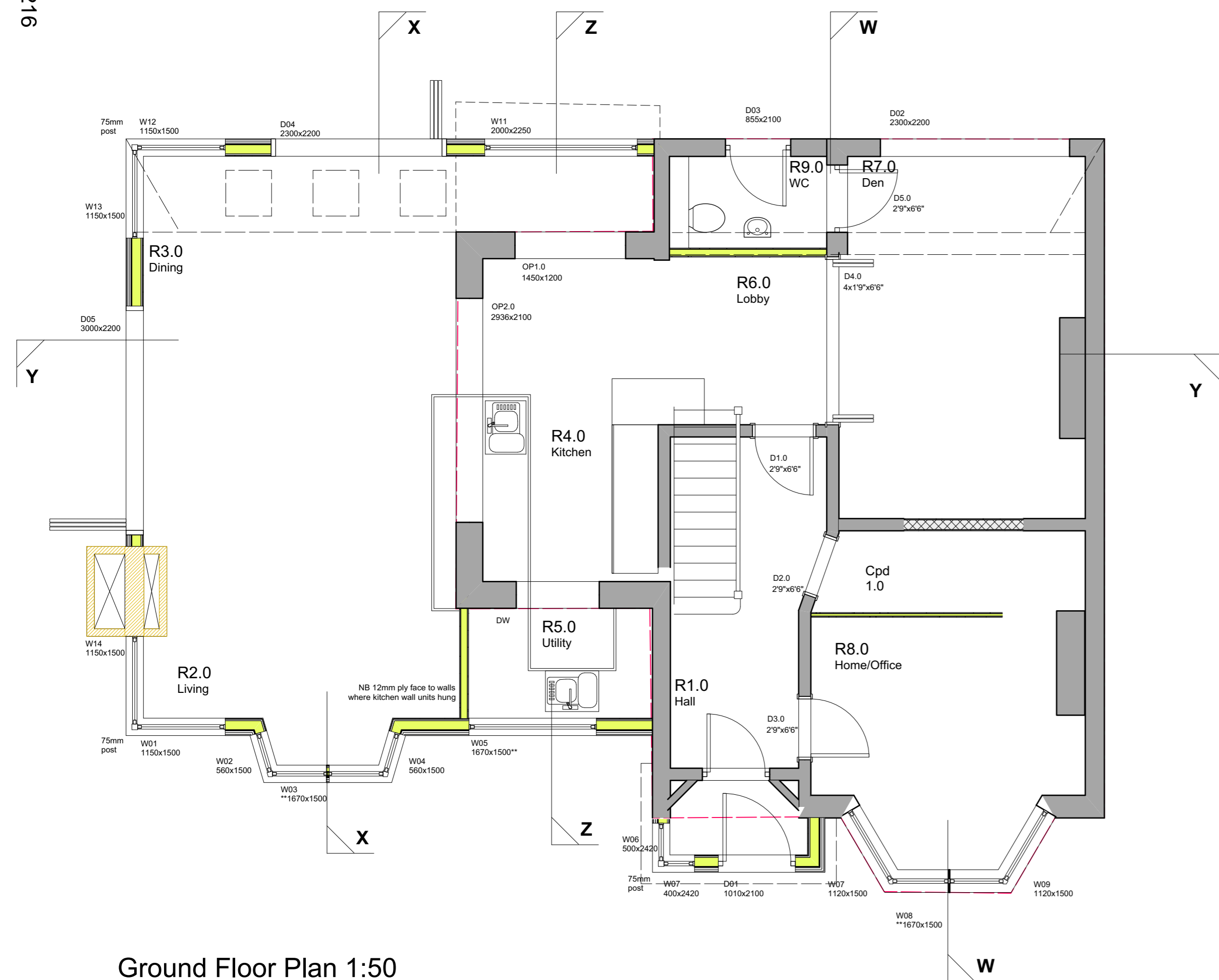
SUBJECT TO BUILDING REGULATION APPROVAL

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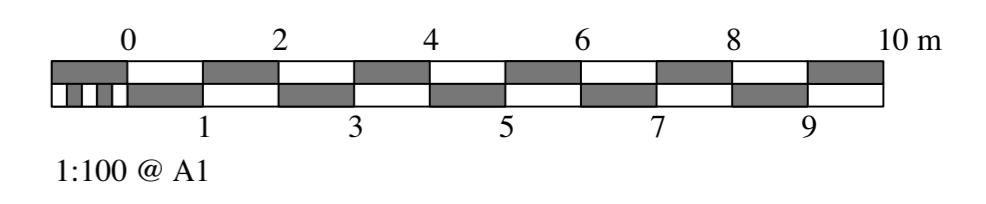
N.B. STRUCTURAL INFORMATION TO BE READ ALONGSIDE STRUCTURAL ENGINEER'S CALCULATIONS SHEETS & SKETCH DETAILS. ATTENTION TO BE PAID TO PADSTONE, BOLT FIXING, BASE PLATES, & GALVANIZING REQUIREMENTS



Page 216



PRELIMINARY



Rev Date Amend Revision
Gerald Moore RIBA, Chartered Architect
 1 Exmor Crescent Worthing West Sussex BN13 2PL
 Tel: 01903 609607 Mobile: 07793 00 44 62
 www.geraldmooreriba.co.uk e-mail: contact@geraldmooreriba.co.uk
 Client: MR & MRS P LOMBAARD
 Project: PROPOSED EXTENSION & ALTERATIONS TO 2 WESTCOOMBE AVENUE LONDON SW20 ORQ
 Drawing: PROPOSED FLOOR PLANS [GENERAL ARRANGEMENT]
 Drg No: 1628/20/GA/01
 Scales: 1:50 Date: JUNE 2020
 Drawn: gm Drwg Size: A1

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Option A

Option B



Existing

Similar to neighbour at
233 Coombe Lane
AND existing

Roof
tiles



Existing

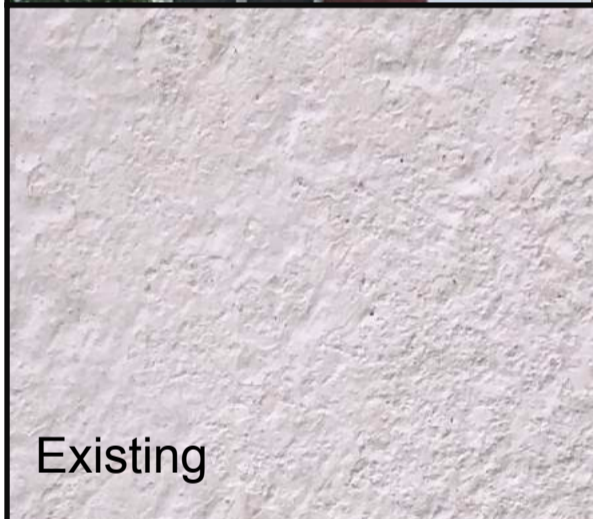


Marley Antique Brown



Plain Tile, 18 Hedgerow
Brown (Granular), Concrete

Render



Existing



K Rend Silicone TC10 Coloured
Render WHITE
Maintenance Free



K Rend Silicone TC10 Coloured
Render WHITE
Maintenance Free

Bricks



Existing

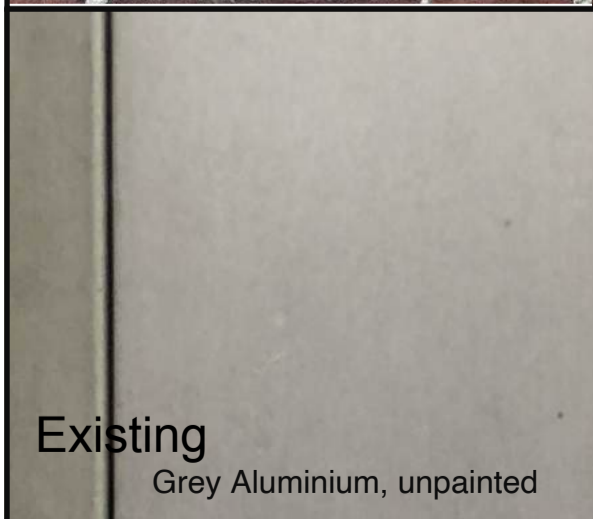


FORTERRA LBC BRINDLE PRESSED
FACING BRICK



London Brick Company Facing
Brick Sandfaced

Window
frame



Existing

Grey Aluminium, unpainted



Grey Aluminium, Coated



Grey Aluminium, Coated

Option A

Using the same tiles as existing + 233 Coombe
Lane Neighbour
Bricks to be colour match to existing
Render similar texture to existing



Option A

Using the same tiles as existing + 233 Coombe
Lane Neighbour
Bricks to be colour match to existing
Render similar texture to existing



Option B

Using the same tiles as several other local homes
Bricks to be colour match 233 Coombe Lane and
239 Coombe Lane (both direct neighbours)
Render similar texture to existing

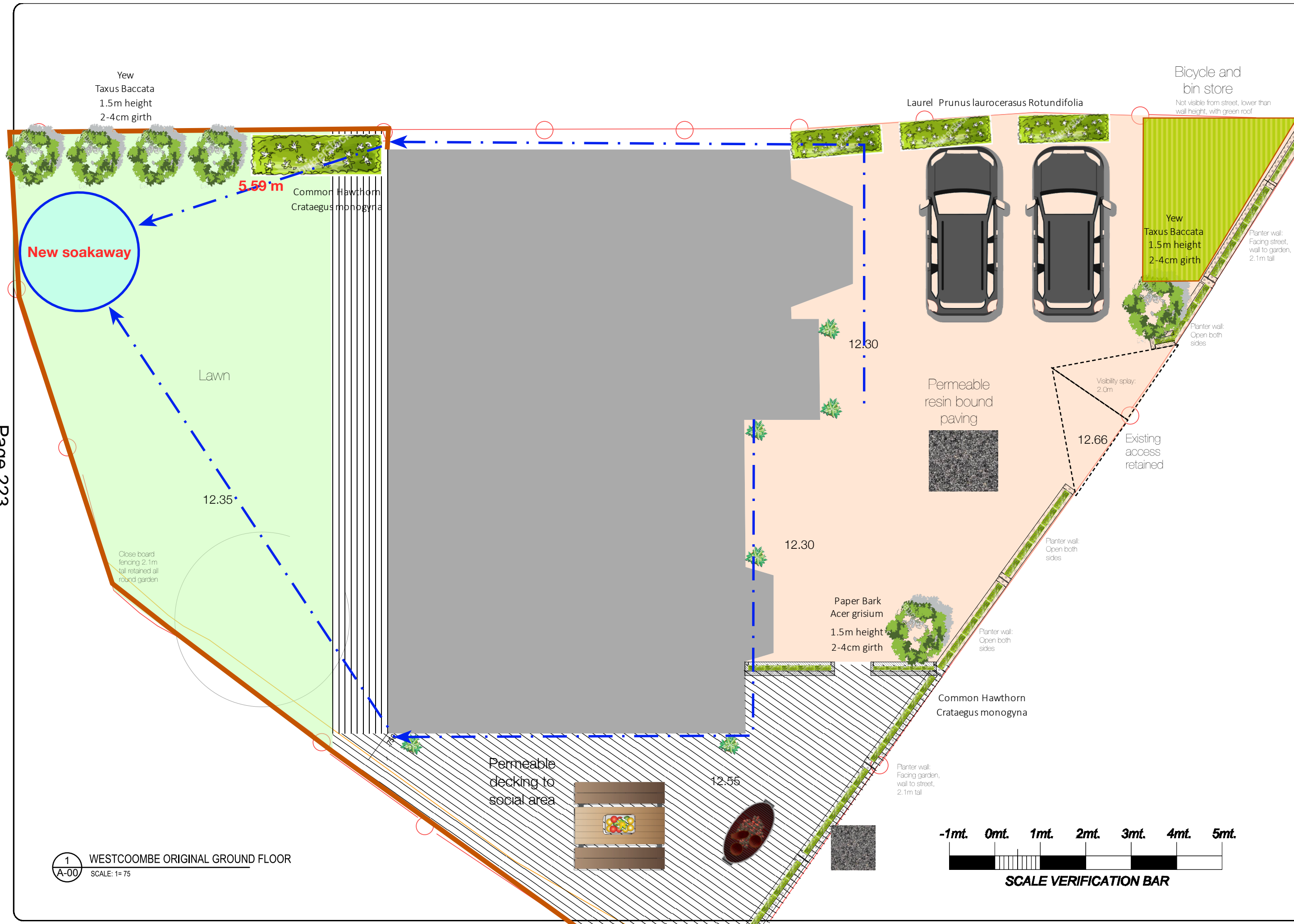


Option B

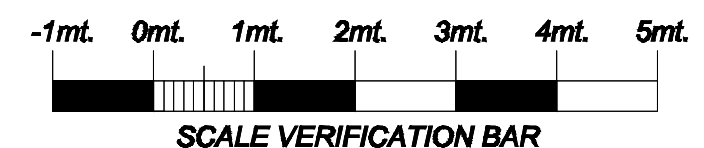
Using the same tiles as several other local homes
Bricks to be colour match 233 Coombe Lane and
239 Coombe Lane (both direct neighbours)
Render similar texture to existing



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1 WESTCOOMBE ORIGINAL GROUND FLOOR
 A-00 SCALE: 1=75



REVISIONS
OAKFIELD GU21 Ltd CHRISTIAN DA SILVA
WESTCOOMBE 2 Westcoombe avenue SW20 0EQ
PROPOSED LANDSCAPING PLAN
27 APR 2020
CLIENT: MR. & MRS LONBAARD
DRAWING NR LA-01

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Committee: Planning Applications

Date: 22nd October 2020

Subject: Planning Appeal Decisions

Lead officer: Head of Sustainable Communities

Lead member: Chair, Planning Applications Committee

Recommendation:

That Members note the contents of the report.

1. PURPOSE OF REPORT AND EXECUTIVE SUMMARY

- 1.1 For Members' information recent decisions made by Inspectors appointed by the Secretary of State for Communities and Local Government in respect of recent Town Planning Appeals are set out below.
- 1.2 The relevant Inspectors decision letters are not attached to this report but can be viewed by following each individual link. Other agenda papers for this meeting can be viewed on the Committee Page of the Council Website via the following link:

[LINK TO COMMITTEE PAGE](#)

DETAILS

THERE HAVE BEEN NO APPEAL DECISIONS ISSUED BY THE PLANNING INSPECTORATE THIS MONTH.

Alternative options

- 3.1 The appeal decision is final unless it is successfully challenged in the Courts. If a challenge is successful, the appeal decision will be quashed and the case returned to the Secretary of State for re-determination. It does not follow necessarily that the original appeal decision will be reversed when it is re-determined.
- 3.2 The Council may wish to consider taking legal advice before embarking on a challenge. The following applies: Under the provision of Section 288 of the Town & Country Planning Act 1990, or Section 63 of the Planning (Listed Buildings and

Conservation Areas) Act 1990, a person or an establishment who is aggrieved by a decision may seek to have it quashed by making an application to the High Court on the following grounds: -

1. That the decision is not within the powers of the Act; or
2. That any of the relevant requirements have not been complied with; (relevant requirements means any requirements of the 1990 Act or of the Tribunal's Land Enquiries Act 1992, or of any Order, Regulation or Rule made under those Acts).

1 CONSULTATION UNDERTAKEN OR PROPOSED

- 1.1. None required for the purposes of this report.

2 TIMETABLE

- 2.1. N/A

3 FINANCIAL, RESOURCE AND PROPERTY IMPLICATIONS

- 3.1. There are financial implications for the Council in respect of appeal decisions where costs are awarded against the Council.

4 LEGAL AND STATUTORY IMPLICATIONS

- 4.1. An Inspector's decision may be challenged in the High Court, within 6 weeks of the date of the decision letter (see above).

5 HUMAN RIGHTS, EQUALITIES AND COMMUNITY COHESION IMPLICATIONS

- 5.1. None for the purposes of this report.

6 CRIME AND DISORDER IMPLICATIONS

- 6.1. None for the purposes of this report.

7 RISK MANAGEMENT AND HEALTH AND SAFETY IMPLICATIONS

- 7.1. See 6.1 above.

8 BACKGROUND PAPERS

8.1. The papers used to compile this report are the Council's Development Control service's Town Planning files relating to the sites referred to above and the agendas and minutes of the Planning Applications Committee where relevant.

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Committee: Planning Applications Committee

Date: 22nd October 2020

Agenda item:

Wards: All

Subject: PLANNING ENFORCEMENT - SUMMARY OF CURRENT CASES

Lead officer: HEAD OF SUSTAINABLE COMMUNITIES

Lead member: CABINET MEMBER FOR REGENERATION, HOUSING AND
TRANSPORT COUNCILLOR MARTIN WHELTON

COUNCILLOR LINDA KIRBY, CHAIR, PLANNING APPLICATIONS COMMITTEE

Contact Officer Ray Littlefield: 0208 545 3911
Ray.Littlefield@merton.gov.uk

Recommendation:

That Members note the contents of the report.

1. Purpose of report and executive summary

This report details a summary of casework being dealt with by the Planning Enforcement Team and contains figures of the number of different types of cases being progressed, with brief summaries of all new enforcement notices and the progress of all enforcement appeals.

Current Enforcement Cases:	464	¹ (443)	New Appeals:	(0)	(0)
New Complaints	29	(33)	Instructions to Legal	0	(0)
Cases Closed	18		Existing Appeals	2	(2)
No Breach:	11		<hr/>		
Breach Ceased:	7		TREE ISSUES		
NFA ² (see below):	0		Tree Applications Received	111	(233)
Total	18		% Determined within time limits:	75%	
New Enforcement Notices Issued			High Hedges Complaint	0	(1)
Breach of Condition Notice:	0		New Tree Preservation Orders (TPO)	0	(1)
New Enforcement Notice issued	0	(0)	Tree Replacement Notice	0	
S.215: ³	0		Tree/High Hedge Appeal	0	(0)
Others (PCN, TSN)	1	(0)			
Total	0	(0)			
Prosecutions: (instructed)	0	(0)			

Note (*figures are for the period from (22nd September 2020 to 13th October 2020*). The figure for current enforcement cases was taken directly from M3 crystal report.

¹ Totals in brackets are previous month's figures

² confirmed breach but not expedient to take further action.

³ S215 Notice: Land Adversely Affecting Amenity of Neighbourhood.

2.0 New Enforcement Actions

283 Galpins Road CR7 6EY. This is concerning a s215 notice served on untidy land. A s215 notice was issued on 23 December 2019. This notice required compliance at the end of February 2020 requiring the Land to be tidied up / cleared. Notice now complied with.

31 Edgehill Road, Mitcham, CR4 2HY. This is concerning a raised platform/garden that has been raised by approximately 90cm. An enforcement notice has been served to remove the raised platform and reduce the garden level by 90cm. The notice would have taken effect on 18/12/19, with a compliance date of 18/03/20, however an appeal has been submitted and is underway.

193 London Road CR4 2TJ. This is concerning untidy land to the side and rear of 193 London Road. An initial site visit was carried out, multiple letters have been sent to the property asking for compliance and for them to contact the Council to confirm a compliance schedule of works. Correspondence from the owner has been received. A further visit was made to confirm the site has not been tidied. A s215 enforcement Notice for untidy land has been drafted and is due to be reviewed and signed off by a manager authorising the service of a s215 Notice. The Land is actively being cleared.

155 Canterbury Road, Morden, SM4 6QG. This is concerning an outbuilding in the rear garden that has had a retrospective planning application refused. An enforcement notice has been served on the property for the outbuilding to be demolished, the notice would have taken effect on 9th December 2019 and the compliance period would have been two months. However it has now been appealed to the Planning Inspectorate. The appeal was dismissed by Decision letter dated 19th August 2020. The compliance date i.e. Demolish the unauthorised rear outbuilding is 19th December 2020.

208 Bishopsford Road, Morden, SM4 6DA. This is concerning the erection of a single storey rear extension onto an existing extension on the ground floor. A Planning Enforcement Notice has been issued requiring the demolition of the Extension. The Notice was issued on 4th October 2019, the Notice came into effect on 10th November 2019 with a compliance period of 3 months, unless an appeal was made before 10th November 2019. An appeal was submitted but rejected by the Planning Inspectorate as it was received by The Planning Inspectorate one day late. Compliance date was 10th February 2020. Further action is under consideration. A new planning application is to be submitted and the structure reduced in size.

The former laundry site, 1 Caxton Road, Wimbledon SW19 8SJ. Planning Permission was granted for 9 flats, with 609square metres of (Class B1) office units. 22 flats have been created. A Planning Enforcement Notice was issued on 11th October 2018 requiring either the demolition of the development or building to the approved scheme. The Notice took effect on 18th November 2018 with a compliance period of 12 calendar months. An appeal was made but subsequently withdrawn the following day. The owner decided to comply with the approved permission and is in the process of returning some the residential units back to their authorised office use. Bath and shower units have been removed; the office units are currently being advertised for let. The garage flat is no longer being used for residential and is in the process of being returned to a garage. Planning Application 19/P1527 for Discharge of Conditions has been submitted and is currently being considered. Revised scheme re-submitted and is currently under consideration.

Works are underway to expose the depth and boundary of the foundations in order to confirm an alternative landscaping scheme is feasible. A further scheme is under consideration.

6 CARTMEL GARDENS, MORDEN SM4 6QN: (Notice 2) This is regarding a side extension not built in accordance with approved plans and being used as a self contained unit of accommodation. A planning Enforcement Notice was subsequently issued on 24th September 2019 and took effect on 24th October 2019. The Notice

requires the cessation of the use of side extension as separate self-contained unit, and the removal of all those fixtures and fittings that facilitate the unauthorised use of the extension including the permanent removal of the facilities in use for cooking facilities, kitchen unit, sink, worktop, appliances, and food preparation areas. This Notice has a compliance period of 3 calendar months. An appeal was submitted but subsequently withdrawn. A second Notice was subject of an appeal now determined.

Some Recent Enforcement Actions

7 Streatham Road, Mitcham, CR4 2AD

The Council served two enforcement notices on 6th June 2019, requiring the outbuilding to be demolished and to clear debris and all other related materials. The second enforcement notice is for an unauthorised front, side and rear (adjacent to Graham Road) dormer roof extensions. An appeal was lost for the dormers to be considered permitted development, the notice requires the owner to demolish the unauthorised front, side and rear roof dormer extensions (adjacent to Graham Road) and to clear debris and all other related materials. Both Notices came into effect on 8th July 2019 unless appeals were made before this date. No appeals were lodged.

The compliance date of the Enforcement Notice relating to the outbuilding to be demolished and to clear debris and all other related materials has now passed without compliance. The second enforcement notice was not complied with and now prosecution proceedings are being undertaken.

The plea hearing has now taken place at Lavender Hill Magistrates Court, where the defendant pleaded not guilty and the second hearing is due on the 14th January 2020.

A second hearing was held on 14th January 2020, and adjourned until 4th February 2020 in order for the defendant to seek further legal advice.

The defendant again appeared in court and pleaded not guilty, a trial date was set for 21st May 2020. Due to the Covid-19 pandemic this has been postponed. The case has been listed for a 'non-effective' hearing on Tuesday 14 July 2020, where a new trial date will be set.

This was postponed until another date yet to be given. The Council has now instructed external Counsel to prosecute in these matters.

The next 'non-effective' hearing date is 2nd October 2020. This date has been re-scheduled to 27th November 2020.

3.00 New Enforcement Appeals

0

6 CARTMEL GARDENS, MORDEN SM4 6QN: (Notice 1) This is regarding a side extension not built in accordance with approved plans. A planning Enforcement Notice was subsequently issued on 24th September 2019 and would have taken effect on 24th October 2019. The notice requires the demolition of the rear extension. This Notice has a compliance period of 3 calendar months. An Appeal was electronically submitted. This Appeal has now been determined by Decision letter dated 23rd June 2020. The Appeal was dismissed and the Enforcement Notice upheld. The compliance period is 3 months from the date of the Decision letter.

183A Streatham Road CR4 2AG. An Enforcement Notice was issued on 1st May 2019 relating to the erection of a rear balcony to the existing rear roof dormer of the property. The Notice requires demolishing the rear balcony to the existing rear roof dormer and restoring the property to that prior to the breach. The Notice would have taken effect on 4th June 2019, with a compliance period of 2 months. An Appeal to The Planning Inspectorate has been made. The appeal was determined by Decision letter dated 18th March 2020. The appeal was dismissed with a slight variation of the wording of the enforcement Notice. The Enforcement Notice had a 2 months compliance period. A site inspection is to be arranged.

47 Edgehill Road CR4 2HY. This is concerning a rear extension not being built to the dimensions provided on the prior approval application. A Planning Enforcement Notice was subsequently issued requiring the demolition of the single storey rear extension. The Notice would have taken effect took effect on 16th September 2019, with a compliance period of 3 calendar months. An Appeal has started. This Appeal has now been determined by Decision letter dated 16th July 2020. The appeal was allowed and the Enforcement Notice quashed.

33 HASSOCKS ROAD, LONDON. SW16 5EU: This was regarding the unauthorised conversion from a single dwelling into 2 x self contained flats against a refusal planning permission. A planning Enforcement Notice was subsequently issued on 10th September 2019 and would have taken effect on 15th October 2019. This Notice has a compliance period of 3 calendar months, unless an appeal is made to the Planning Inspectorate before the Notice takes effect. An Appeal has been submitted, and has started. The appeal site visit was postponed, by The Planning Inspectorate. This Appeal has now been determined by Decision letter dated 17th July 2020. The Appeal was dismissed and the Enforcement Notice upheld. The Notice was varied and the time for compliance extended from 3 months to 6 months from the date of the Appeal Decision letter. However, minor costs were awarded to the appellant for extra work and or time that had been spent on the appeal that were not needed.

76 Shaldon Drive, Morden, SM4 4BH. An enforcement notice was served on 14th August 2019 relating to an outbuilding being used as a self-contained unit. The notice requires the removal of all kitchen facilities, fixtures, fittings, cooker, worktops, kitchen units. The notice takes effect on 16th September 2019, with a compliance period of 1 month. An Appeal has been electronically submitted, This Appeal has now started. The date of the Planning Inspectors site visit is 20th October 2020.

1.1.1. **Existing enforcement appeals**

2

Appeals determined

0

3.4 Requested update from PAC

None

4. Consultation undertaken or proposed

None required for the purposes of this report

5 Timetable

N/A

6. Financial, resource and property implications

N/A

7. Legal and statutory implications

N/A

8. Human rights, equalities and community cohesion implications

N/A

9. Crime and disorder implications

N/A

10. Risk Management and Health and Safety implications.

N/A

11. Appendices – the following documents are to be published with this report and form part of the report Background Papers

N/A

12. Background Papers